

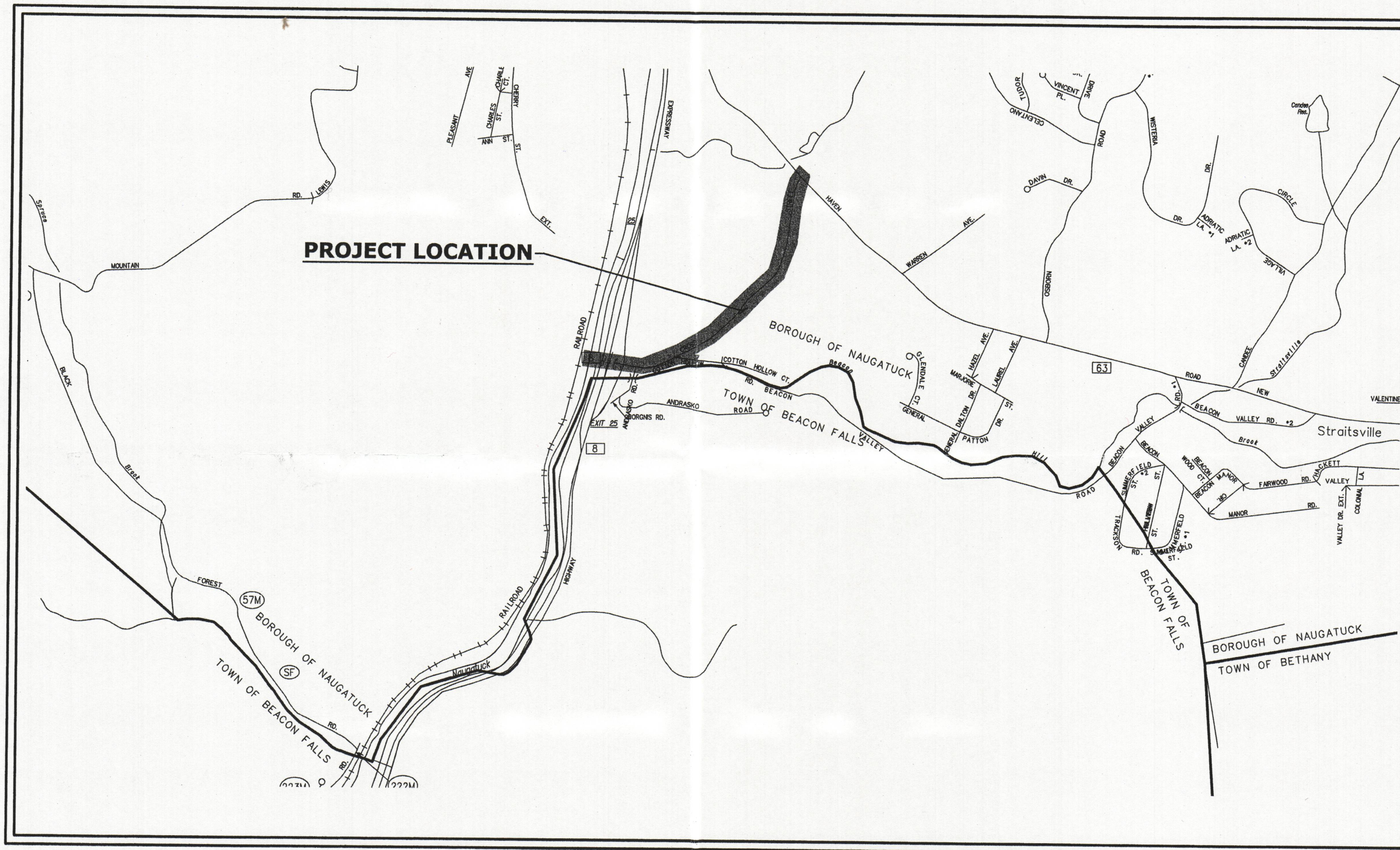
# RECONSTRUCTION OF CROSS STREET

NAUGATUCK, CONNECTICUT  
 CTDOT PROJECT NO. 87-145  
 FEDERAL AID PROJECT NO. 1087 (114)  
 MMI PROJECT NO. 2129-28

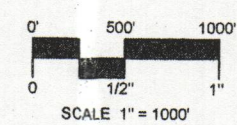
CONSTRUCTION DOCUMENTS  
 SEPTEMBER 7, 2018

## GENERAL NOTES

1. BASED UPON FIELD TOPOGRAPHIC SURVEY CONDUCTED BY: MILONE & MACBROOM, INC. AND SHOWN ON THE PLANS ENTITLED, "RIGHT OF WAY AND TOPOGRAPHIC SURVEY" FOR THE RECONSTRUCTION OF CROSS STREET FROM ROUTE 8 NORTHEASTERLY TO THE INTERSECTION OF NEW HAVEN ROAD (ROUTE 63), SHEET NUMBERS: S01 THROUGH S10, SCALE: 1"=20' REVISED ON: SEPT. 15, 2018. STREETLINE INFORMATION DEPICTED ON THESE PLANS WERE COMPILED FROM VARIOUS SOURCES AND MEETS A-2/T-2 STANDARDS. NORTH ARROW AND BEARINGS ARE BASED UPON THE CONNECTICUT COORDINATE SYSTEM (NAD 1983), ESTABLISHED WITH G.P.S. ELEVATIONS, CONTOURS AND BENCHMARKS ARE BASED UPON NAVD 1988.
2. INFORMATION REGARDING THE LOCATION OF EXISTING UTILITIES HAS BEEN BASED UPON AVAILABLE INFORMATION AND MAY BE INCOMPLETE, AND WHERE SHOWN SHOULD BE CONSIDERED APPROXIMATE. THE LOCATION OF ALL EXISTING UTILITIES SHOULD BE CONFIRMED PRIOR TO BEGINNING CONSTRUCTION. CALL "CALL BEFORE YOU DIG", 811 OR CBYD.COM AT LEAST TWO FULL WORKING DAYS PRIOR TO CONSTRUCTION. ALL UTILITY LOCATIONS THAT DO NOT MATCH THE VERTICAL OR HORIZONTAL CONTROL SHOWN ON THE PLANS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION.
3. MILONE & MACBROOM INC. ACCEPTS NO RESPONSIBILITY FOR THE ACCURACY OF MAPS AND DATA WHICH HAVE BEEN SUPPLIED BY OTHERS.
4. THE EXACT LOCATION AND SIZE OF ELECTRIC, TELEPHONE, CABLE TELEVISION AND GAS UTILITIES ARE TO BE DETERMINED BY THE RESPECTIVE UTILITY COMPANIES. LIMITS SHOWN ON THE PLANS ARE GRAPHICAL IN NATURE AND SHOULD BE FIELD VERIFIED.
5. ALL DIMENSIONS AND ELEVATIONS SHALL BE VERIFIED IN THE FIELD PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER.
6. SEDIMENT AND EROSION CONTROL MEASURES AS DEPICTED ON THESE PLANS AND DESCRIBED WITHIN THE SEDIMENT AND EROSION CONTROL NARRATIVE SHALL BE IMPLEMENTED AND MAINTAINED UNTIL PERMANENT COVER AND STABILIZATION IS ESTABLISHED. ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL CONFORM TO THE "GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL", CONNECTICUT - 2002, THE BOROUGH OF NAUGATUCK AND IN ALL CASES BEST MANAGEMENT PRACTICES SHALL PREVAIL.
7. ALL DISTURBED LAWN AREAS SHALL RECEIVE A MINIMUM OF 4" TOPSOIL, AND BE SEEDED WITH GRASS AS SHOWN ON THE PLANS.
8. ALL PROPOSED CONTOURS AND SPOT ELEVATIONS INDICATE FINISHED GRADE.
9. THE PLANS REQUIRE A CONTRACTOR'S WORKING KNOWLEDGE OF LOCAL, MUNICIPAL, WATER AUTHORITY, AND STATE CODES FOR UTILITY SYSTEMS. ANY CONFLICTS BETWEEN MATERIALS AND LOCATIONS SHOWN, AND LOCAL REQUIREMENTS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO THE EXECUTION OF WORK. THE ENGINEER WILL NOT BE HELD LIABLE FOR COSTS INCURRED TO IMPLEMENT OR CORRECT WORK WHICH DOES NOT CONFORM TO LOCAL CODE.
10. COMPLIANCE WITH THE PERMIT CONDITIONS IS THE RESPONSIBILITY OF BOTH THE CONTRACTOR AND THE PERMITTEE.
11. THE CONTRACTOR MUST MAINTAIN (REPAIR/REPLACE WHEN NECESSARY) THE SILTATION CONTROL UNTIL ALL WORK IS COMPLETED AND ALL DISTURBED AREAS ARE PERMANENTLY STABILIZED.
12. ALL CONSTRUCTION MATERIALS AND METHODS SHALL CONFORM TO THE APPLICABLE SECTIONS OF THE THE BOROUGH OF NAUGATUCK STANDARDS AND SPECIFICATIONS, AND STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, FORM 817 INCLUDING SUPPLEMENTAL SPECIFICATIONS TO STANDARD SPECIFICATIONS DATED JANUARY, 2018 AND AS SUPPLEMENTED IN THE SPECIAL PROVISIONS.
13. ALL MAILBOXES SHALL BE RELOCATED USING EXISTING SUPPORTS, WHERE EXISTING SUPPORTS ARE NOT SUITABLE FOR RELOCATION, THE CONTRACTOR SHALL PROVIDE A SIMILAR APPLICATION AT NO ADDITIONAL EXPENSE. THIS WORK SHALL BE INCLUDED IN THE COST OF "CLEARING AND GRUBBING".
14. ALL RELOCATED SIGNS SHALL BE INSTALLED ON NEW POSTS WITH NEW HARDWARE, UNLESS DETERMINED OTHERWISE BY THE ENGINEER IN THE FIELD.
15. IRON PINS OR MONUMENTS DISTURBED BY CONSTRUCTION SHALL BE RESET BY A CT LICENSED SURVEYOR, WHICH SHALL BE INCLUDED IN THE COST OF "CLEARING AND GRUBBING".
16. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION WITH APPROPRIATE UTILITY COMPANIES REGARDING RELOCATIONS AND/OR OF THEIR FACILITIES AND SCHEDULING OF SUCH WORK.
17. MATERIAL EXCAVATED WHEN TRENCHING FOR STORM, SANITARY OR UTILITY INSTALLATIONS SHALL BE APPROVED BY THE ENGINEER FOR USE AS BACKFILL. PRIOR TO PLACEMENT, THERE SHALL BE NO SEPARATE PAYMENT FOR IMPORTING AND PLACING SUITABLE BACKFILL IN ACCORDANCE WITH SECTION 2.05.03 OF FORM 817.
18. ALL WASTE MATERIALS (INCLUDING WASTEWATER) SHALL BE DISPOSED OF IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL LAW. LITTER AND DEBRIS SHALL BE PICKED UP AT THE END OF EACH WORK DAY.
19. ALL NEW AND RELOCATED UTILITY POLES TO BE INSTALLED A MINIMUM OF 18" BEHIND THE CURB LINE AND SHALL PROVIDE WHERE POSSIBLE 48" FOR PEDESTRIAN ACCESSIBILITY WHEN LOCATED IN A SIDEWALK.
20. ALL UTILITIES RELOCATING THEIR FACILITIES SHALL OBTAIN THEIR OWN CTDOT HIGHWAY ENCROACHMENT PERMIT.



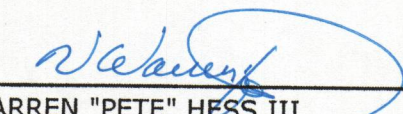
PROJECT SITE VICINITY MAP:



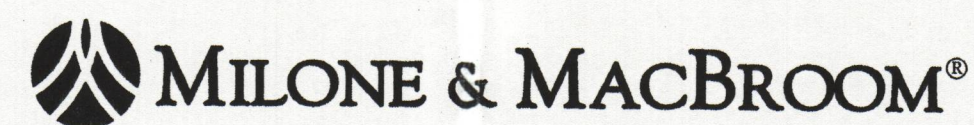
## LIST OF DRAWINGS

NO.	TITLE
01	TITLE SHEET
02	DETAILED ESTIMATE SHEET
03	INDEX PLAN & GENERAL NOTES
04	TYPICAL CROSS SECTIONS
05-10	MISCELLANEOUS DETAILS
11-17	EXISTING CONDITIONS & BASELINE LAYOUT PLANS
18-20	BORING LOGS
21-27	ROADWAY PLANS
28-35	ROADWAY PROFILES
36-37	INTERSECTION GRADING PLAN
38-44	DRAINAGE PLANS
45	TRAFFIC CONTROL SIGNAL PLAN TRAFFIC CONTROL SIGNAL-INTERSECTION NO.087-220 FIO
46-55	SIGNING AND PAVEMENT MARKING PLANS
56-59	MAINTENANCE AND PROTECTION OF TRAFFIC PLAN
60	DETOUR PLAN
61-89	CROSS SECTIONS
90-91	STRUCTURAL PLANS
	FRONTIER RELOCATION PLANS FIO CTDOT HIGHWAY STANDARDS INDEX SHEET CTDOT TRAFFIC STANDARDS INDEX SHEET

APPROVED BY:

  
 N. WARREN "PETE" HESS III  
 MAYOR  
 BOROUGH OF NAUGATUCK  
 DATE: 9/26/18

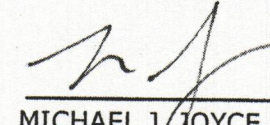
PREPARED BY:

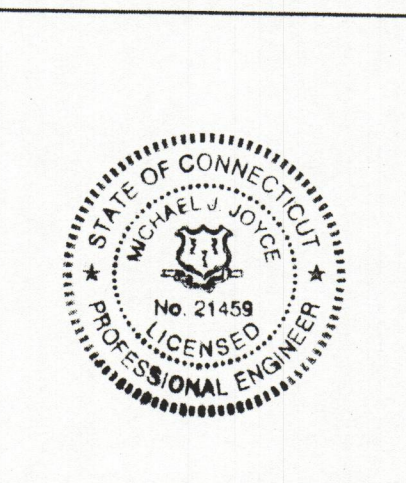


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DESIGNED BY:

  
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 MICHAEL J. JOYCE, P.E.  
 CONN. PROFESSIONAL REG. NO. 21459  
 DATE: 9/7/18



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STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION,  
 "STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND  
 INCIDENTAL CONSTRUCTION," FORM 817, GOVERN INCLUDING  
 SUPPLEMENTS THERETO DATED JANUARY 2018.

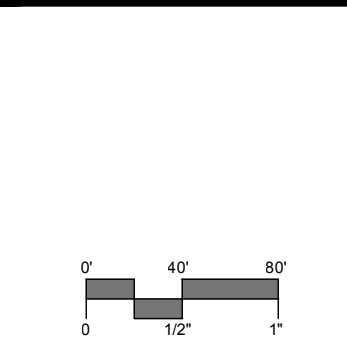
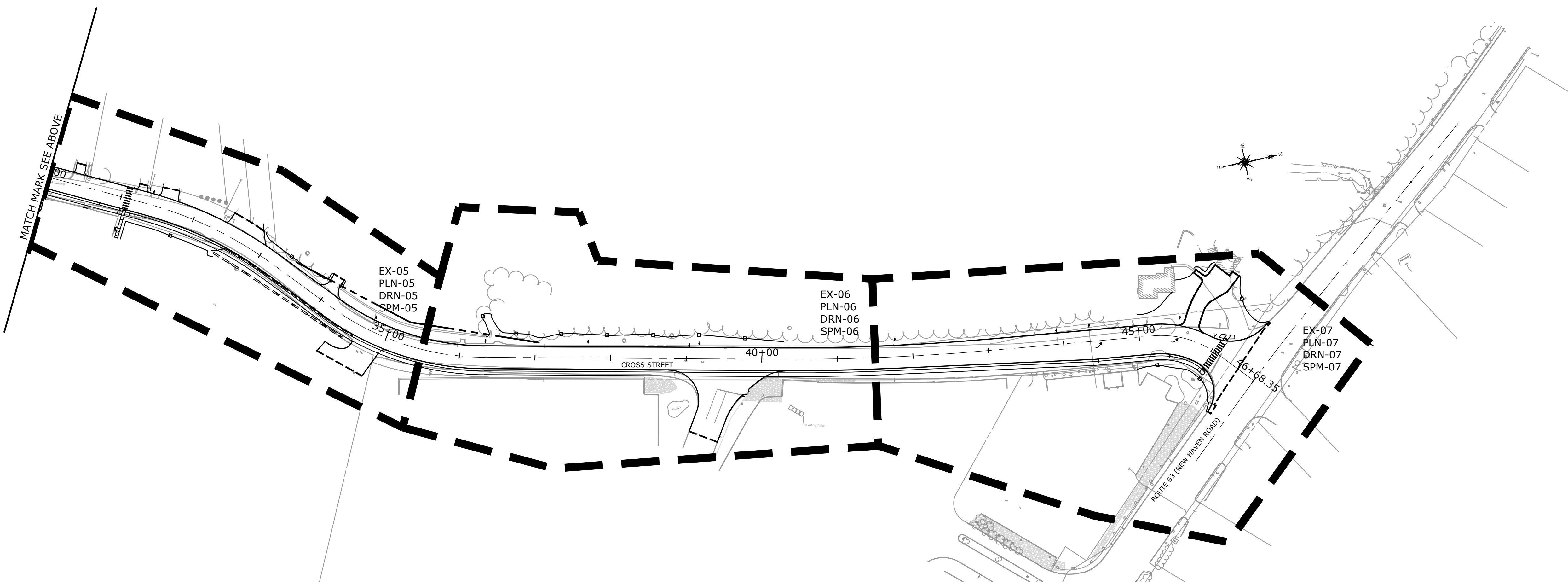
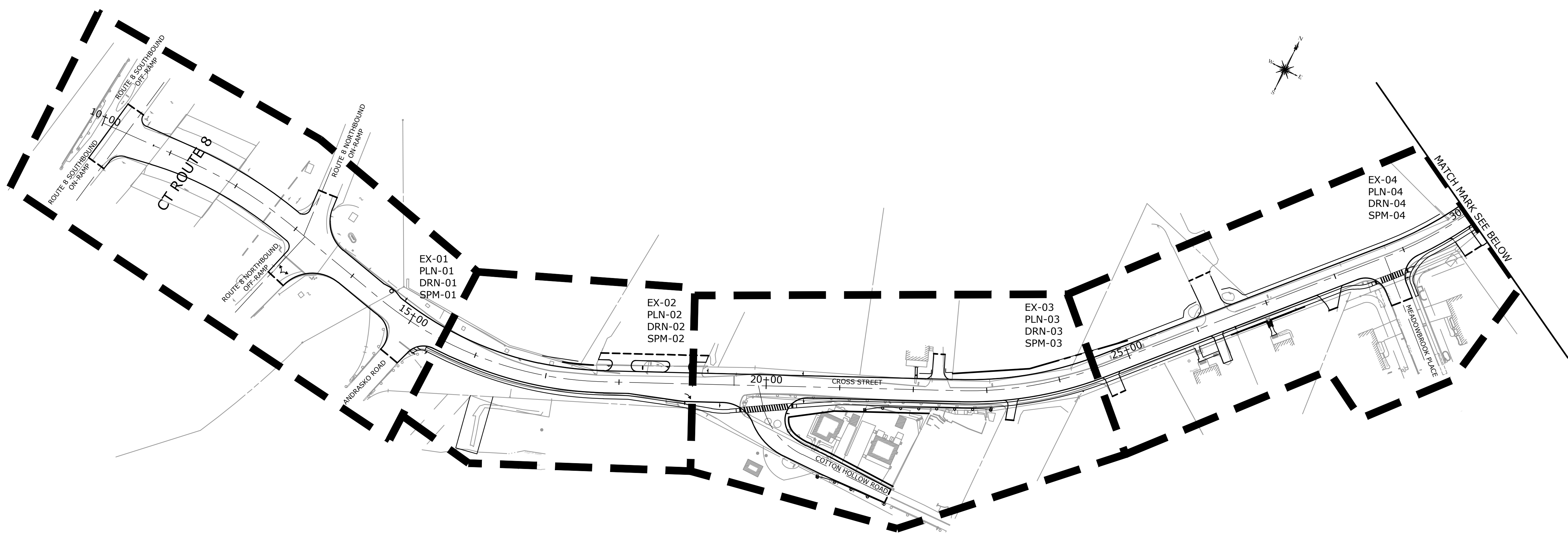
**CROSS STREET**  
 DESIGN SPEED: 35 MPH  
 PROJECT LENGTH: 3,629.73 LF  
 BEGIN STATION: 10+12.57'  
 END STATION: 46+42.30'







SHEET 3 OF 3 - RECONSTRUCTION OF CROSS STREET - NAUGATUCK, CONNECTICUT  
 DRAWN BY: M. J. JACOBSON  
 CHECKED BY: M. J. JACOBSON  
 DATE: 09/07/2018



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DESCRIPTION	DATE	BY

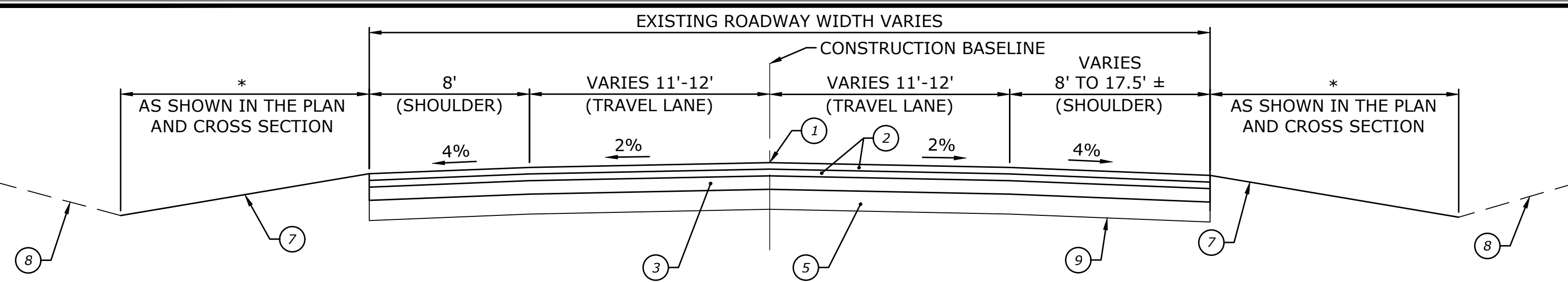
**INDEX PLAN AND GENERAL NOTES**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=80'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. IN-1		

**03**  
SHEET NO.

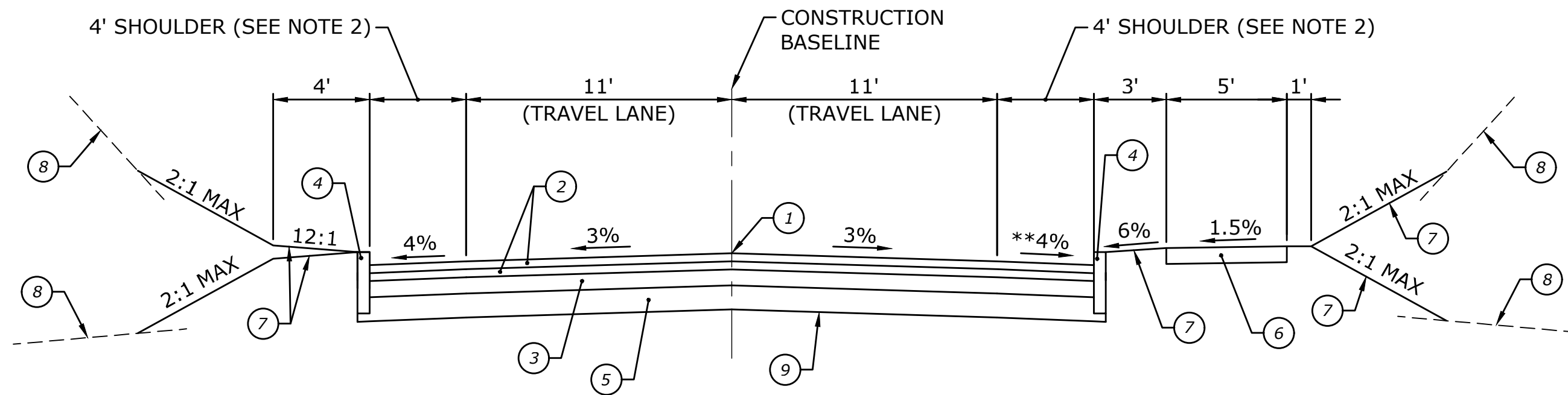


MILONE & MACBROOM, INC. 2015



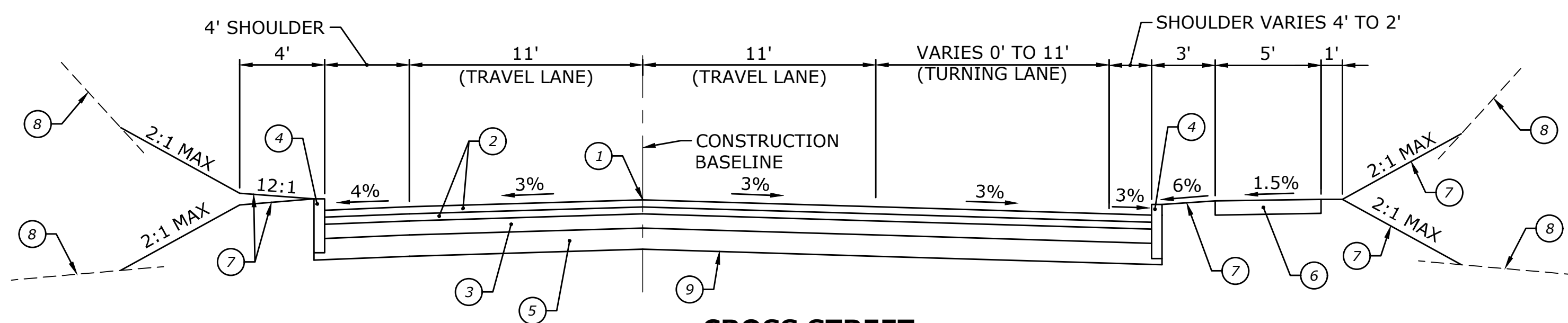
**CROSS STREET**  
STA 10+56 TO STA 14+57  
(SEE NOTE 1)

\* REMOVE EXISTING CONCRETE BLOCKS UNDER THE ROUTE-8 OVERPASS (TO BE PAID FOR UNDER "CLEARING AND GRUBBING") AND REPLACE WITH CRUSHED STONE (SEE DETAIL ON THIS SHEET)

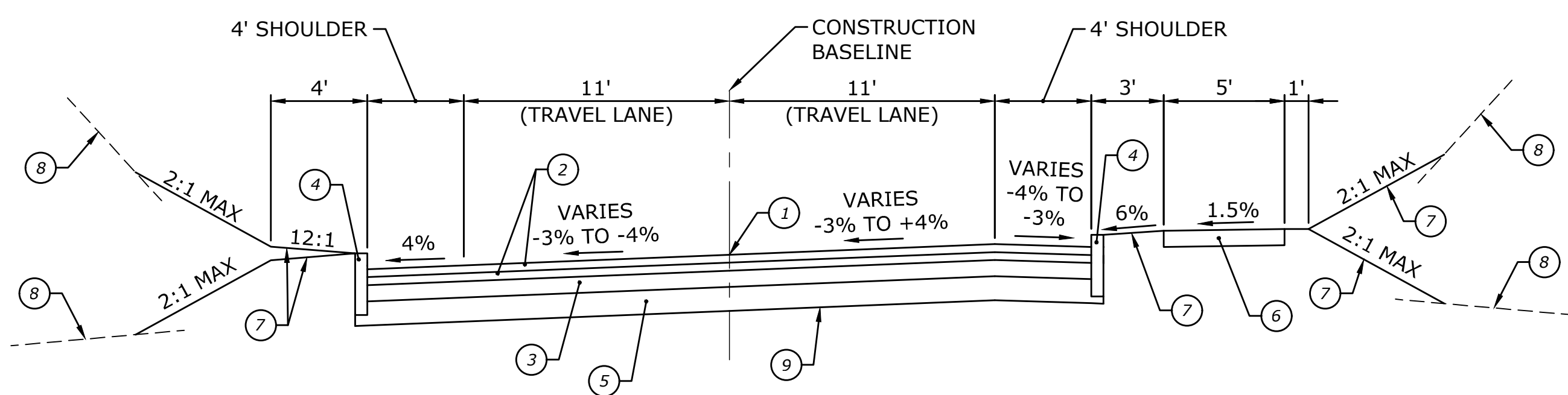


**CROSS STREET**  
STA 15+45 TO STA 18+13  
COTTON HOLLOW ROAD TO STA 32+50  
STA 37+50 TO STA 41+49

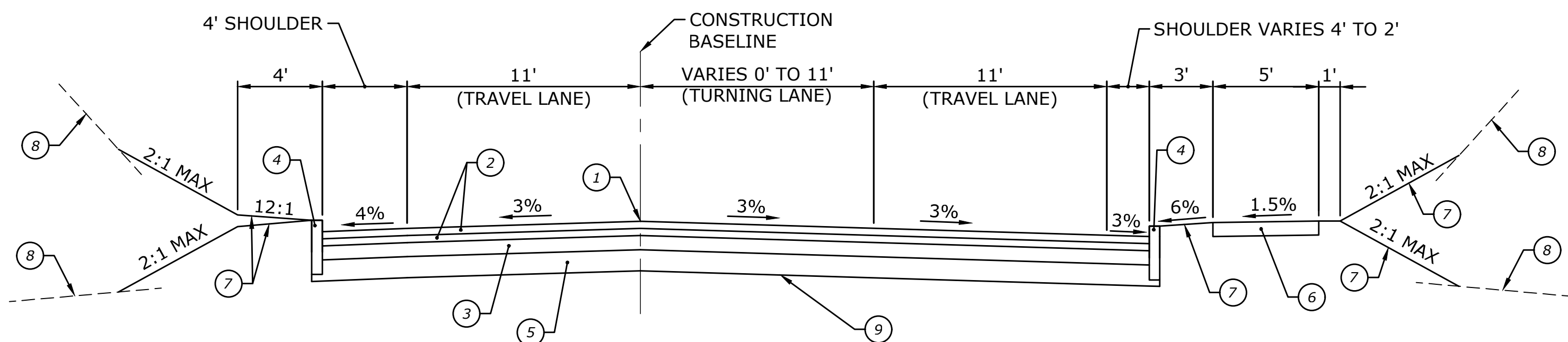
\*\* CROSS SLOPE VARIES FROM 4% TO 3% BETWEEN STA. 17+90 TO STA. 18+13



**CROSS STREET**  
STA 18+13 TO COTTON HOLLOW ROAD



**CROSS STREET - SUPER ELEVATED SECTION**  
STA 32+50 TO STA 37+50



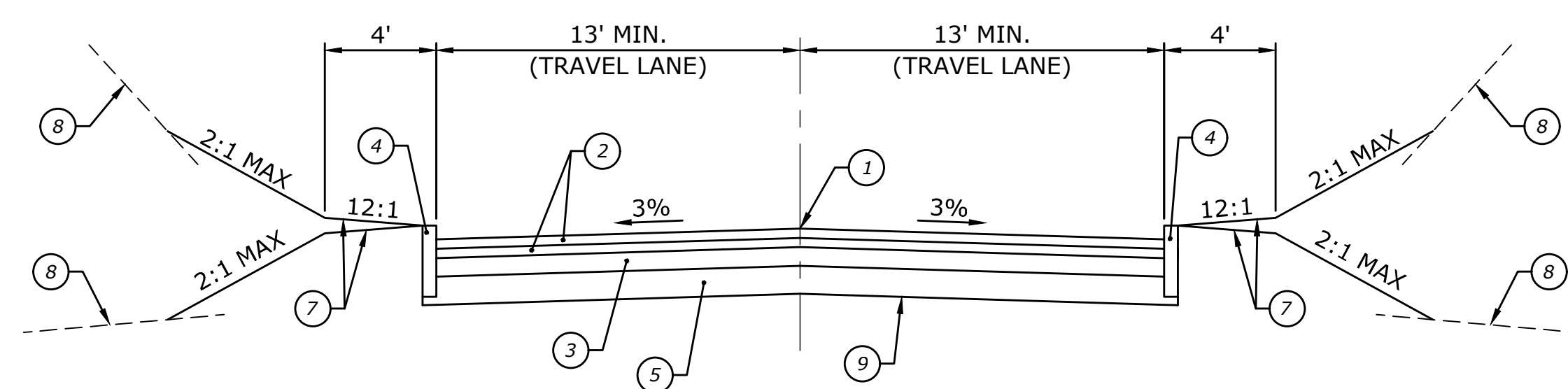
**CROSS STREET**  
STA 41+49 TO ROUTE 63

**NOTES**

- SEE ROADWAY PLANS FOR LOCATION OF PROPOSED CURBING.
- SHOULDER WIDTH VARIES FROM 8' TO 4' FROM STA 14+57 THROUGH INTERSECTION WITH ANDRASKO ROAD TO STA. 18+13.
- CROSS SLOPE OF PAVEMENT TRANSITION FROM STA. 14+50 (2%) THROUGH INTERSECTION WITH ANDRASKO ROAD TO STA. 15+50 (3%), SEE INTERSECTION GRADING PLAN FOR ADDITIONAL INTERSECTIONS.
- FOR ROADWAY WIDTH BETWEEN STA. 14+57 TO STA. 15+45, SEE ROADWAY PLANS.
- NO TYPICAL CROSS SECTIONS ARE PROVIDED FOR THE FOLLOWING LOCATIONS:  
-ROUTE 8 SOUTHBOUND ON AND OFF RAMP TO CROSS STREET STA. 10+70±  
-ROUTE 8 NORTHBOUND ON AND OFF RAMP TO CROSS STREET  
-ANDRASKO ROAD AT CROSS STREET  
-MEADOW BROOK PLACE AT CROSS STREET
- PAVEMENT STRUCTURE SHALL BE IN ACCORDANCE WITH "CROSS STREET" TYPICAL CROSS SECTIONS. USE ROADWAY PLANS FOR DIMENSIONS AND INTERSECTION GRADING PLANS FOR PROPOSED GRADES.

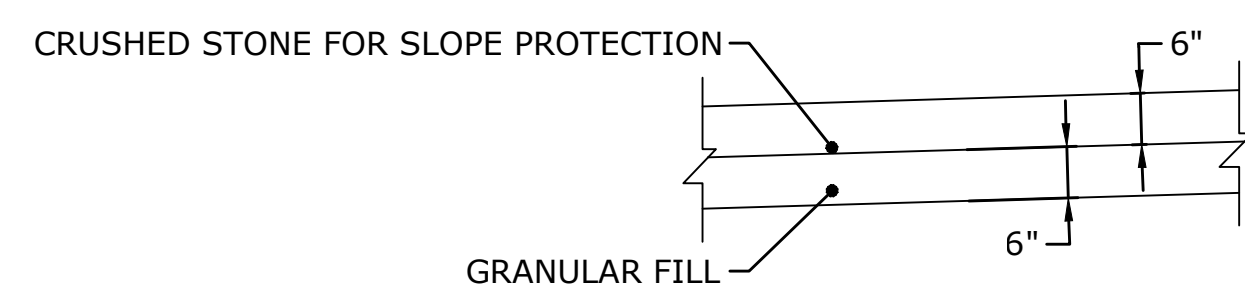
**LEGEND**

- ① POINT OF APPLICATION OF GRADE
- ② 2" HMA S0.5 (DESIGN LEVEL 2)
- ③ 3" HMA S1.0 (DESIGN LEVEL 2)
- ④ CONCRETE CURBING
- ⑤ 12" SUBBASE
- ⑥ CONCRETE SIDEWALK
- ⑦ 4" TOP SOIL & TURF ESTABLISHMENT
- ⑧ EXISTING GRADE
- ⑨ FORMATION OF SUBGRADE



**COTTON HOLLOW ROAD**  
CROSS STREET TO STA 92+45

NOTE: ROADWAY WIDTH VARIES FROM 26' AT STA. 91+50 TO EXISTING AT STA. 92+45



**SECTION THROUGH CRUSHED STONE FOR SLOPE PROTECTION**  
NOT TO SCALE

DESCRIPTION	DATE	BY

TYPICAL CROSS SECTIONS  
RECONSTRUCTION OF CROSS STREET  
NAUGATUCK, CONNECTICUT

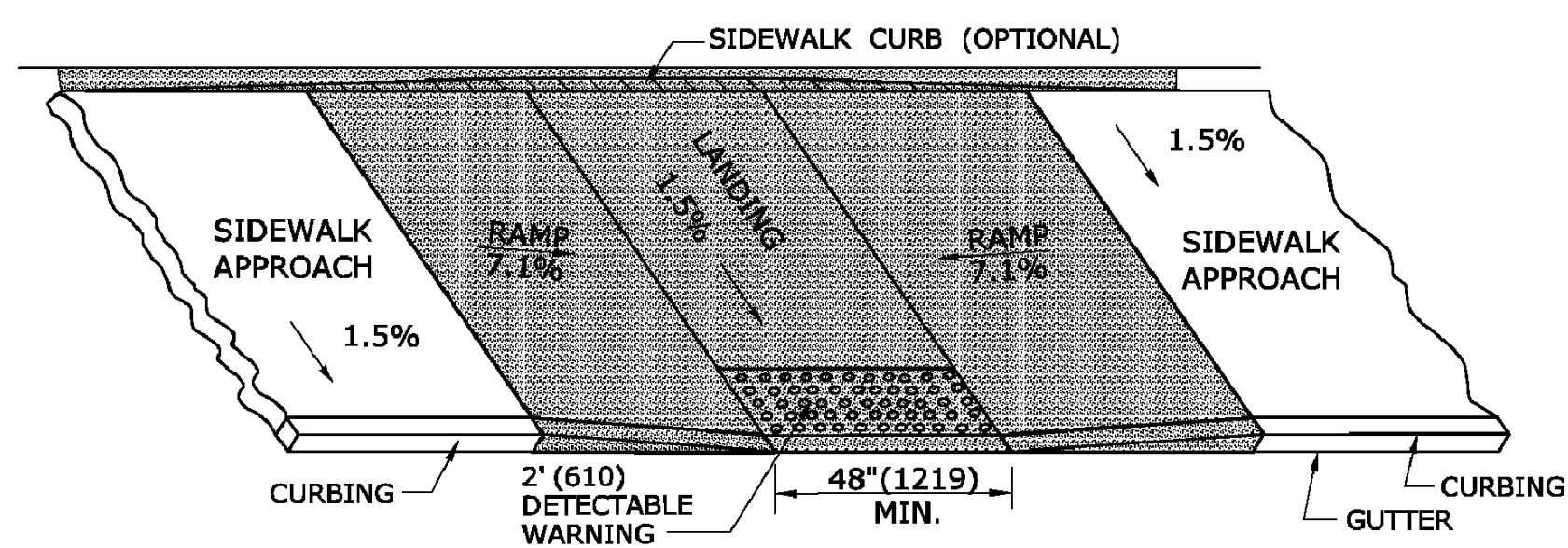
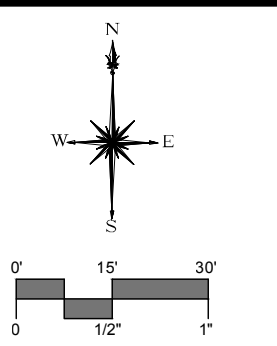
DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE NOT TO SCALE		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. TYP-01		

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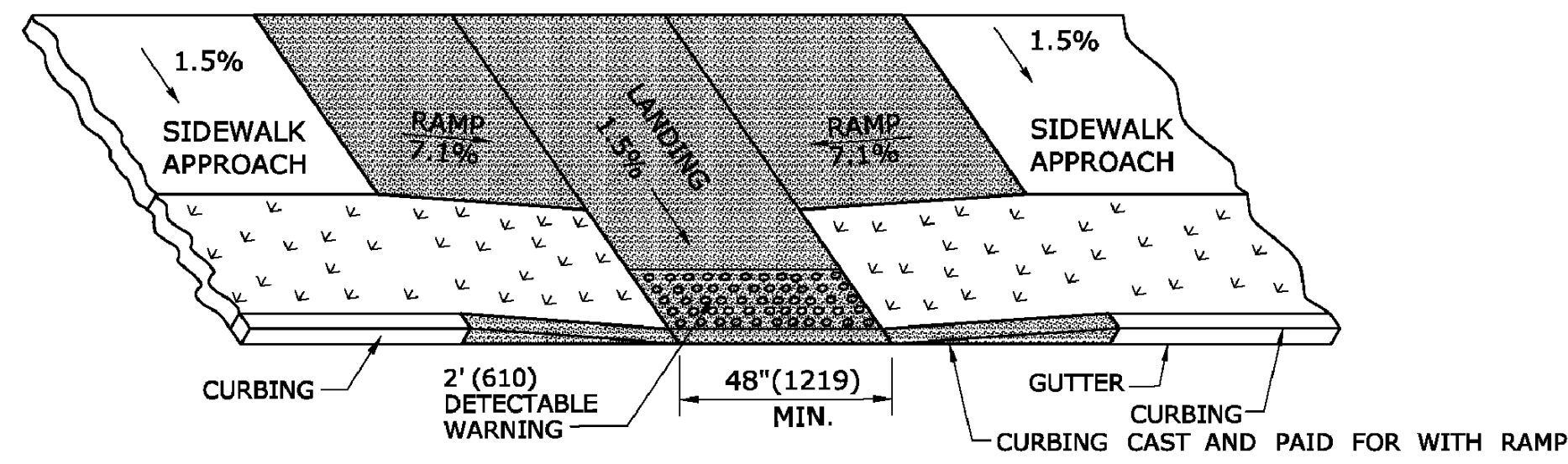




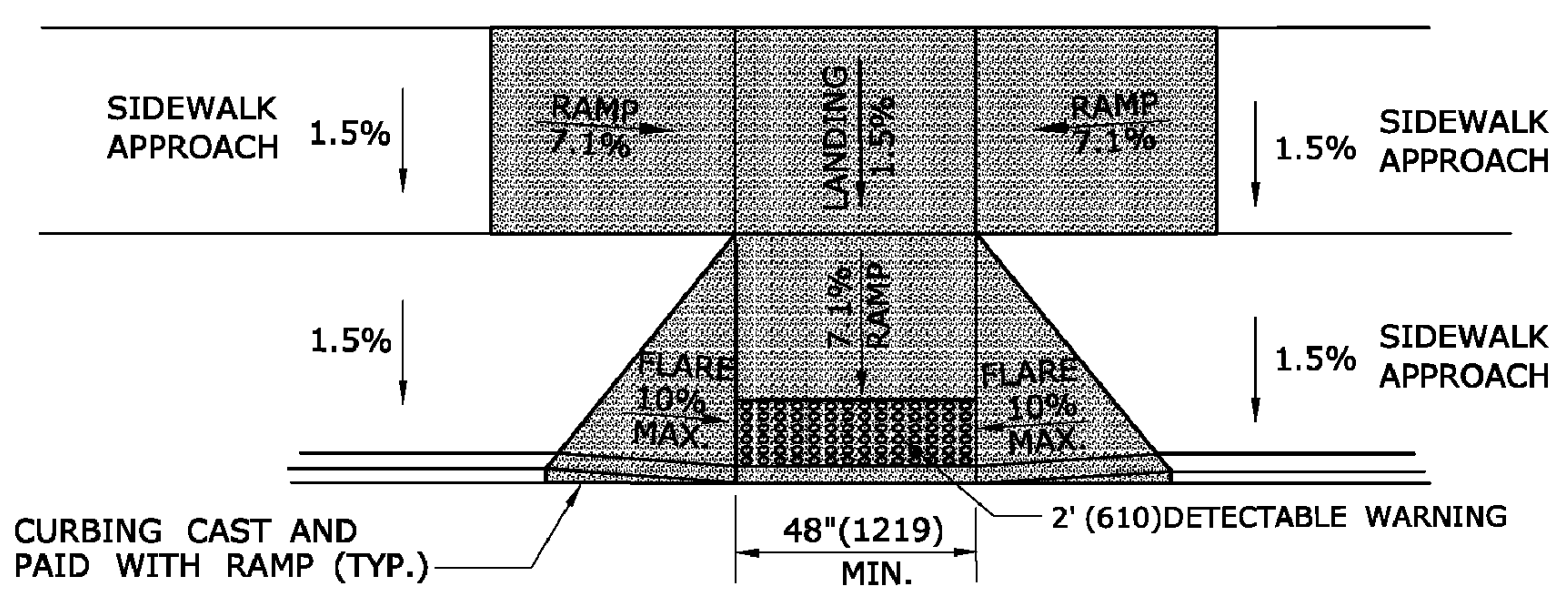




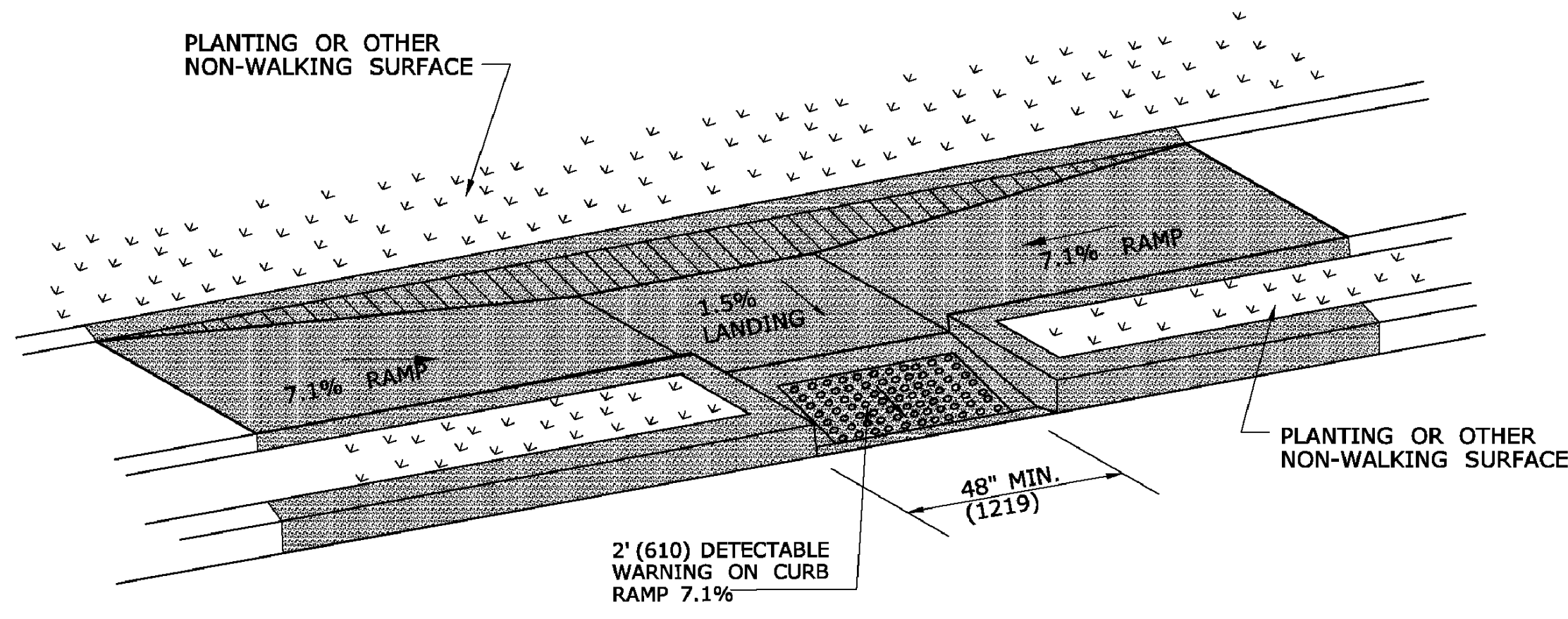
**PARALLEL SIDEWALK RAMP (TYPE 1) NO UTILITY STRIP**



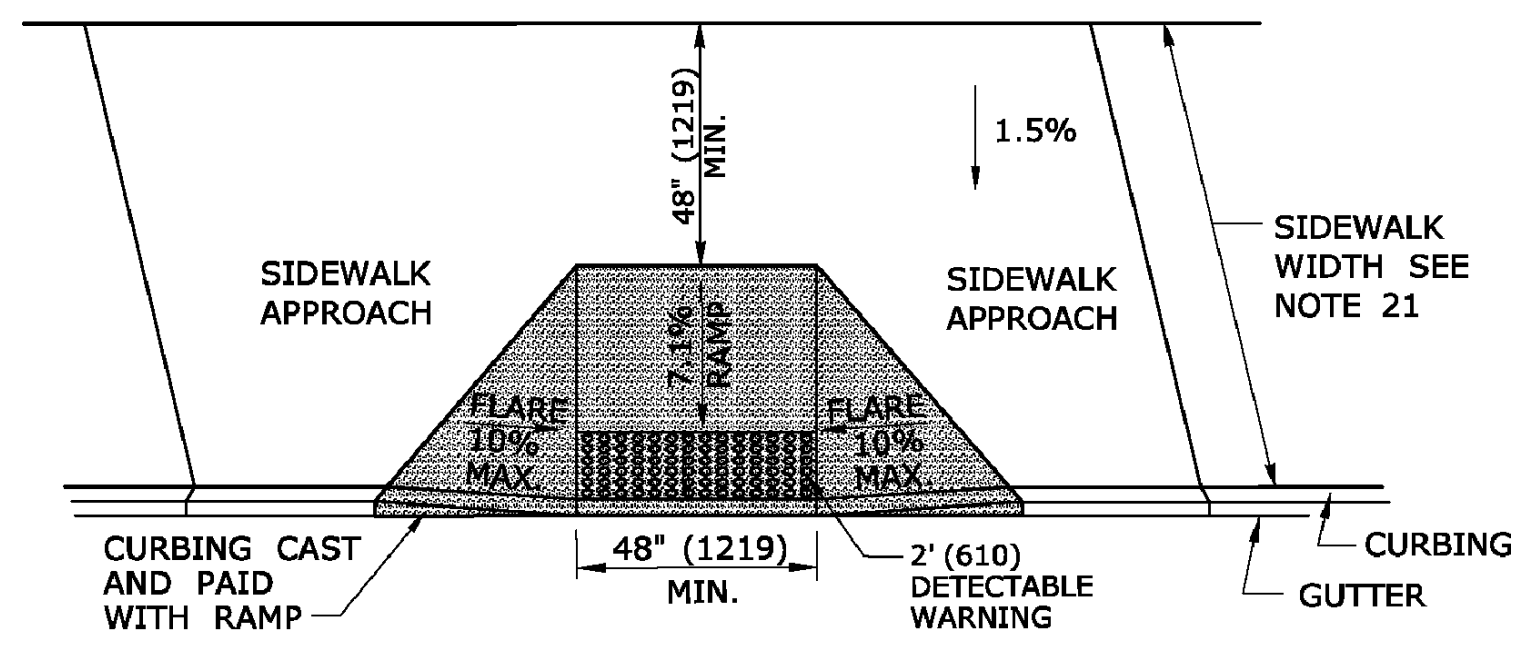
**PARALLEL SIDEWALK RAMP (TYPE 1a) WITH UTILITY / GRASS STRIP**



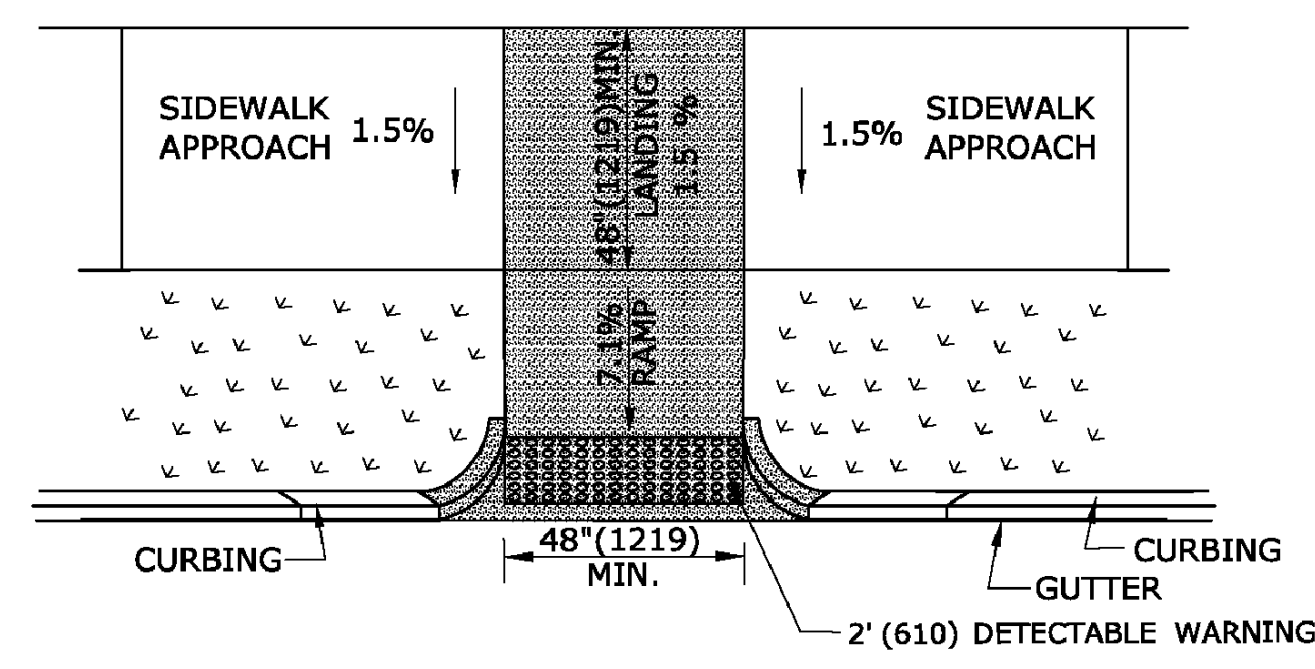
**PARALLEL/PERPENDICULAR SIDEWALK RAMP NO UTILITY/GRASS STRIP (TYPE 1b)**



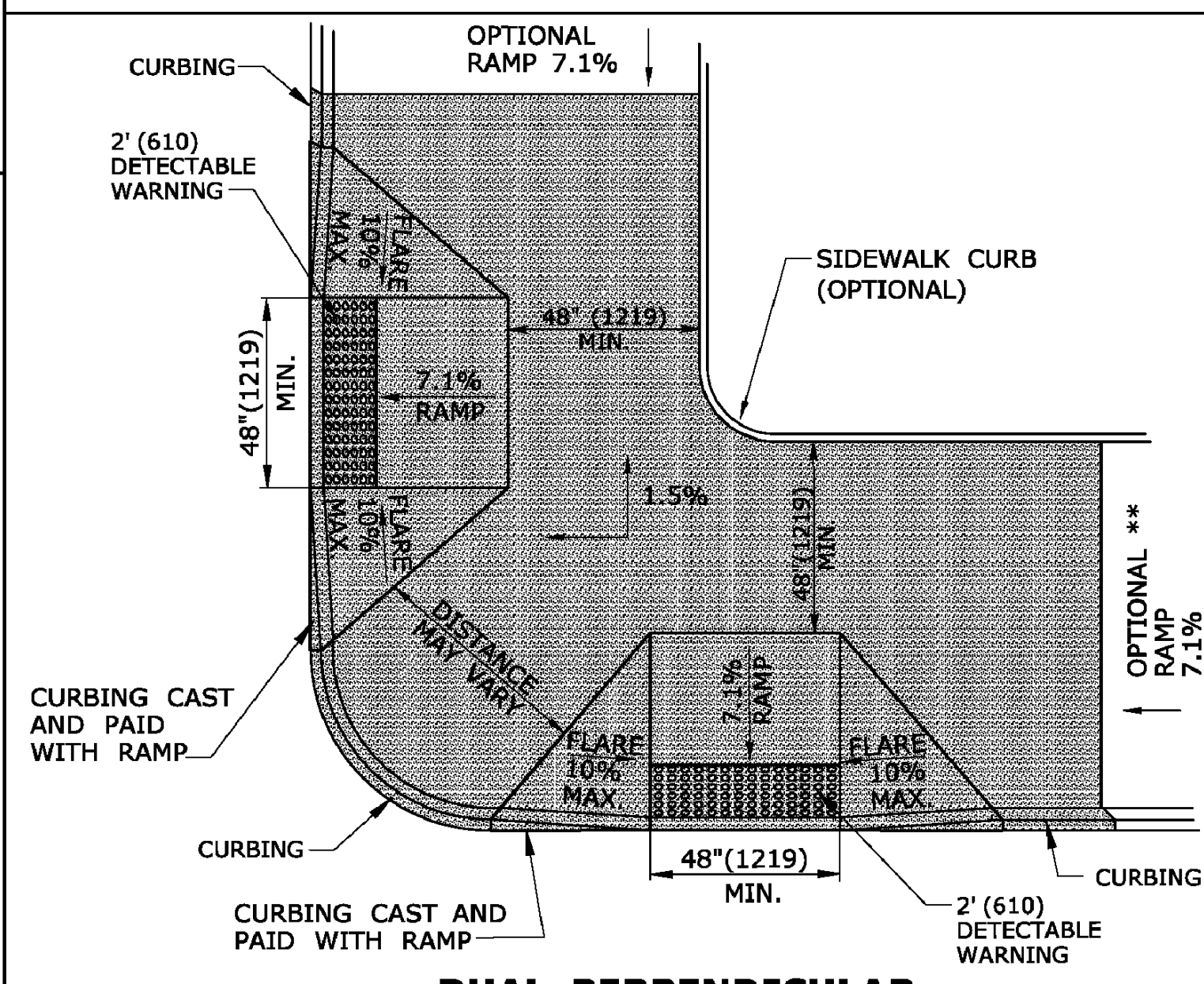
**PARALLEL SIDEWALK RAMP (TYPE 1c) WITH UTILITY / GRASS STRIP**



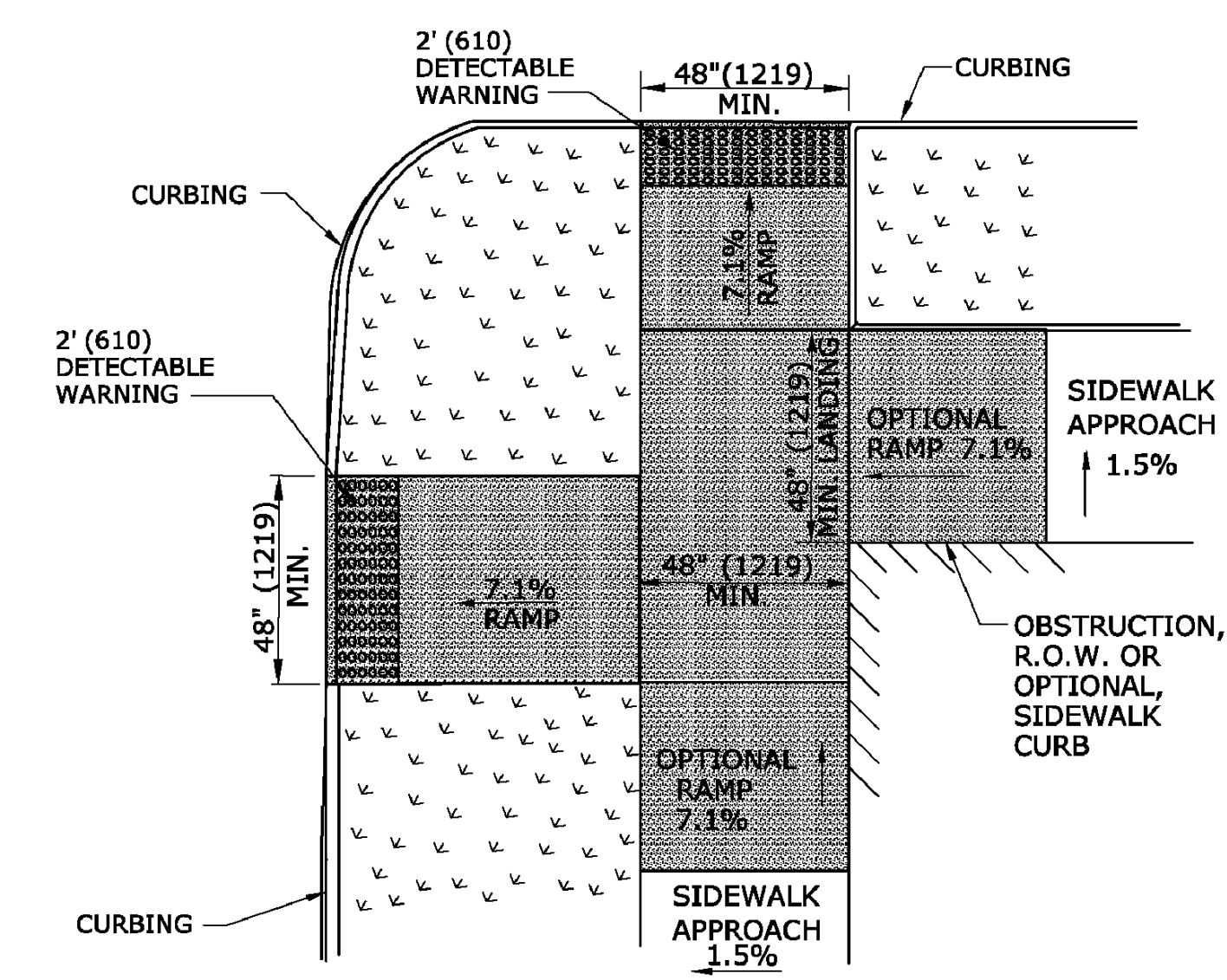
**PERPENDICULAR SIDEWALK RAMP W/ 48" (1219) MIN. BY PASS LANDING (TYPE 2)**



**PERPENDICULAR SIDEWALK RAMP W/CURB RETURNS / UTILITY GRASS STRIP (TYPE 2a)**  
\* OPTIONAL FLARE ONE SIDE OF RAMP



**DUAL PERPENDICULAR SIDEWALK RAMP (TYPE 3)**  
SEE NOTES 19  
\* OPTIONAL CURB RETURN ON ONE SIDE OF RAMP  
\*\* SEE NOTE 23



**DUAL PERPENDICULAR SIDEWALK RAMP (TYPE 3a) WITH UTILITY / GRASS STRIP**  
SEE NOTE 20

**GENERAL NOTES:**

1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP SHOULD NOT EXCEED 5%. THE MAXIMUM GRADE DIFFERENCE BETWEEN THE GUTTER AND CURB RAMP SHALL NOT EXCEED 13%. SEE DETAIL 1 ON SHEET 4.
2. RAMP GRADE SHALL BE UNIFORM, FREE OF SAGS AND ABRUPT GRADE CHANGES. RUNNING SLOPES OF RAMP SHALL NOT EXCEED 8.33% AND SHALL NOT EXCEED 15' (4.5m) WITHOUT PROVIDING A LANDING.
3. ALL RAMP SHALL BE CONSTRUCTED OF CLASS "F" CONCRETE IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
4. SIDEWALK RAMP SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE OF ALL SIDEWALK RAMP SHALL BE STABLE, FIRM AND SLIP RESISTANT. SURFACE DISCONTINUITIES SHALL NOT EXCEED 1/2" (13) MAX. VERTICAL DISCONTINUITIES BETWEEN 1/4" (6.4) AND 1/2" (13) MAX. SHALL BE BEVELED 1:2 MINIMUM APPLIED ACROSS THE ENTIRE LEVEL CHANGE.
5. DIAGONAL SIDEWALK RAMP AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES. DIAGONAL AND PERPENDICULAR RAMP SHALL HAVE THE RAMP CUT PERPENDICULAR TO THE TANGENT OF THE CURB RADIUS FOR THE DESIGNATED ACCESSIBLE ROUTE. BOTH LONGITUDINAL SIDES OF THE RAMP SHOULD BE THE SAME LENGTH. SKEWED RAMP SHOULD BE AVOIDED. FLARES ARE NOT CONSIDERED PART OF PEDESTRIAN ACCESS ROUTE. DIAGONAL RAMP SHOULD NOT BE INSTALLED WHERE CURB RADIUS IS LESS THAN 20'(6096).
6. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT. 8.3% MAXIMUM SLOPE MAY NOT BE ACHIEVABLE DUE TO EXISTING SIDEWALK GRADE. IN RECOGNITION OF THIS, A LIMIT OF 15' (4572) FOR REMOVAL SHALL BE USED UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF "CONCRETE SIDEWALK RAMP" OR "CONCRETE SIDEWALK".
7. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' (3658) UNLESS OTHERWISE NOTED.
8. CONCRETE SIDEWALK RAMP, SHALL BE PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", AS DEFINED BY THE CONSTRUCTION LIMITS ON THE PLANS AND SHALL BE FIELD VERIFIED.
9. SIDEWALK RAMP SHALL BE CONSTRUCTED WITH THE TOE AT THE GUTTER CAST INTEGRALLY WITH RAMP UNLESS DIRECTED OTHERWISE BY THE ENGINEER (SEE TYPICAL SECTION ON SHEET 3). CURB REMOVAL AND CAST IN PLACE CURBING REQUIRED FOR THE RAMP, SHALL BE INCLUDED WITH PAY ITEM "CONCRETE SIDEWALK RAMP". CURBING OUTSIDE LIMITS OF RAMP OR LANDING SHOWN ON SHEET 3 SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
10. PREFERRED LOCATION TO INSTALL DETECTABLE WARNING STRIP SHALL BE 6" (152) FROM THE EDGE OF ROAD ALONG THE FULL WIDTH OF THE RAMP. FOR ALTERNATE LOCATIONS, REFER TO DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 4.
11. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF RUNNING SLOPE (PERPENDICULAR TO CURB OR SLOPE BREAK). THE TRANSITION FROM RAMP TO GUTTER SHALL BE FLUSH WITHOUT A LIP.
12. WHERE COMMERCIAL DRIVEWAYS ARE PROVIDED WITH TRAFFIC SIGNALS AND THE SIDEWALK IS CONTINUOUS THROUGH DRIVEWAY, DETECTABLE WARNINGS ARE REQUIRED AT THE JUNCTION BETWEEN THE PEDESTRIAN ROUTE AND DRIVEWAY.
13. CONSTRUCT A SIDEWALK CURB WHEN THERE IS INSUFFICIENT BUFFER AVAILABLE TO GRADE OR WHEN CALLED FOR IN PLANS. PAID FOR WITH SIDEWALK RAMP WHEN REQUIRED FOR RAMP.
14. THE TOP AND BOTTOM OF RAMP SHOULD BE PROVIDED WITH A 4' x 4' (1219 x 1219) MINIMUM LEVEL LANDING AREA WITH A CROSS SLOPE LESS THAN OR EQUAL TO 2% IN ANY DIRECTION.
15. UTILITY POLES, LUMINAIRE, PEDESTRIAN OR SIGNAL POLES, GRATES, ACCESS COVERS, AND OTHER APPURTENANCES SHALL NOT BE LOCATED ON RAMP, LANDINGS, BLENDED TRANSITIONS, AND @ GUTTERS WITHIN THE PEDESTRIAN ACCESS ROUTE.
16. APPROACH SIDEWALK WIDTHS, GRASS STRIP OR UTILITY STRIP WIDTHS MAY VARY.
17. APPROACH SIDEWALK AND LANDING CROSS SLOPE SHALL NOT EXCEED 2%.
18. THE RUNNING OR CROSS SLOPES ON LANDINGS AT MID BLOCK CROSSING MAY BE WARPED TO MEET STREET OR HIGHWAY GRADE.
19. FOR PERPENDICULAR CURB RAMP A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE TOP OF CURB RAMP. WHERE THE LEVEL LANDING IS RESTRICTED AT THE BACK OF SIDEWALK THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN.
20. FOR PARALLEL CURB RAMP, A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE BOTTOM OF CURB RAMP. IF THE LEVEL LANDING IS RESTRICTED ON 2 OR MORE SIDES, THE LEVEL LANDING SHALL BE 4'(1.2m)x 5'(1.5m) WITH THE 5' (1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
21. WHEN WIDTH OF SIDEWALK IS >48" AND A PERPENDICULAR SIDEWALK RAMP IS INSTALLED, THE FLARED SIDES SHALL BE 10% MAX. IF WIDTH OF SIDEWALK IS <48" THE FLARED SIDES MUST NOT EXCEED 8.33% (12:1).
22. SHADED AREAS ARE TYPICAL PAY LIMITS FOR CONCRETE SIDEWALK RAMP BUT, MAY VARY AS DIRECTED BY THE ENGINEER.
23. OPTIONAL RAMP, WHEN REQUIRED, SHALL BE PAID FOR AS PART OF CONCRETE SIDEWALK RAMP.

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

DESCRIPTION	DATE	BY

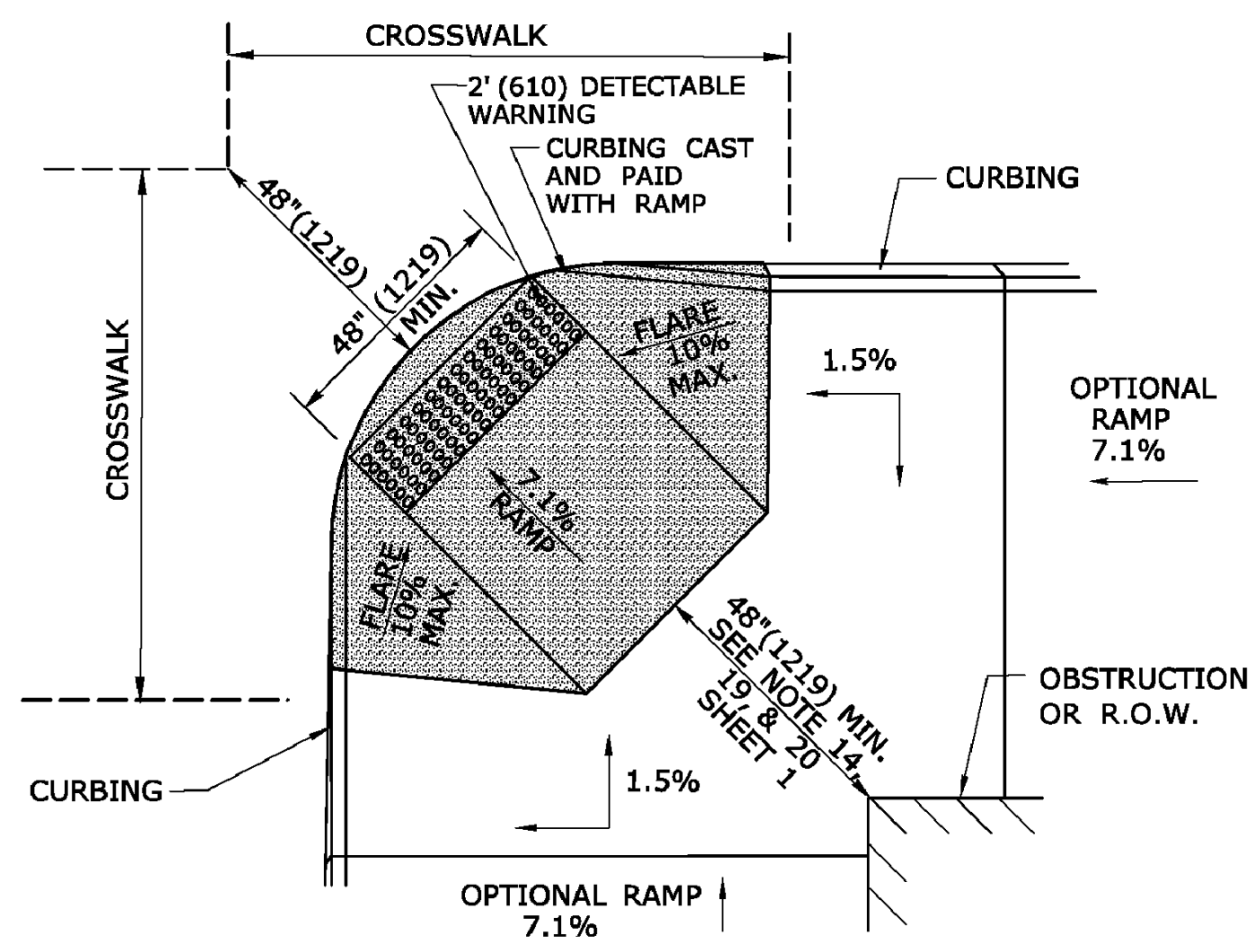
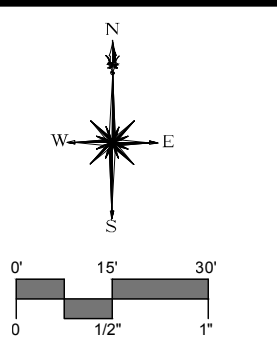
MISCELLANEOUS DETAILS-SIDEWALK RAMPS SHEET 1  
RECONSTRUCTION OF CROSS STREET  
NAUGATUCK, CONNECTICUT

TDS DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE N.T.S.		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. MDS-02		
SHEET NO. 06		

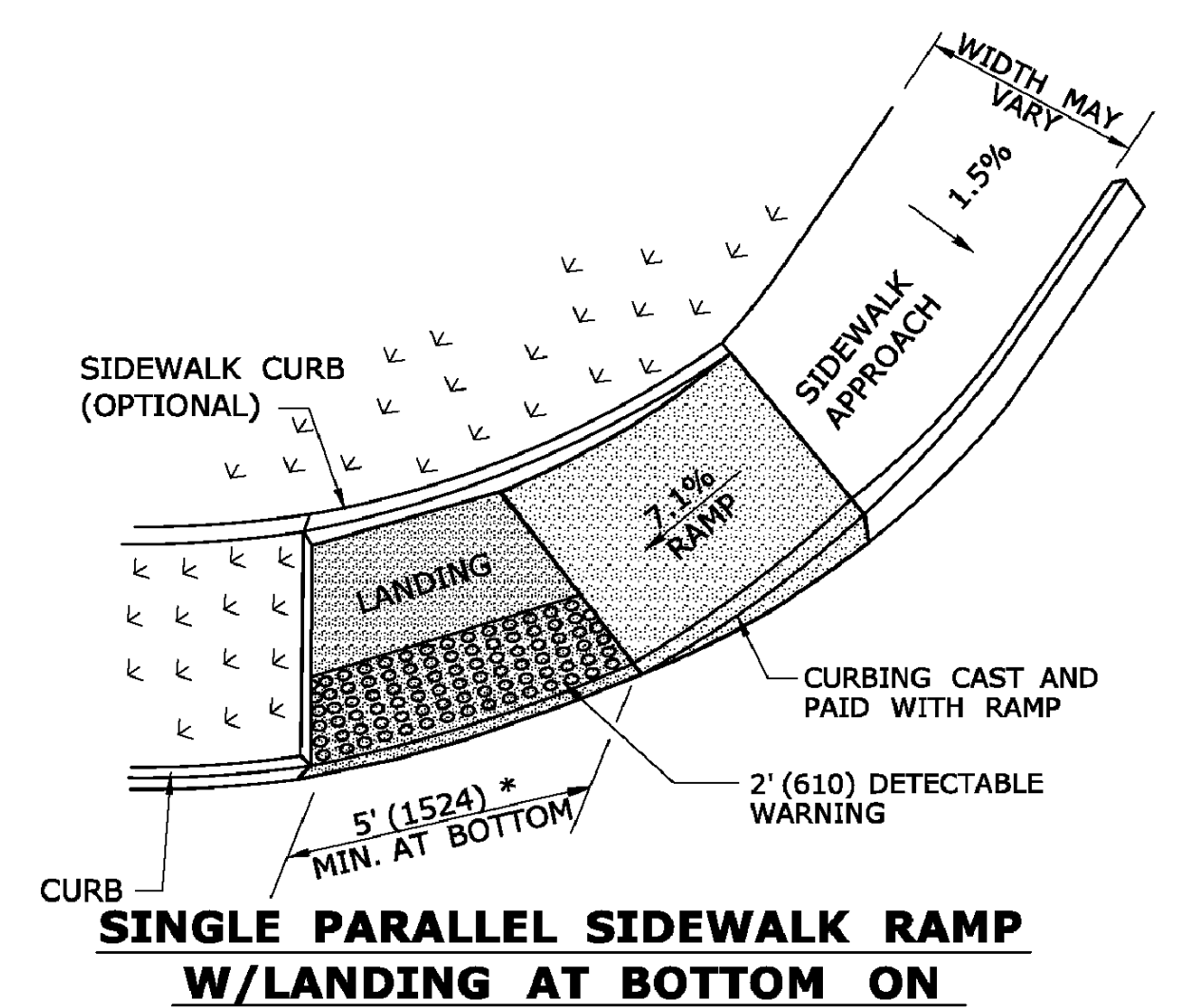
MILONE & MACBROOM, INC. 99 REALTY DRIVE, CHESHIRE, CONNECTICUT 06410 (203) 271-1773 FAX (203) 272-9733 WWW.MILONEANDMACBROOM.COM



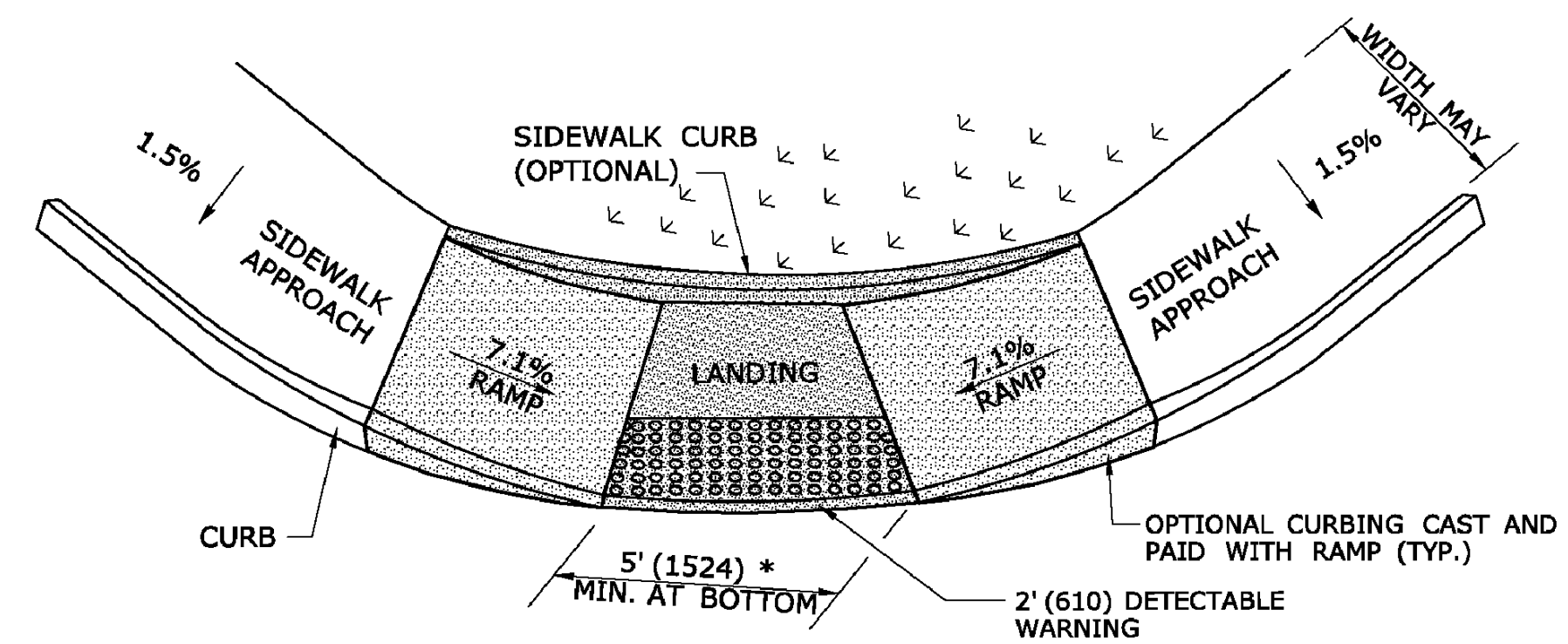
MILONE & MACBROOM, INC. 99 REACH DRIVE, CHESHIRE, CONNECTICUT 06410 (203) 271-1773 FAX (203) 272-9733 WWW.MILONEANDMACBROOM.COM



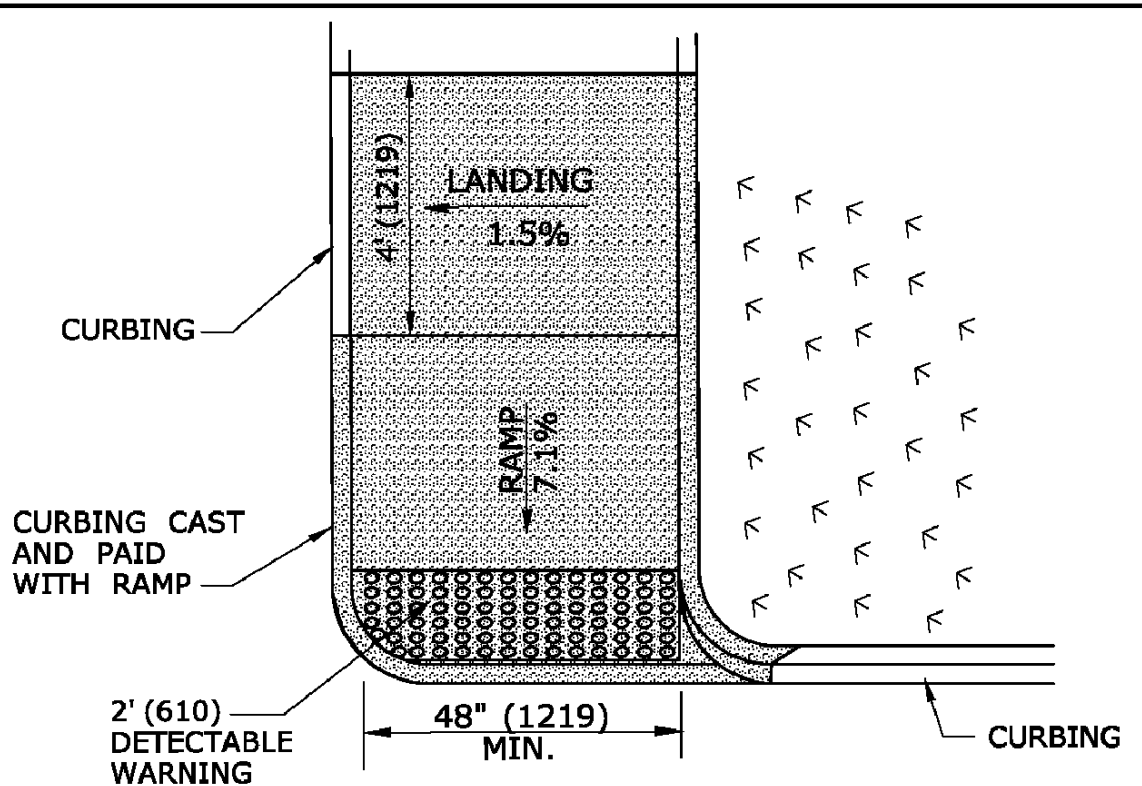
**DIAGONAL SIDEWALK RAMP (TYPE 4)  
W/LANDING AT TOP**



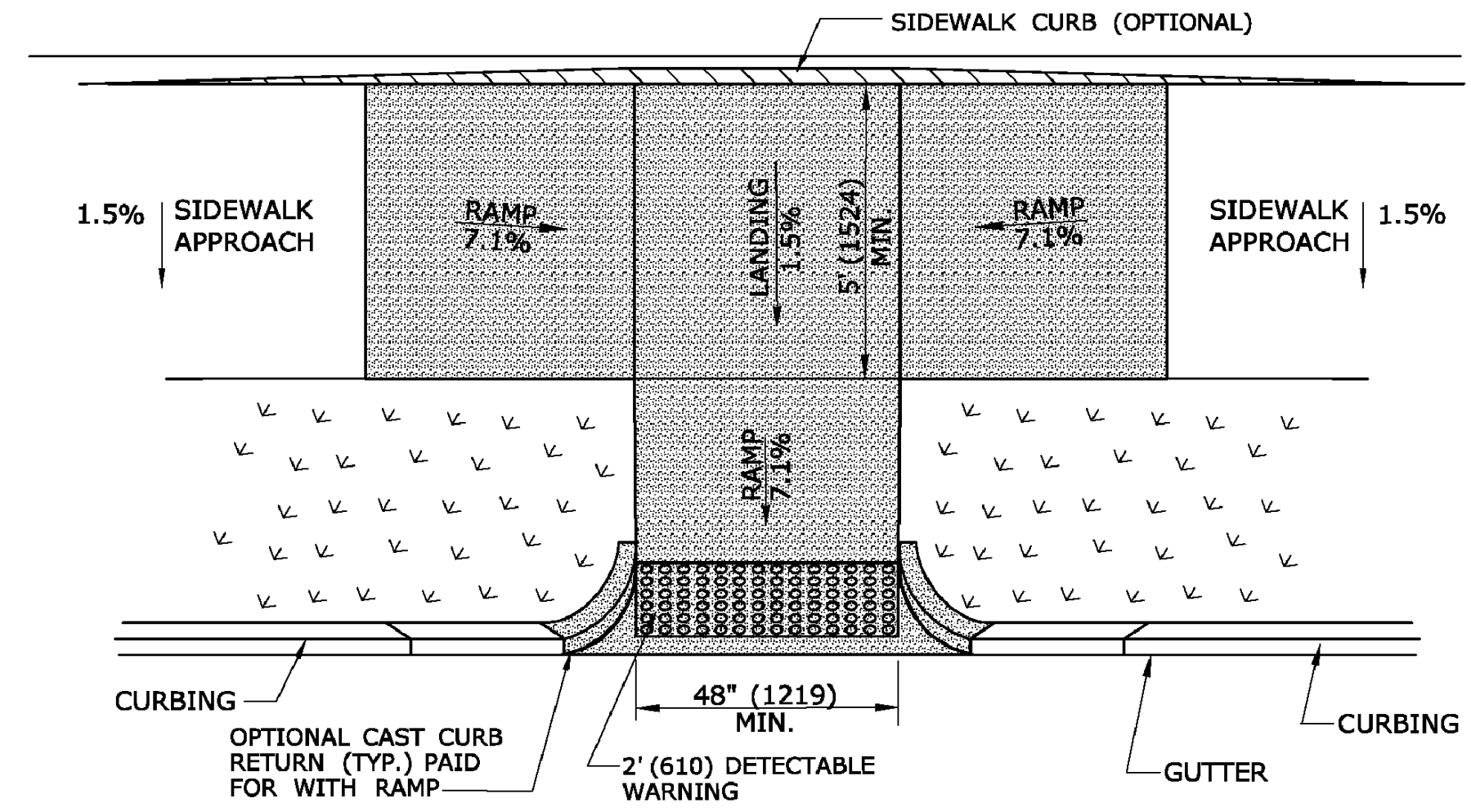
**SINGLE PARALLEL SIDEWALK RAMP  
W/LANDING AT BOTTOM ON  
CORNER (TYPE 4c)**  
\* SEE NOTE 20 SHEET 1



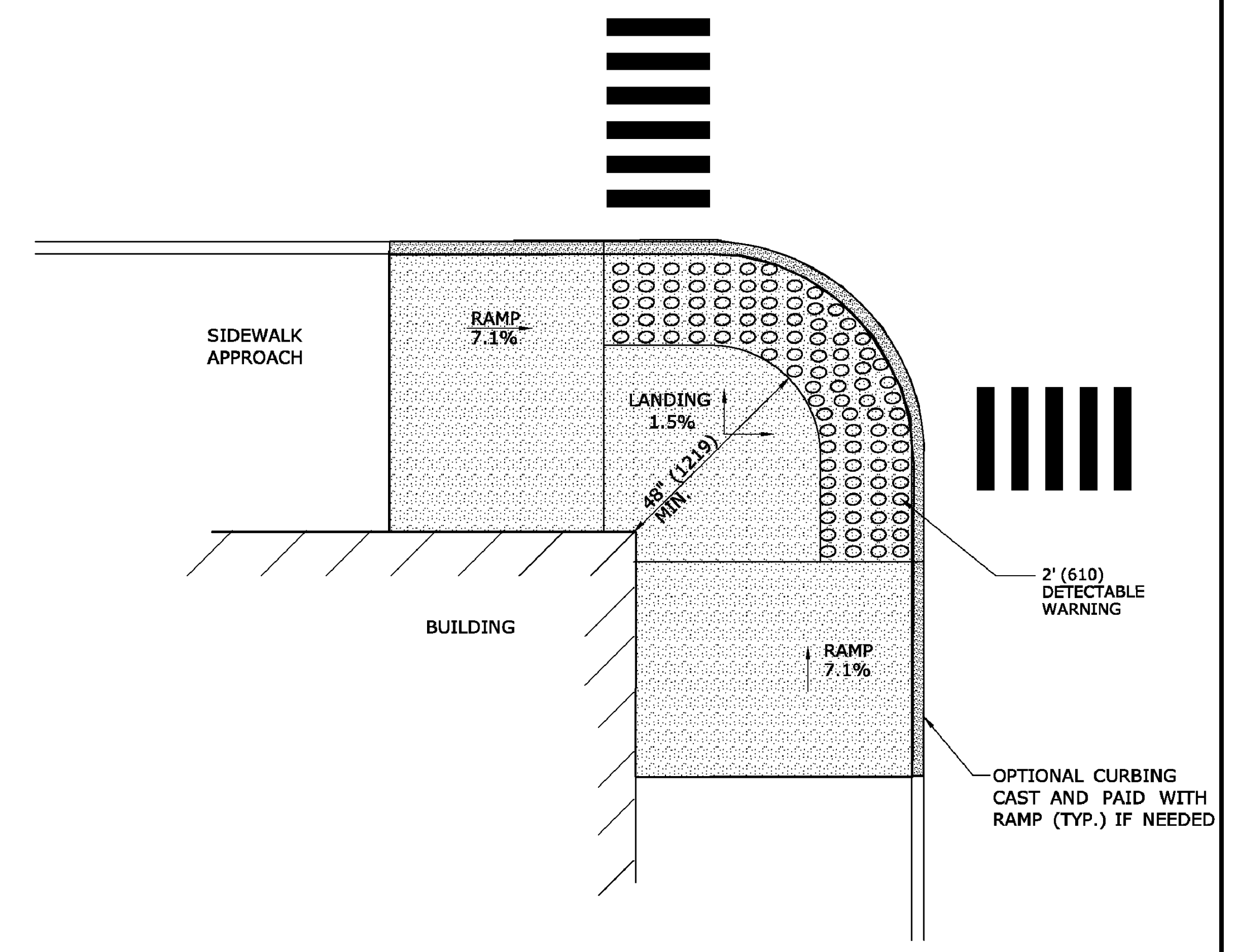
**DOUBLE PARALLEL SIDEWALK RAMP  
W/LANDING AT BOTTOM ON CORNER (TYPE 4f)**  
\* SEE NOTE 20 SHEET 1



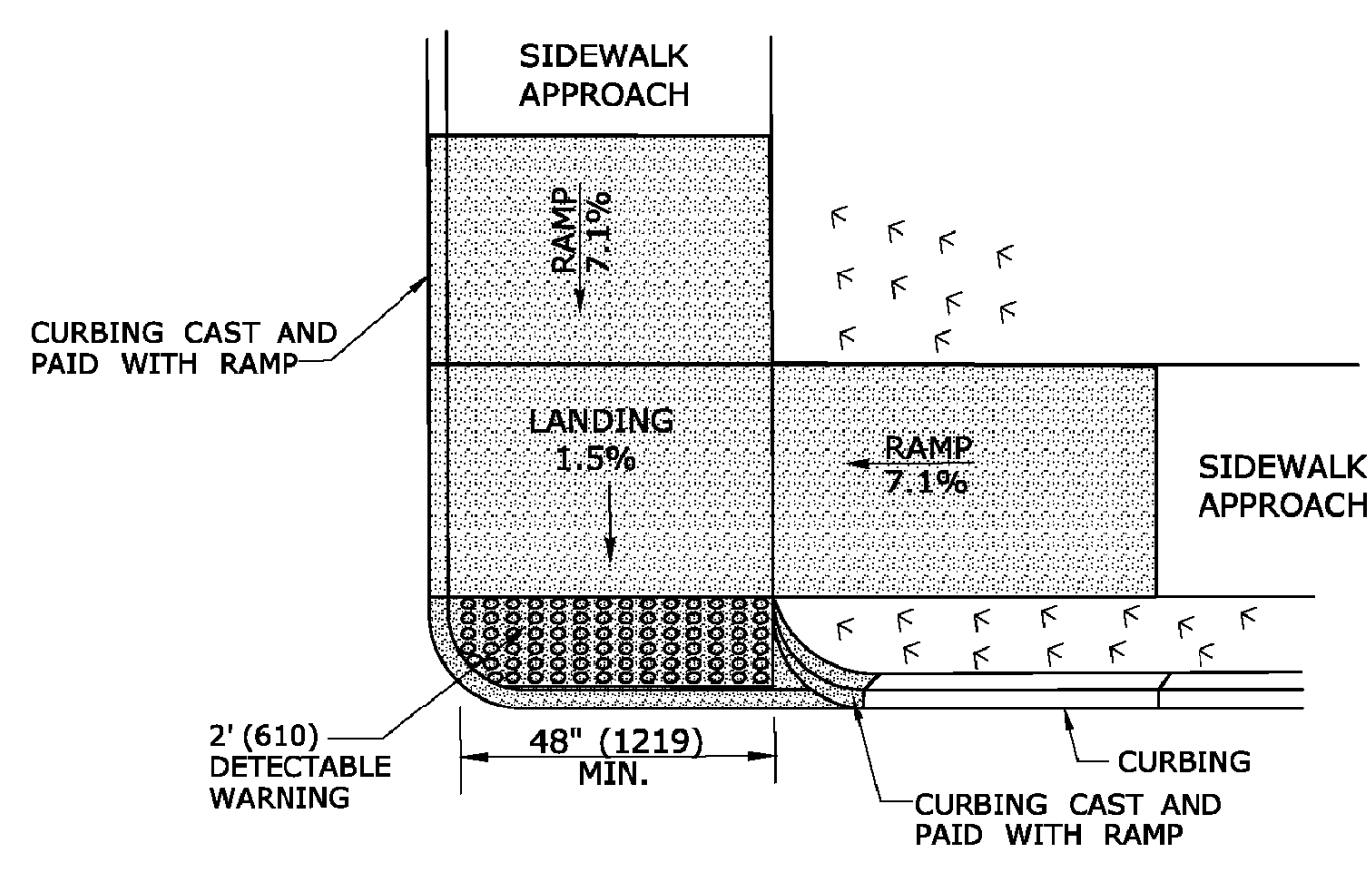
**SINGLE DIRECTION  
PERPENDICULAR SIDEWALK RAMP  
NO / UTILITY GRASS STRIP  
(TYPE 4a)**



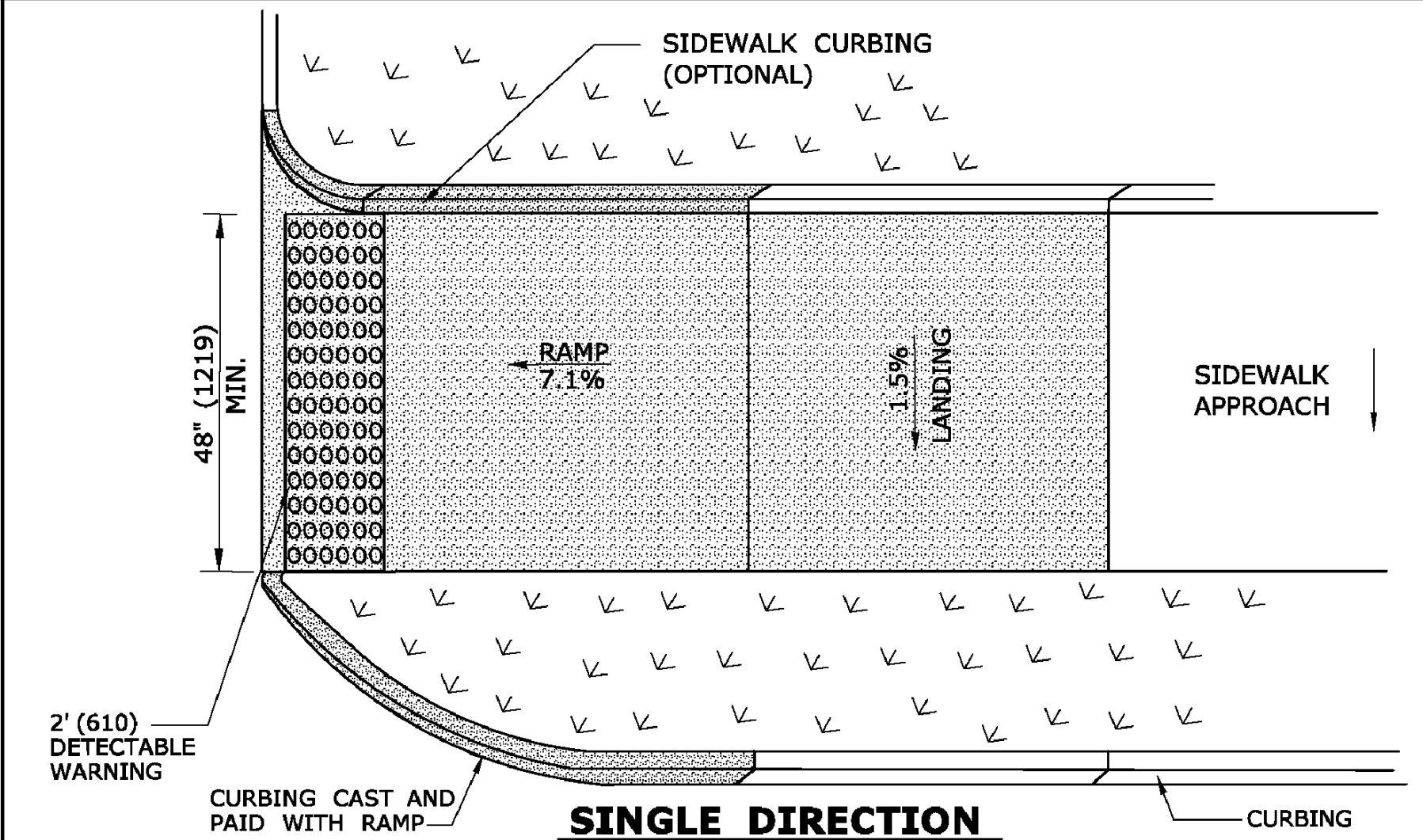
**PARALLEL/PERPENDICULAR SIDEWALK RAMP  
COMBINATION W/ CURB RETURNS (TYPE 4d)**  
\* OPTIONAL FLARE ONE SIDE



**RESTRICTED CONDITION  
DIAGONAL SIDEWALK RAMP  
(TYPE 4g)**



**DOUBLE DIRECTION  
PARALLEL SIDEWALK RAMP  
NO / UTILITY GRASS STRIP  
(TYPE 4b)**  
SEE NOTE 20 SHEET 1



**SINGLE DIRECTION  
PERPENDICULAR SIDEWALK RAMP  
W/ UTILITY GRASS STRIP (TYPE 1)**  
REFER TO DETECTABLE WARNING PLACEMENT ON SHEET 4

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

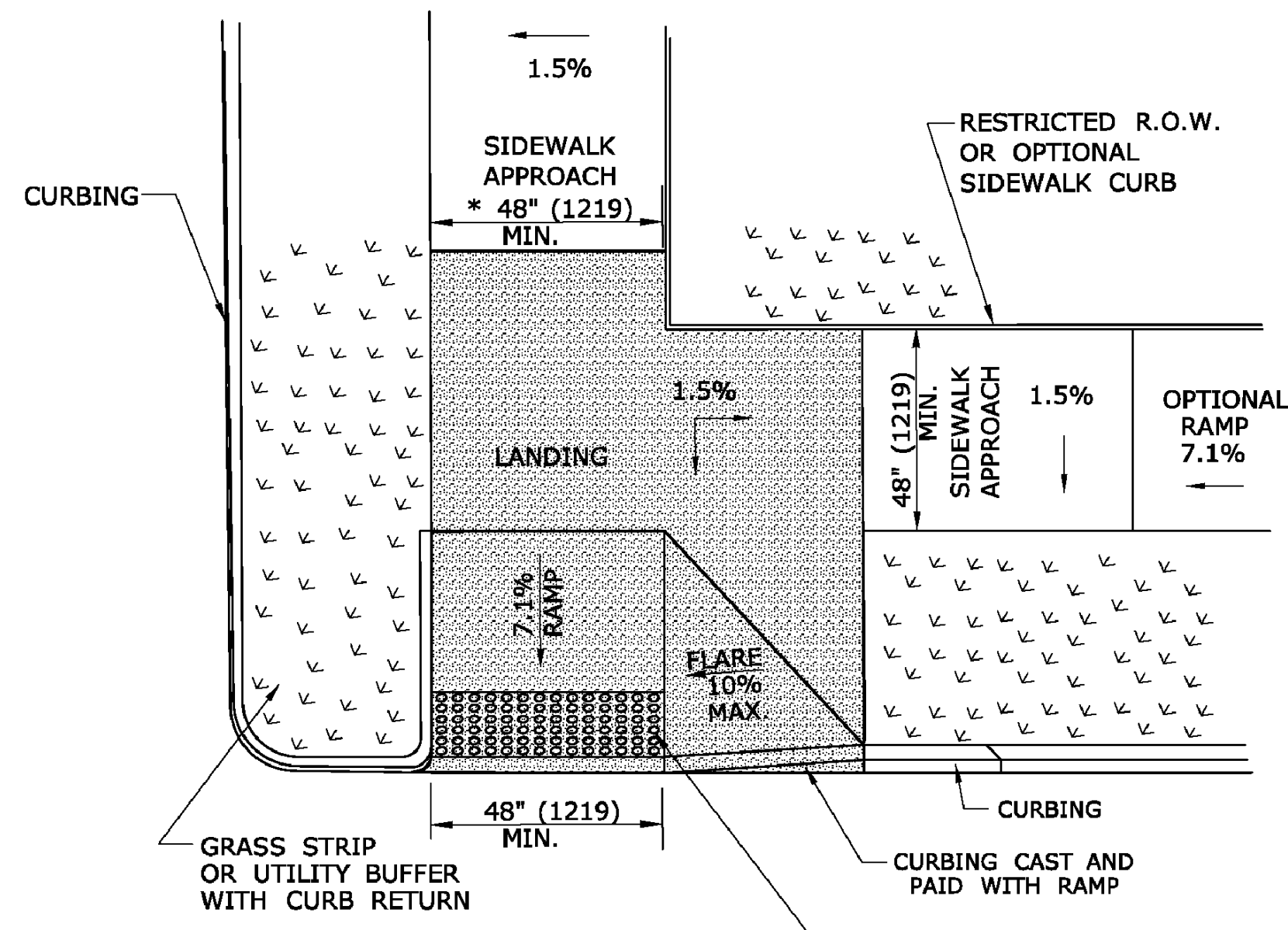
DESCRIPTION	DATE	BY

MISCELLANEOUS DETAILS-SIDEWALK RAMPS SHEET 2  
RECONSTRUCTION OF CROSS STREET  
NAUGATUCK, CONNECTICUT

TDS DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE: N.T.S.		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: MDS-03		

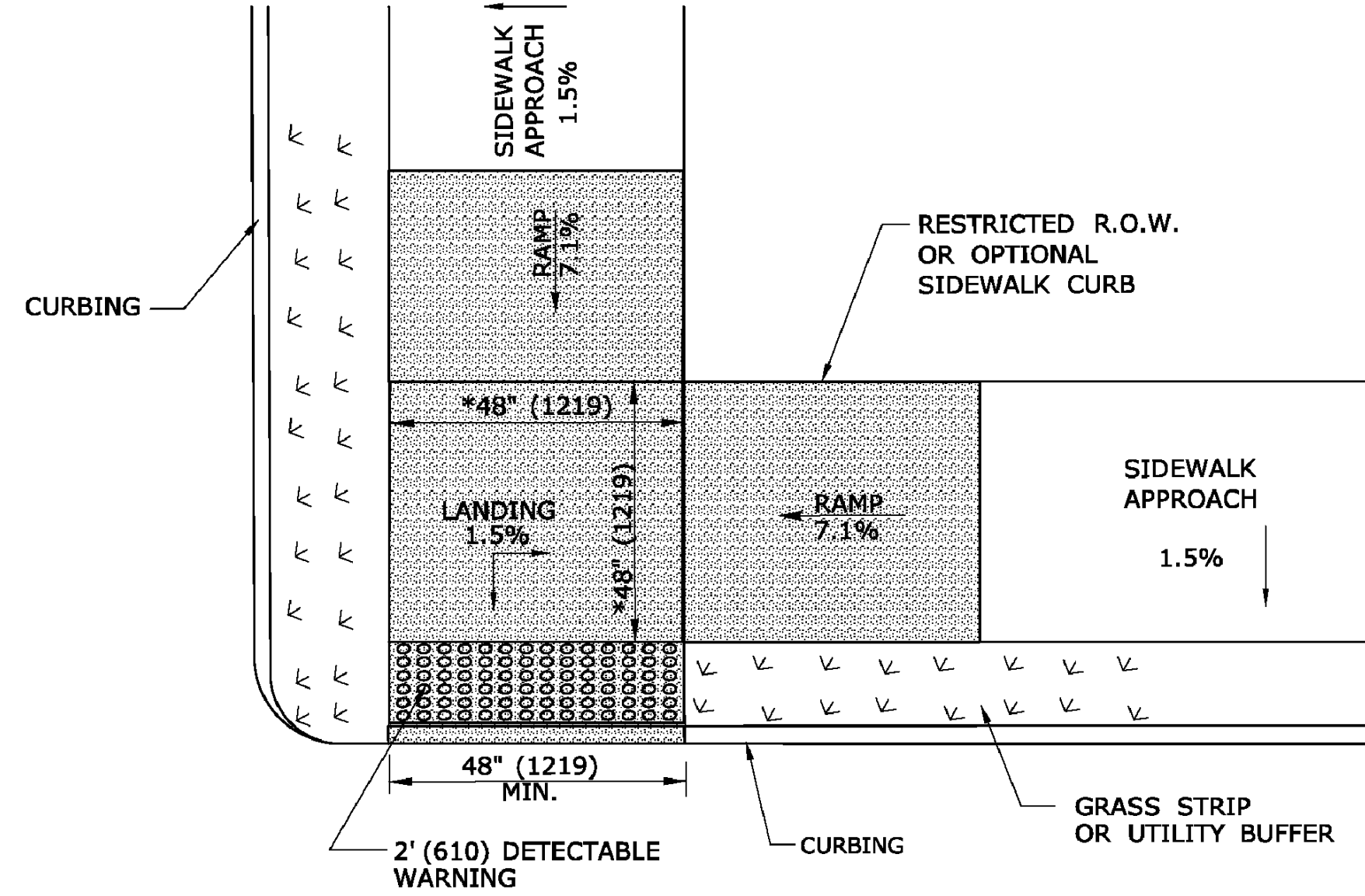


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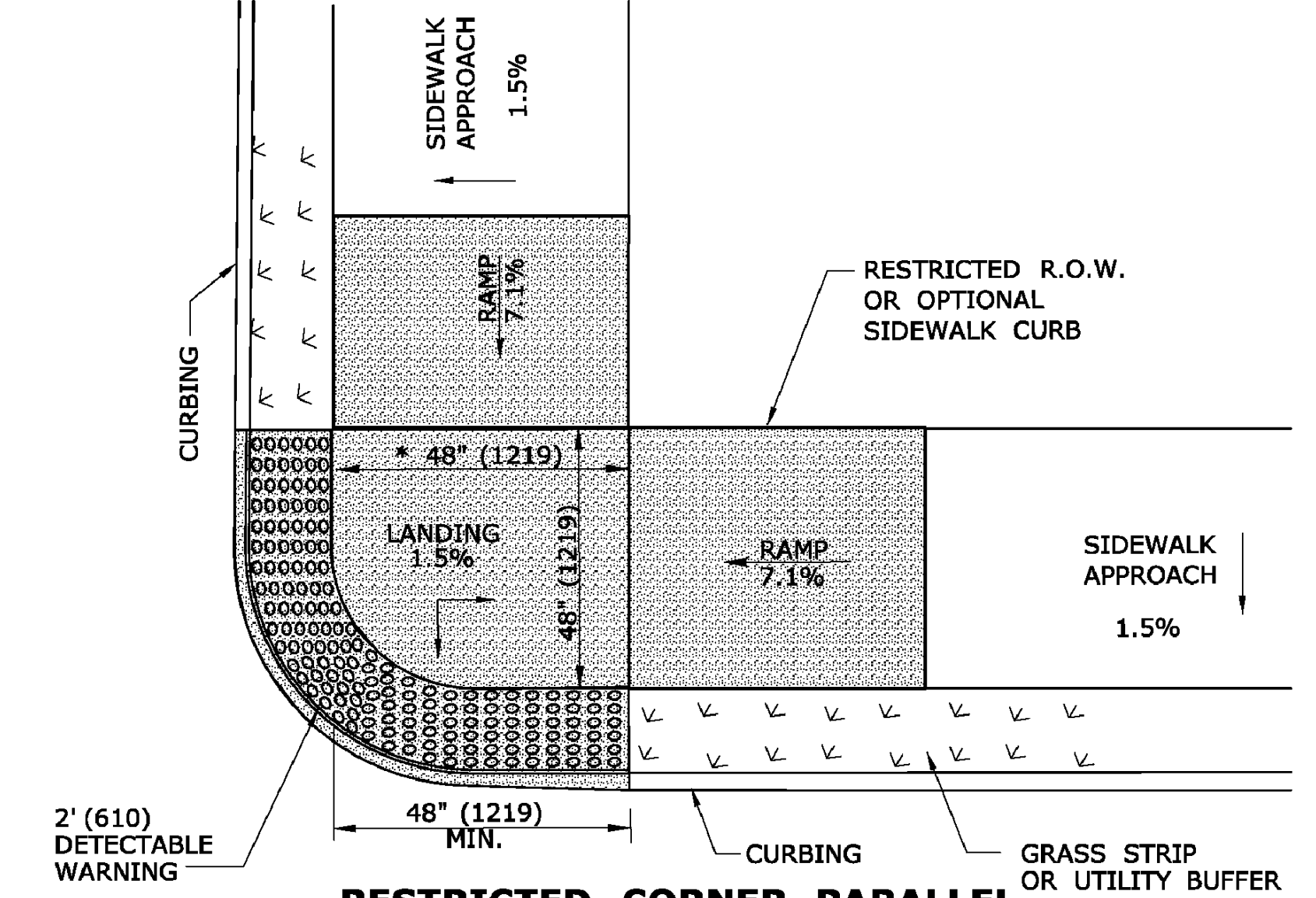
**RESTRICTED CORNER PERPENDICULAR SIDEWALK RAMP (TYPE 5)**

\* SEE NOTE 19 SHEET 1



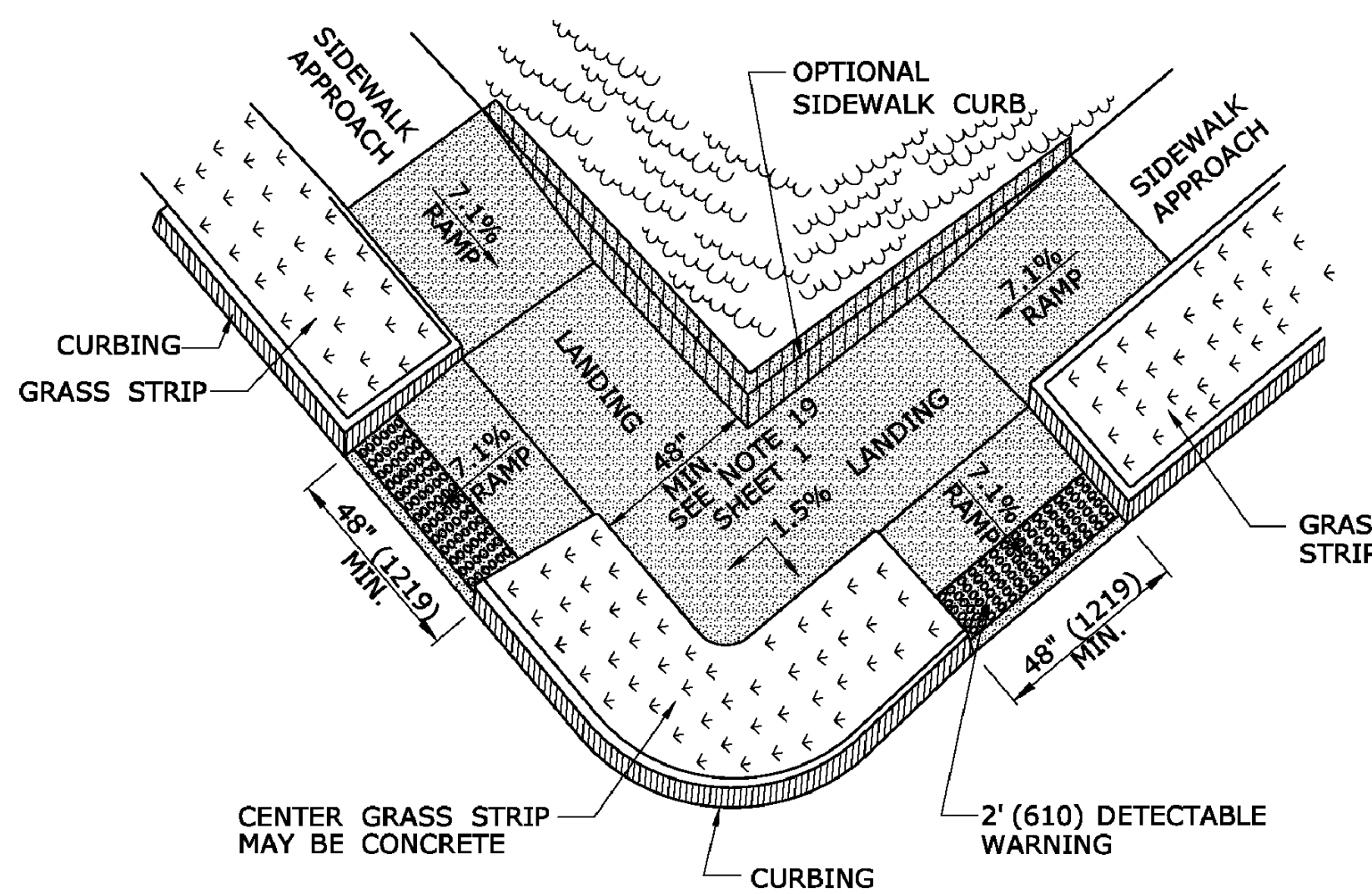
**RESTRICTED CORNER PARALLEL DOUBLE SIDEWALK RAMP W/CENTER LANDING AND UTILITY GRASS STRIP (TYPE 5a)**

\* SEE NOTE 20 SHEET 1



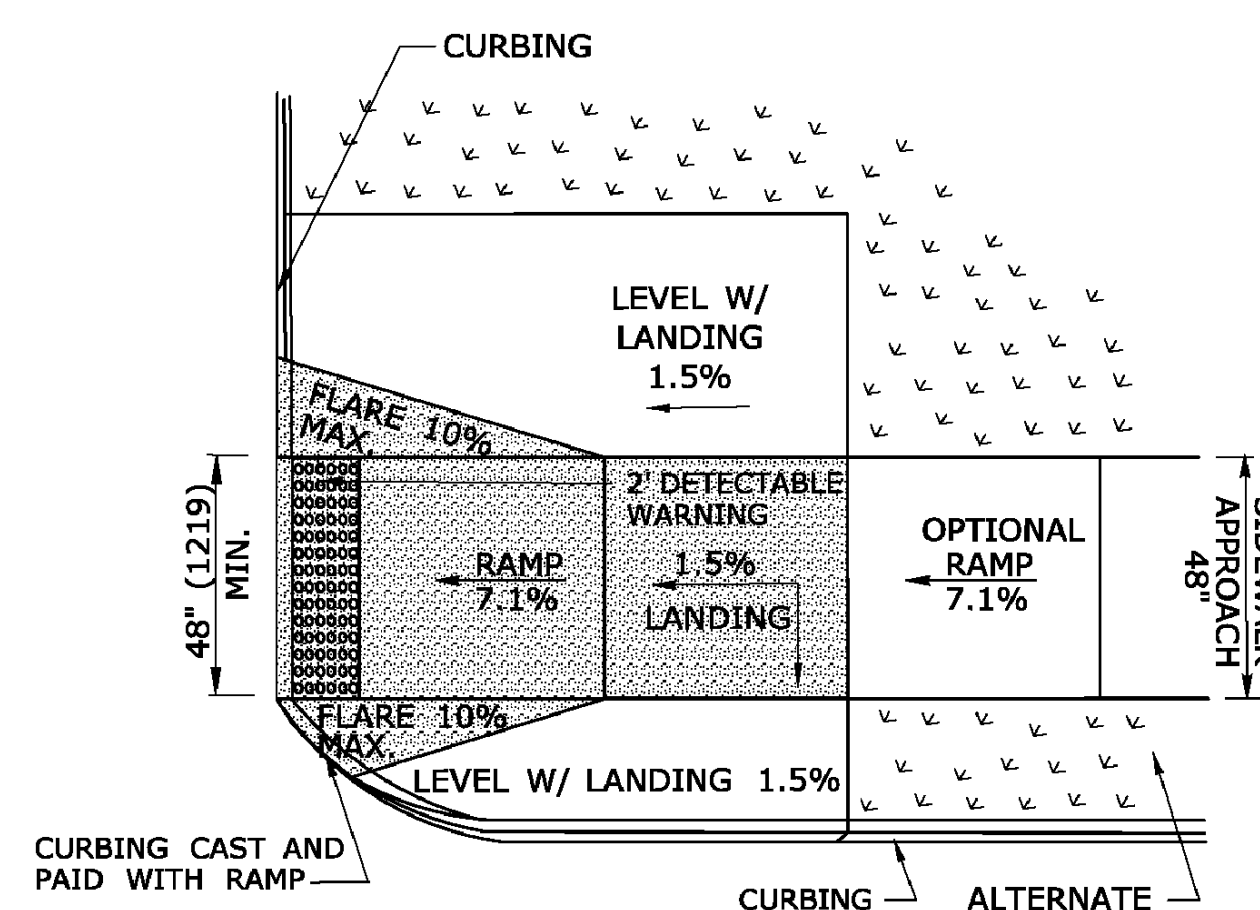
**RESTRICTED CORNER PARALLEL SIDEWALK RAMP W/ CENTER LANDING TWO DIRECTION AND UTILITY GRASS STRIP (TYPE 5b)**

\* SEE NOTE 20 SHEET 1

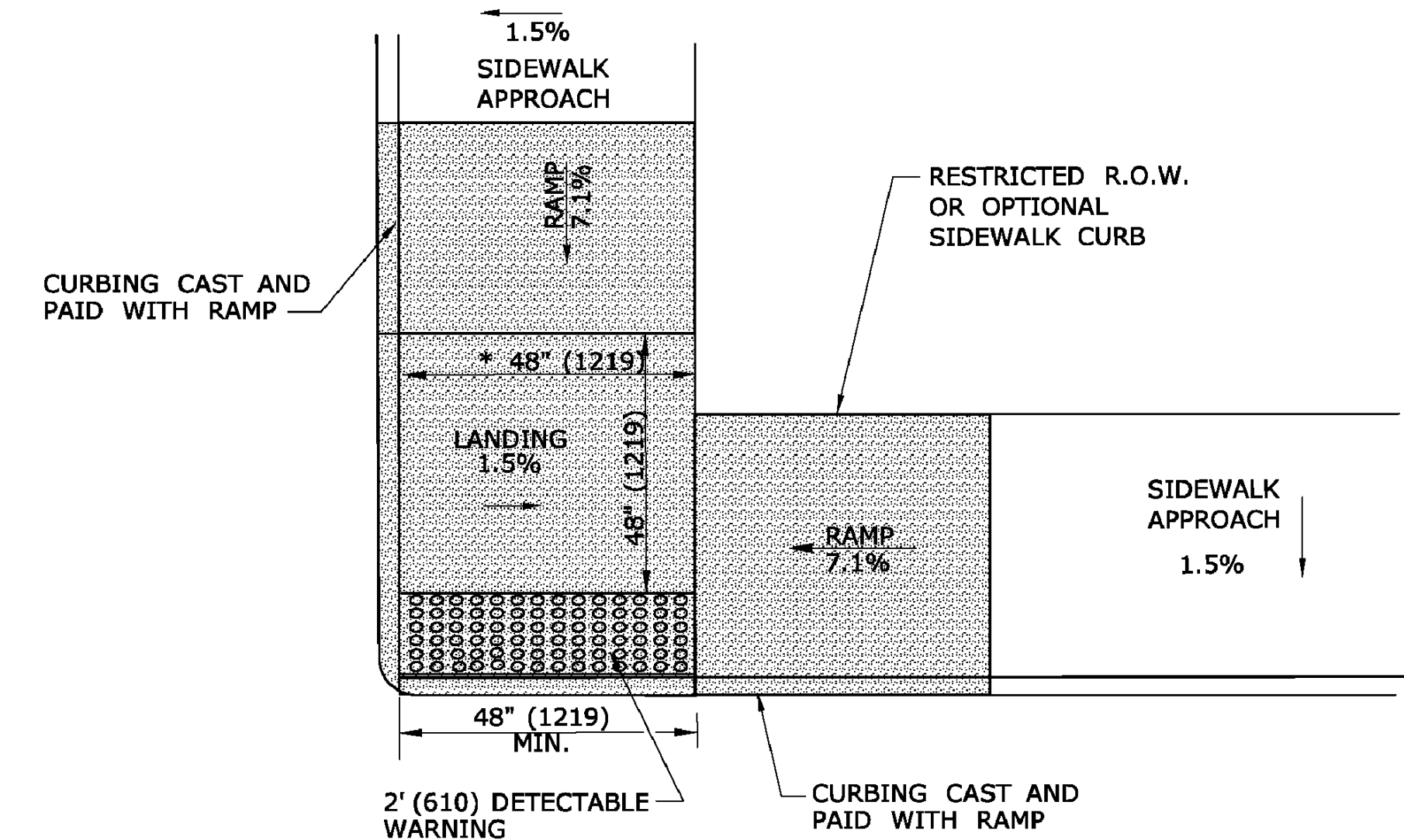


**COMBINATION SIDEWALK RAMP (TYPE 6)**

\* SEE NOTE 19 SHEET 1

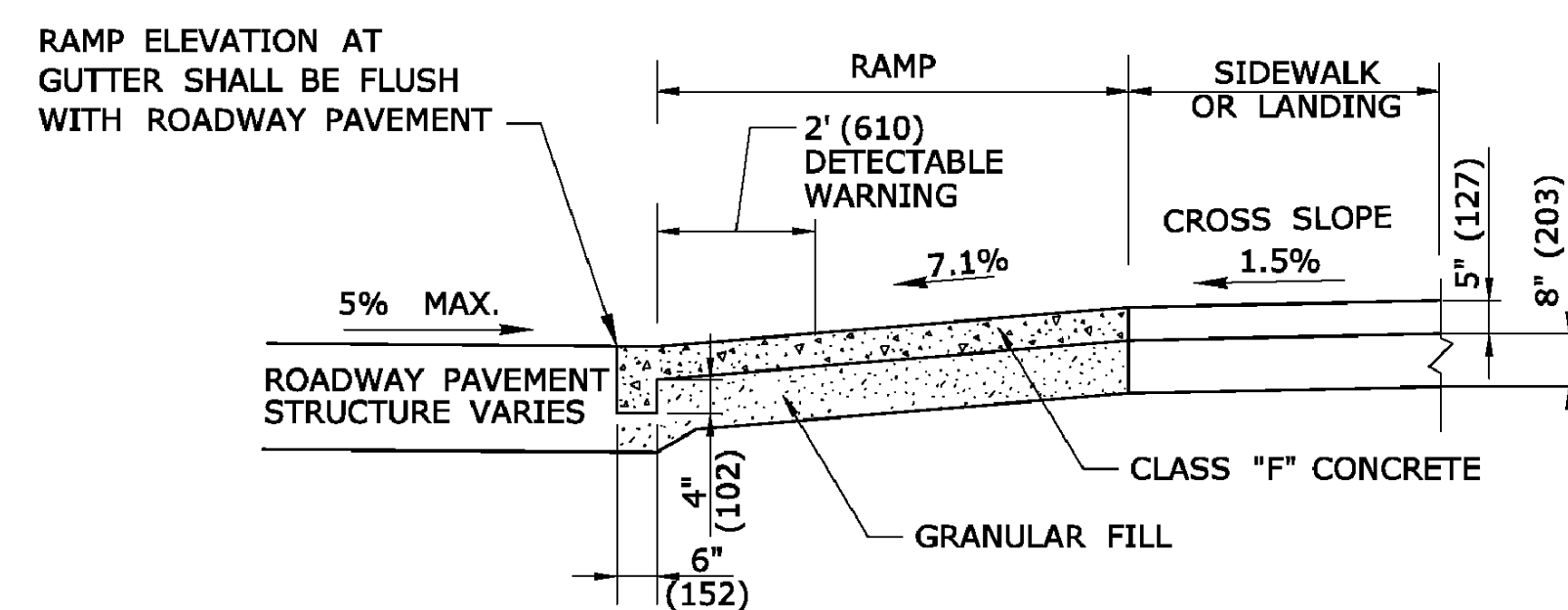


**PERPENDICULAR SIDEWALK RAMP ONE DIRECTION ON CORNER (TYPE 6a)**



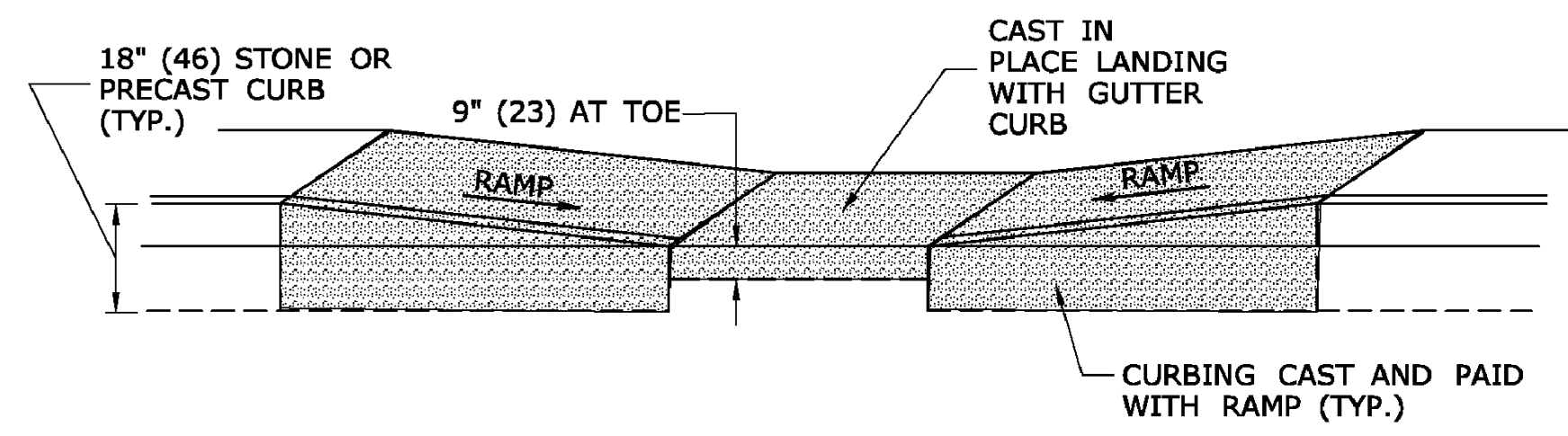
**RESTRICTED CORNER PARALLEL SIDEWALK RAMP W/CENTER LANDING NO GRASS STRIP (TYPE 5c)**

\* SEE NOTE 20 SHEET 1

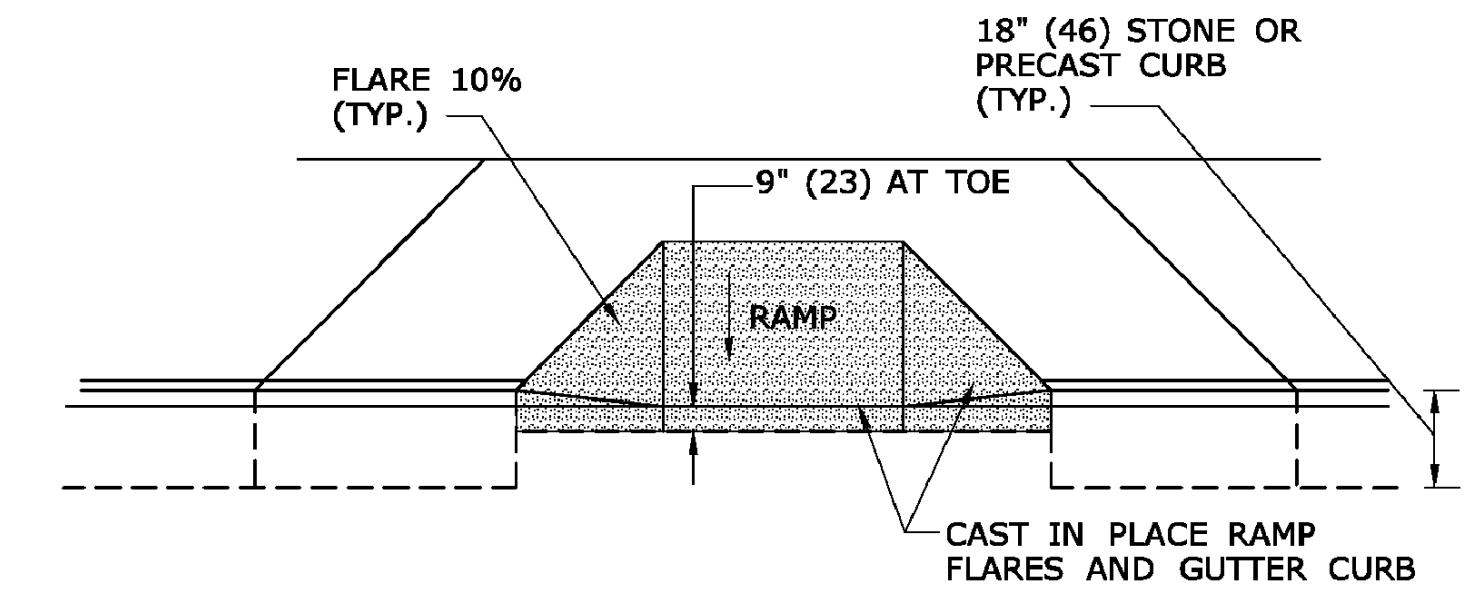


**TYPICAL SECTION THRU SIDEWALK RAMP**

SEE NOTE 2 AND 17 SHEET 1

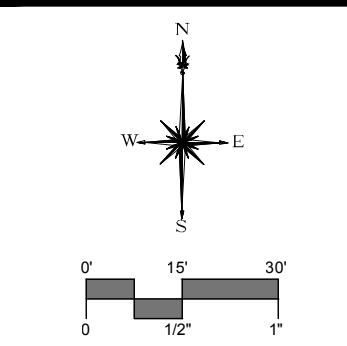


**TYPICAL ELEVATION PARALLEL SIDEWALK RAMP WITH CAST IN PLACE GUTTER**



**TYPICAL ELEVATION PERPENDICULAR SIDEWALK RAMP WITH CAST IN PLACE GUTTER**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED



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DATE	BY	DESCRIPTION

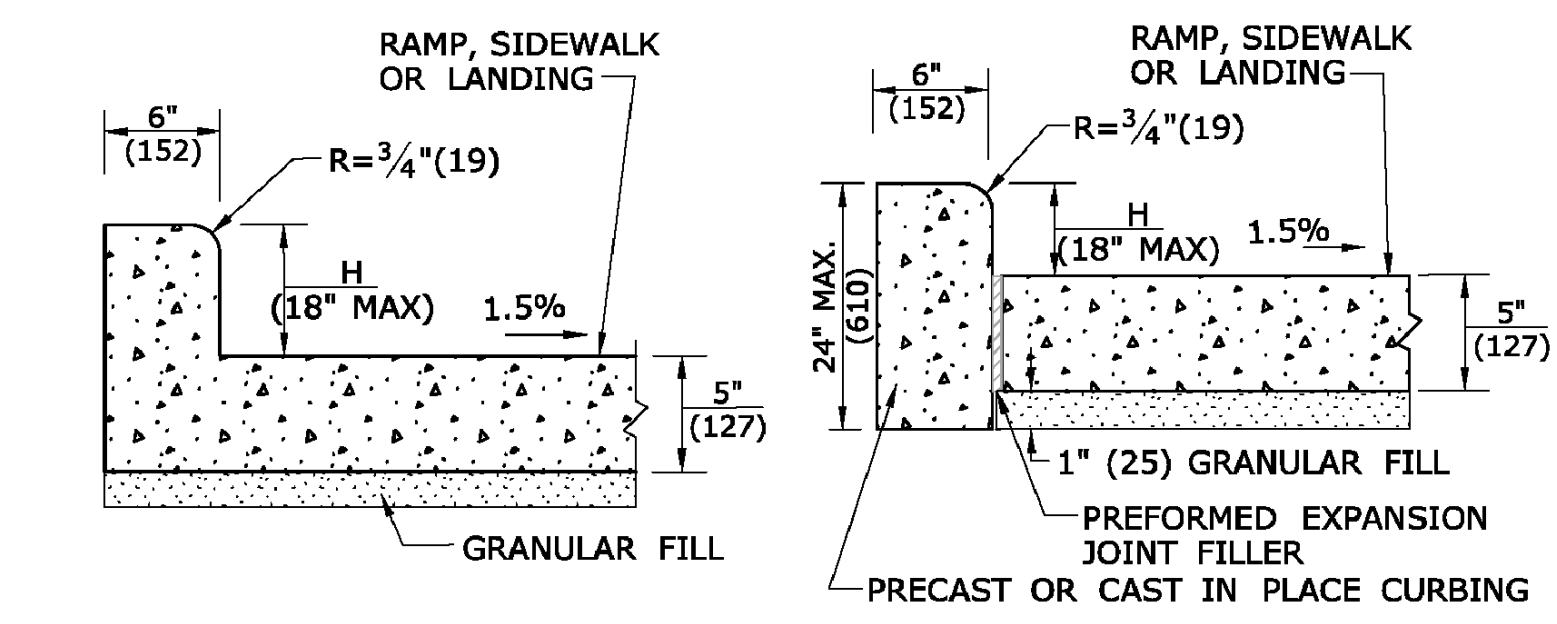
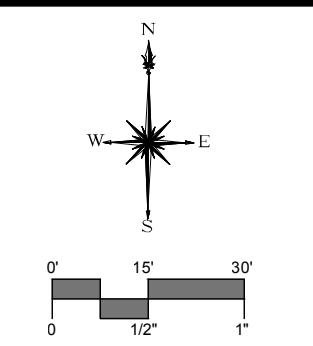
MISCELLANEOUS DETAILS-SIDEWALK RAMPS SHEET 3  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT

TDS DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE: N.T.S.		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: MDS-04		

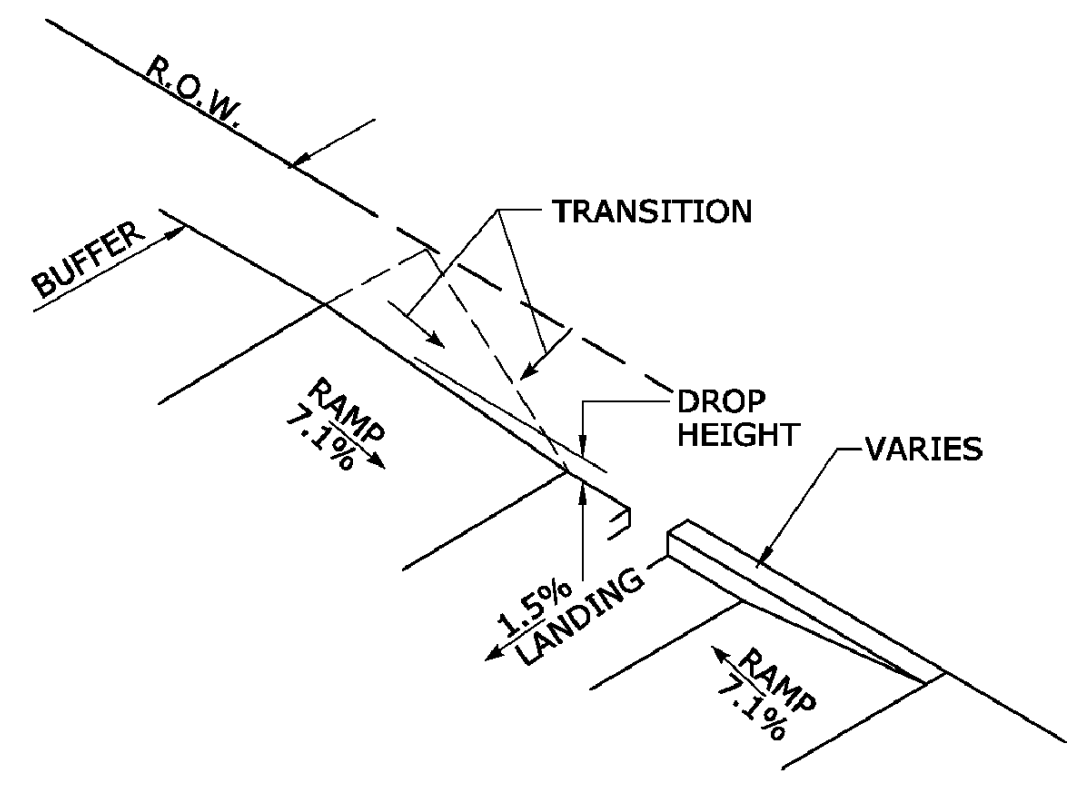
**08**  
 SHEET NO.



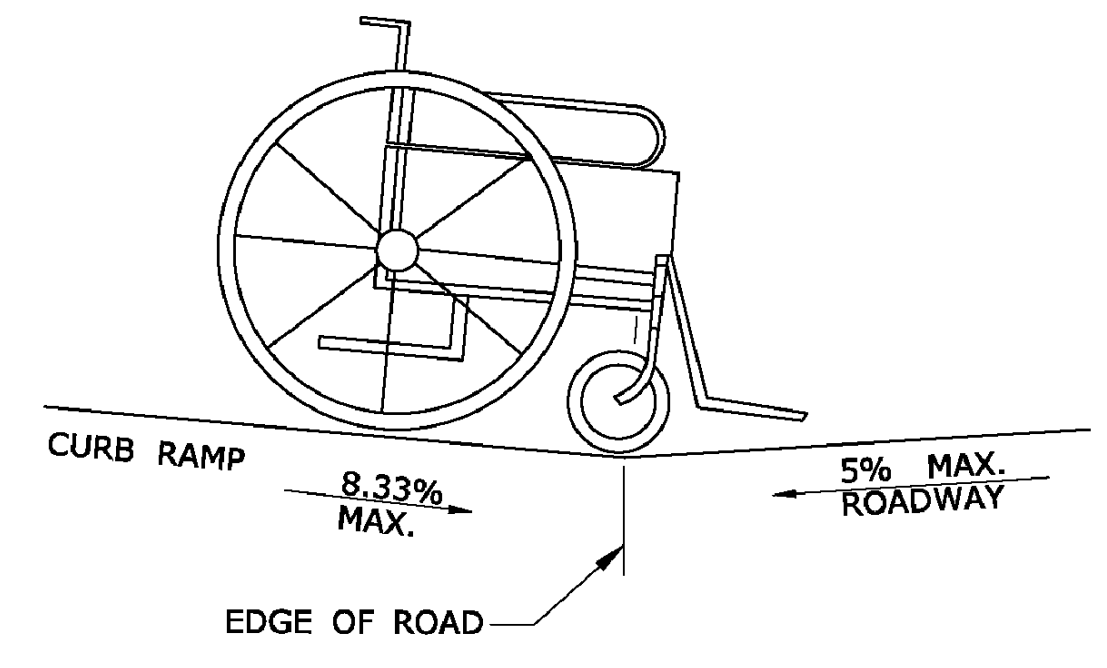
MILONE & MACBROOM, INC. 2015



**MONOLITHIC CAST CURB**      **SEPARATELY CAST CURB**  
**SIDEWALK CURB OPTIONS AT BACK OF SIDEWALK**



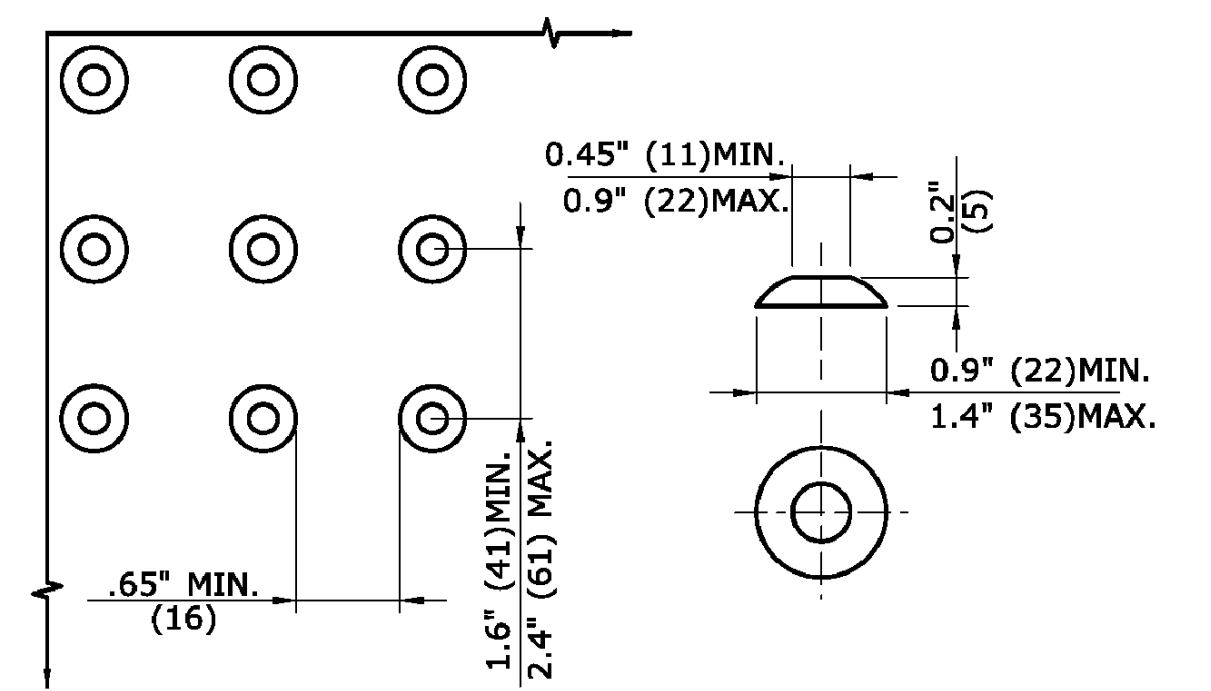
**BACK OF SIDEWALK CURB OR BUFFER TRANSITION**



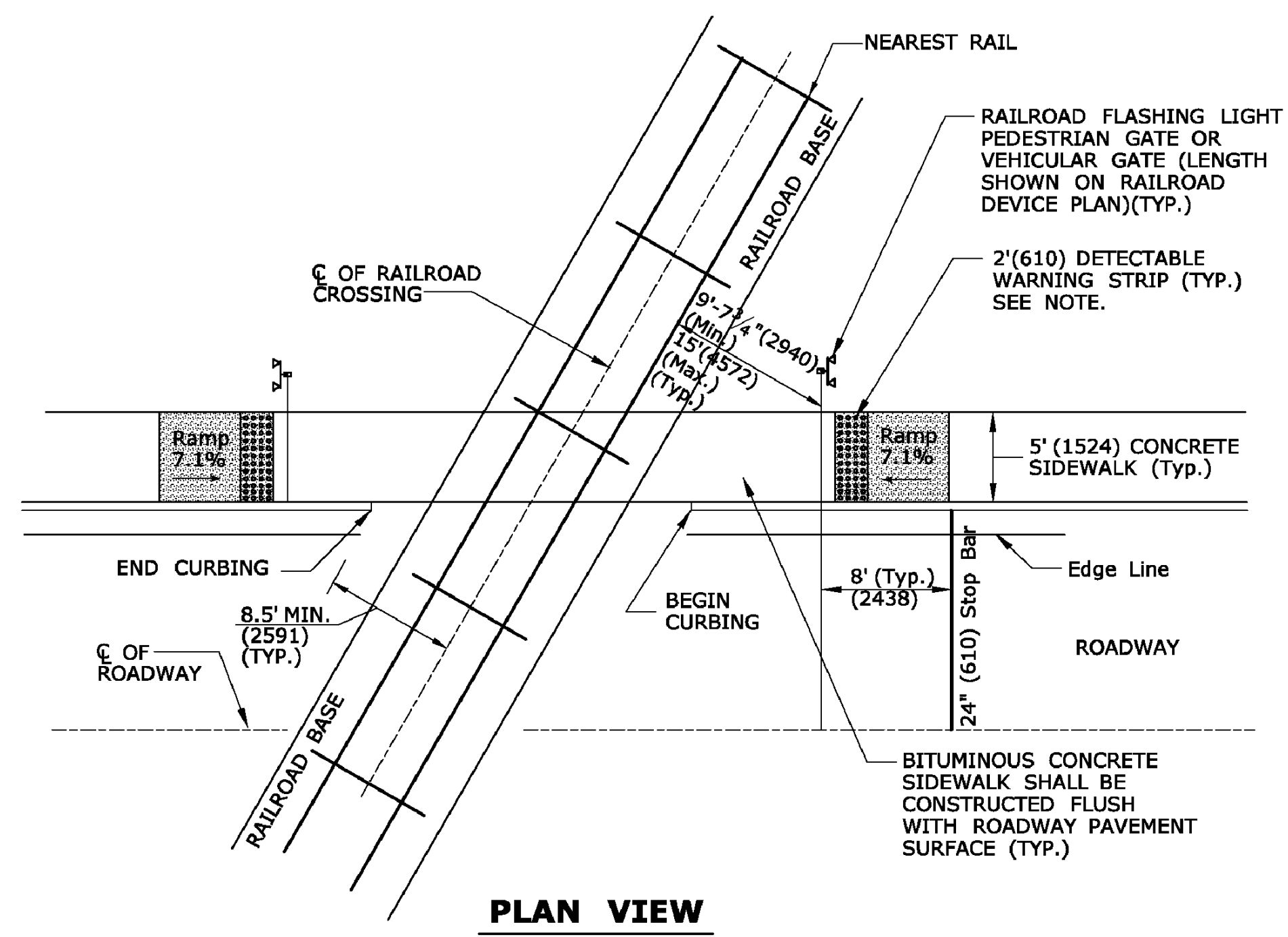
**DETAIL 1**  
**SEE GRADE CHANGE AT ROADWAY INTERFACE**  
 SEE NOTE 1 SHEET HW-921 02a

**GENERAL NOTES:**

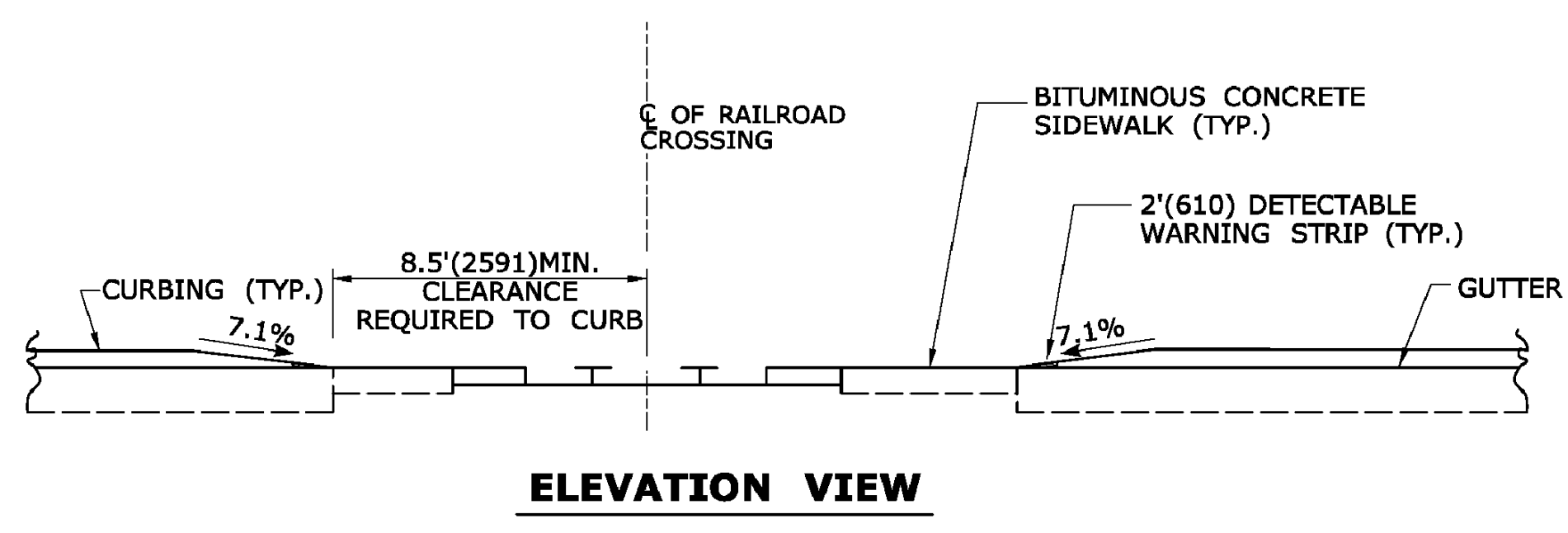
1. RAMPED MEDIANS SHALL HAVE A CURB RAMP AT EITHER END AND LEVEL LANDING A MINIMUM OF 5' x 5' (1.5m x 1.5m) IN BETWEEN. CUT-THROUGH MEDIANS SHALL BE A MINIMUM OF 6' (1.8m) LONG AND 5' (1.5m) WIDE. FOR ALL MEDIANS, CUT-THROUGH OR RAMPED, A 2' (610) STRIP OF DETECTABLE WARNINGS SHALL BE INSTALLED AT THE ENTRANCE AND EXIT.
2. SEE GENERAL NOTES ON SHEET 1.



**DOME SPACING**      **DOME SECTION**  
**STANDARD DOME ON DETECTABLE WARNING TILES**



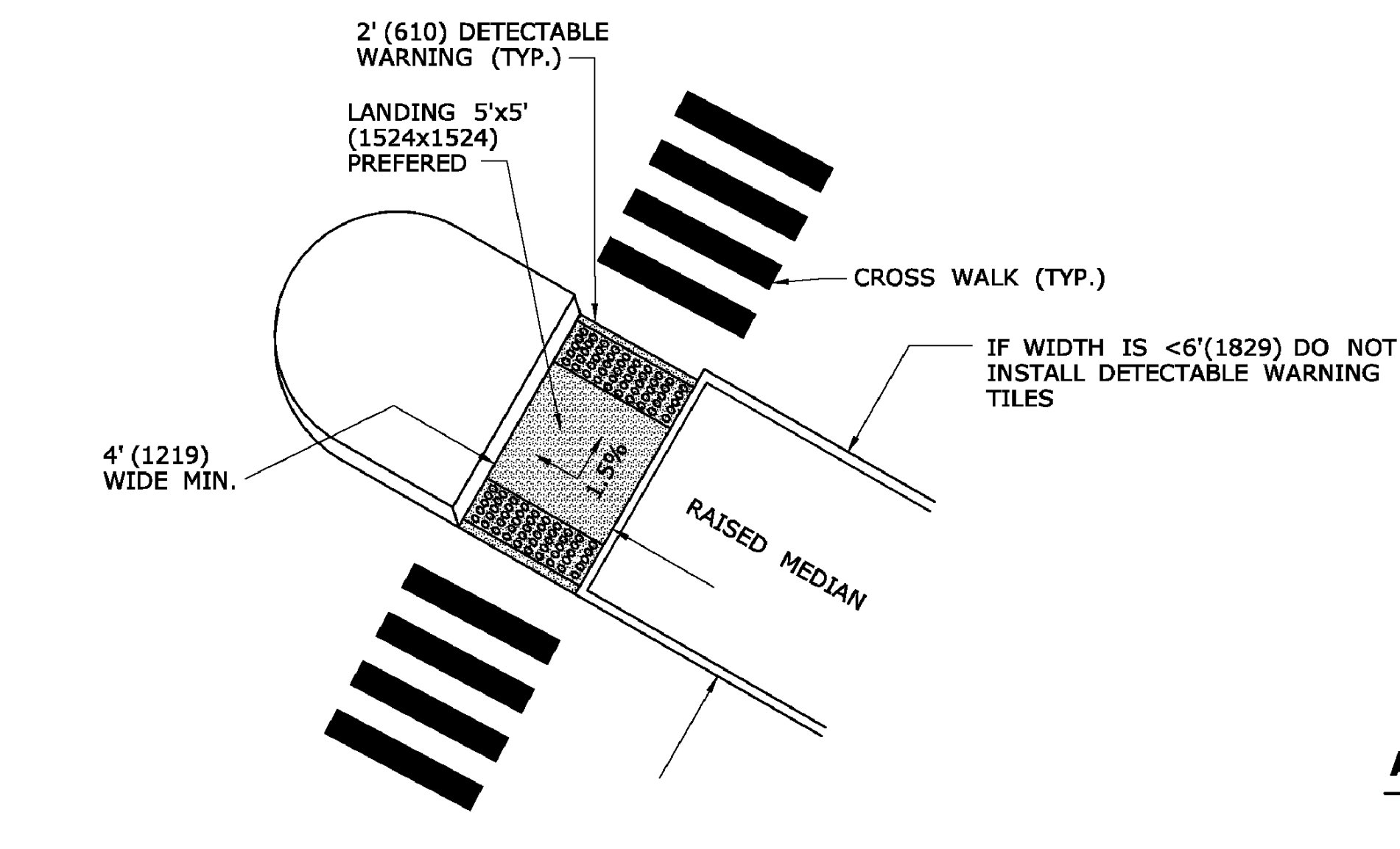
**PLAN VIEW**



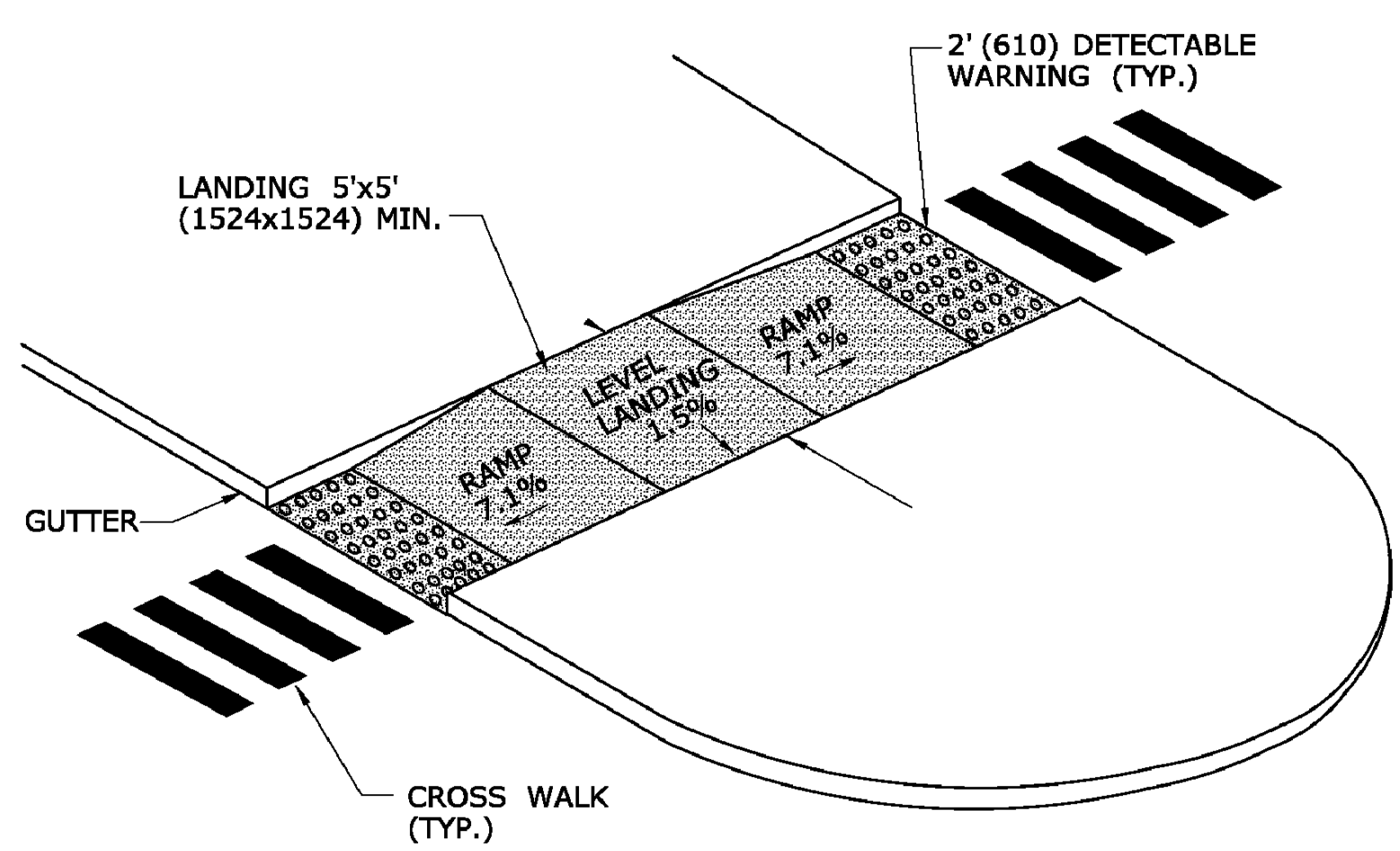
**ELEVATION VIEW**

**DETECTABLE WARNINGS AT RAILROAD CROSSING**

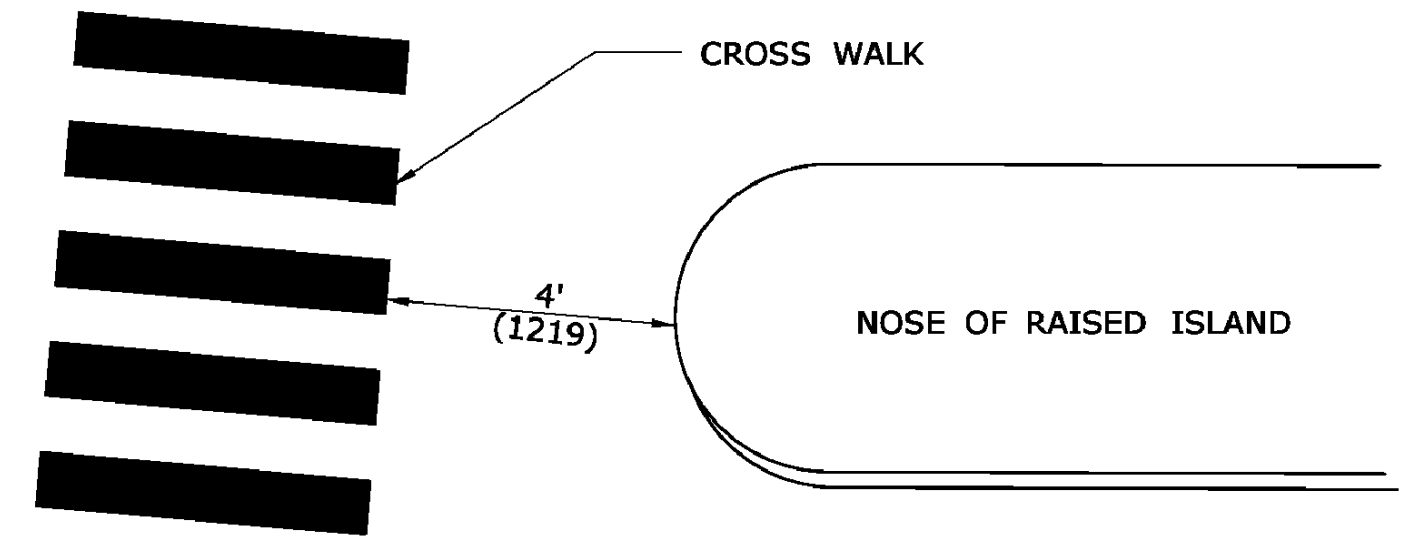
NOTE: WHEN NO GATE IS PRESENT, INSTALL DETECTABLE WARNING SURFACE 12' (3.6m) FROM THE NEAREST RAIL IF GATE IS PRESENT, INSTALL DETECTABLE WARNING 2' (610) PRIOR TO GATE. THE ROWS OF TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL BE INSTALLED PARALLEL WITH THE DIRECTION OF PEDESTRIAN TRAVEL.



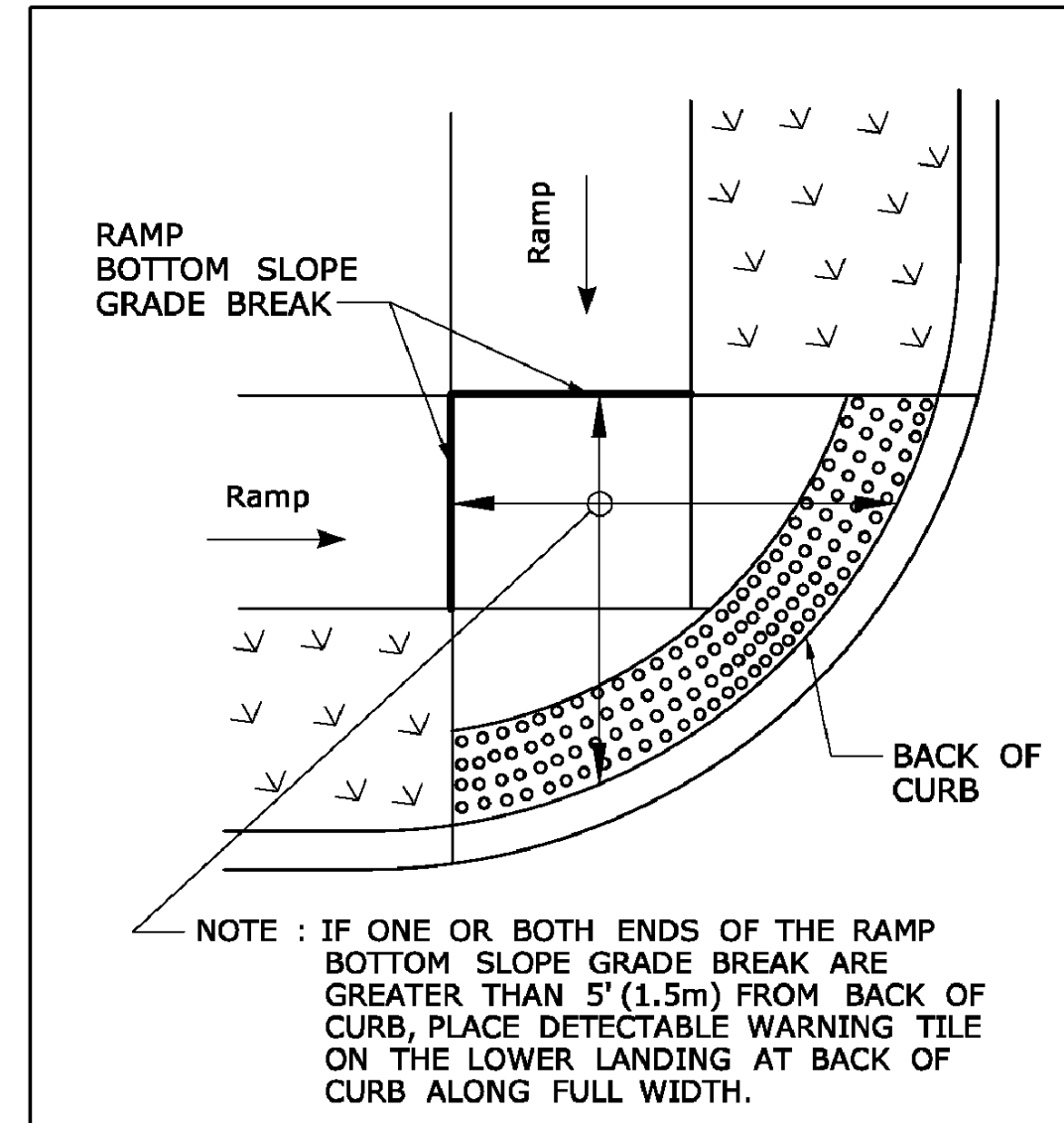
**CUT-THROUGH MEDIAN ISLAND**



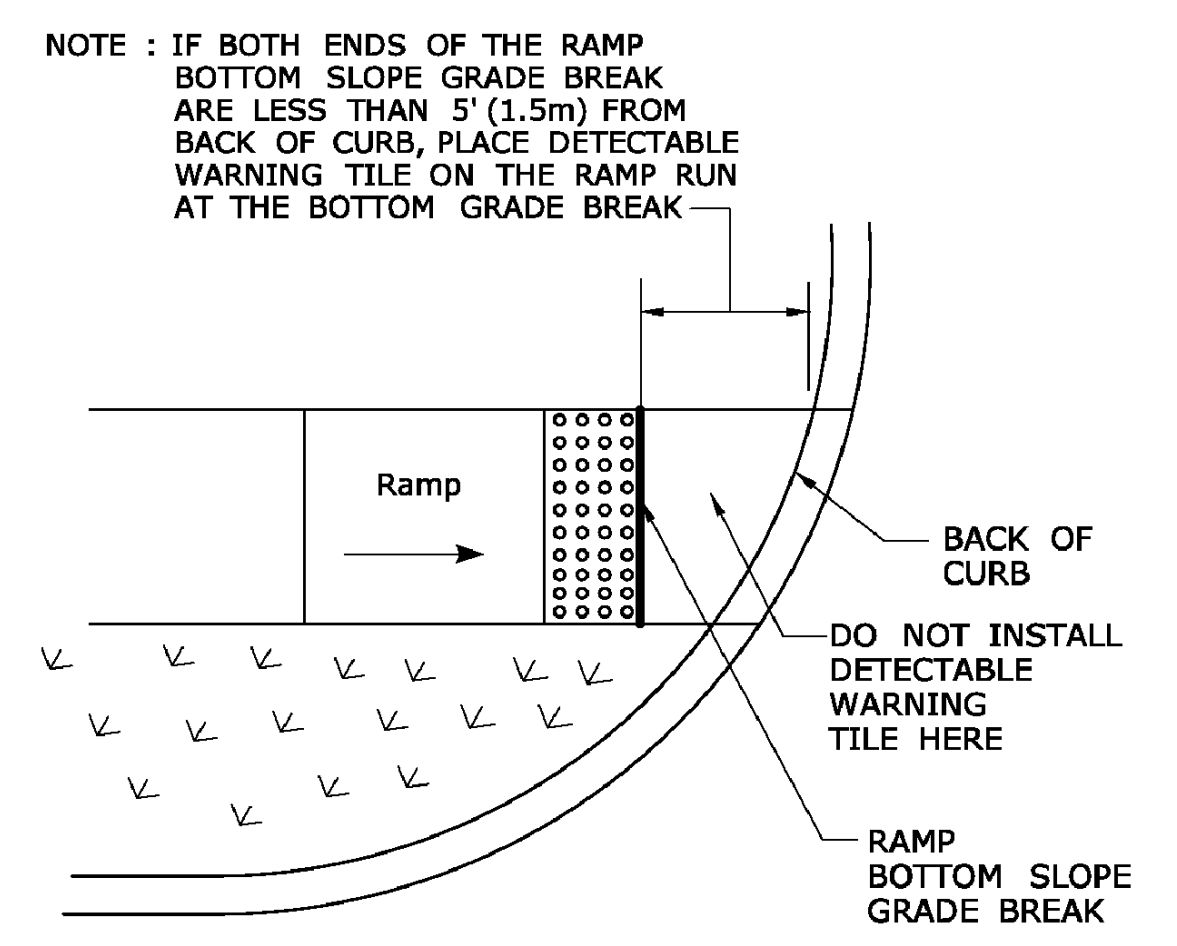
**RAISED MEDIAN ISLAND WITH LANDING AND RAMPS**



**ALTERNATE CROSSWALK WITH MEDIAN ISLAND PULLED BACK**



**DETECTABLE WARNING PLACEMENT DETAIL 1**



**DETECTABLE WARNING PLACEMENT DETAIL 2**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

DESCRIPTION

DATE

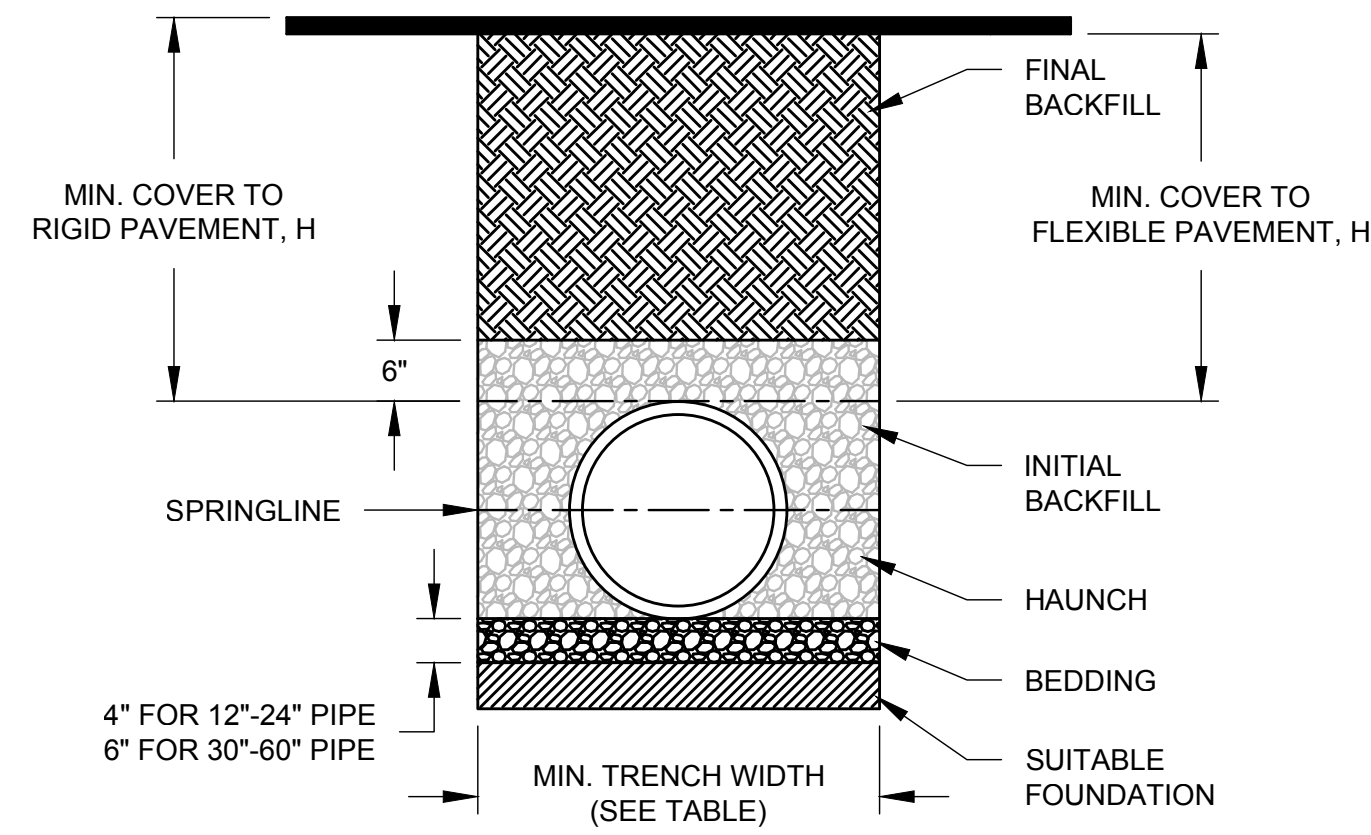
MISCELLANEOUS DETAILS-SIDEWALK RAMPS SHEET 4  
 RECONSTRUCTION OF CROSS STREET

TDS DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE: N.T.S.		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: MDS-05		

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NAUGATUCK, CONNECTICUT





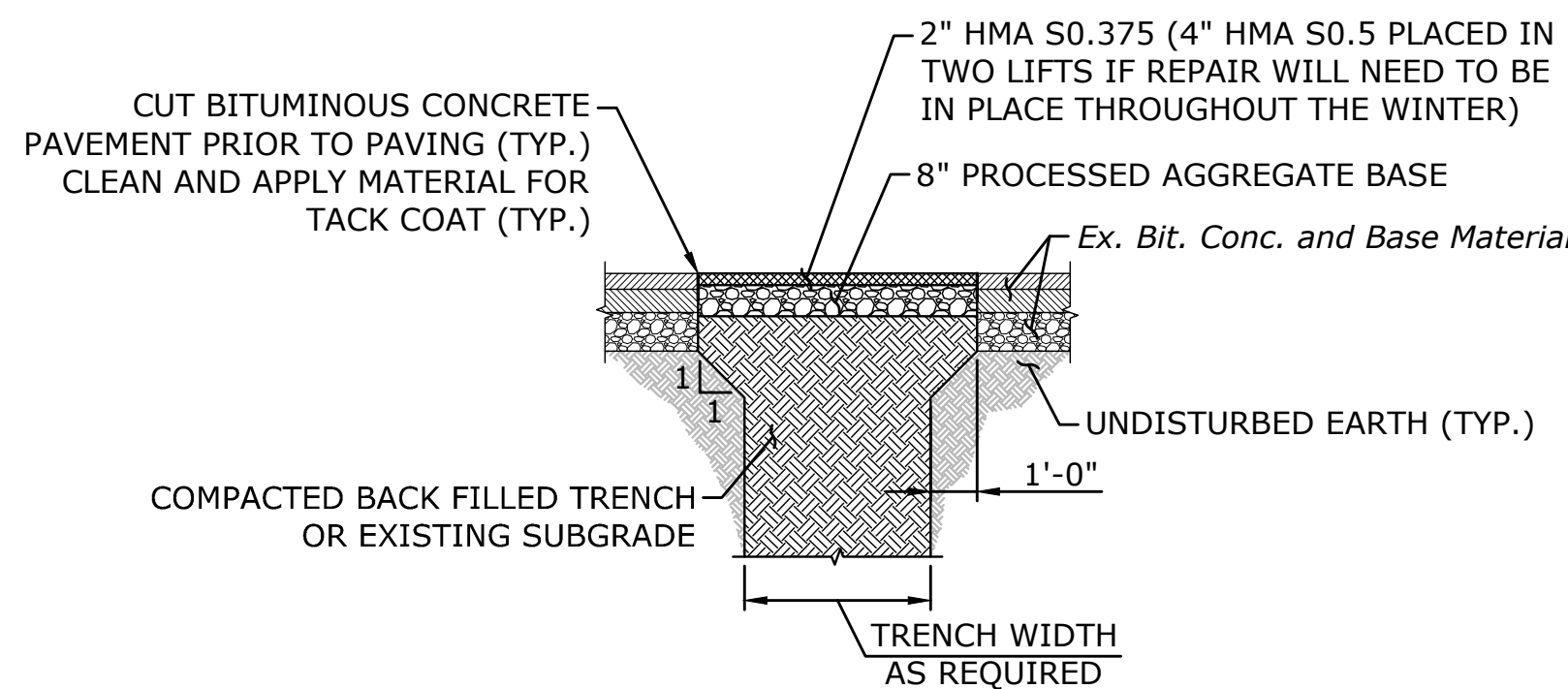
RECOMMENDED MINIMUM TRENCH WIDTHS

PIPE DIAM.	MIN. TRENCH WIDTH
12"	30"
15"	34"
18"	39"
24"	48"

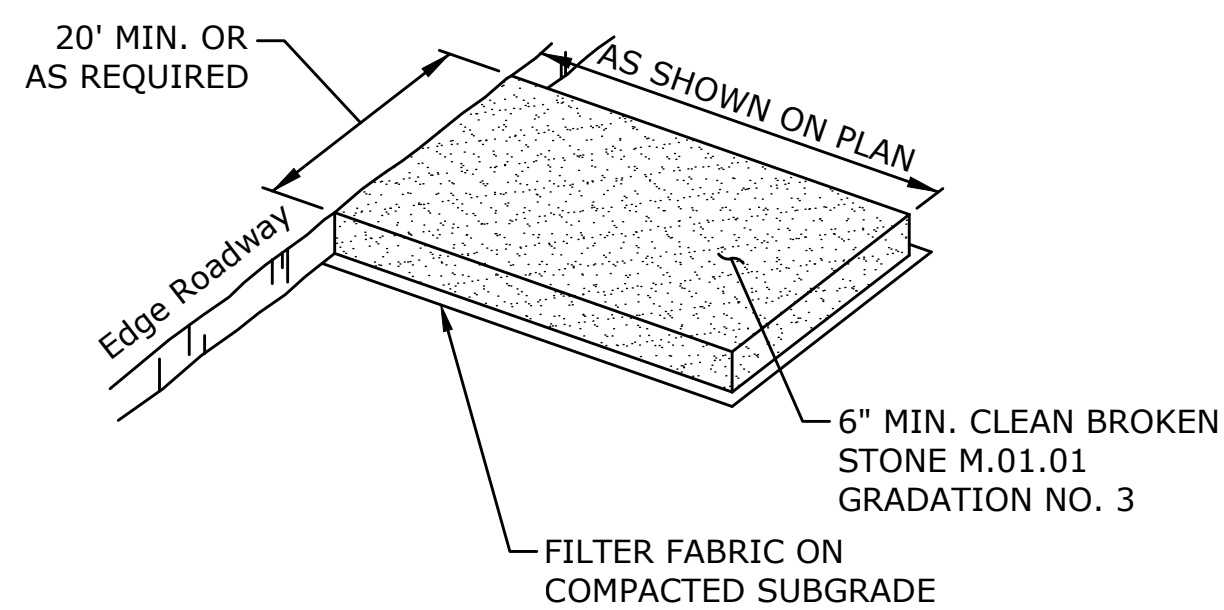
**NOTES:**

- ALL PIPE SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH ASTM D2321, "STANDARD PRACTICE FOR UNDERGROUND INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY FLOW APPLICATIONS", LATEST ADDITION
- MEASURES SHOULD BE TAKEN TO PREVENT MIGRATION OF NATIVE FINES INTO BACKFILL MATERIAL, WHEN REQUIRED.
- FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL AS SPECIFIED BY THE ENGINEER, AS AN ALTERNATIVE AND AT THE DISCRETION OF THE DESIGN ENGINEER, THE TRENCH BOTTOM MAY BE STABILIZED USING A GEOTEXTILE MATERIAL.
- BEDDING: SUITABLE MATERIAL SHALL BE CLASS I, II OR III. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER, UNLESS OTHERWISE NOTED BY THE ENGINEER, MINIMUM BEDDING THICKNESS SHALL BE 4" (100mm) FOR 4"-24" (100mm-600mm); 6" (150mm) FOR 30"-60" (750mm-1500mm).
- INITIAL BACKFILL: SUITABLE MATERIAL SHALL BE CLASS I, II OR III IN THE PIPE ZONE EXTENDING NOT LESS THAN 6" ABOVE CROWN OF PIPE. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. MATERIAL SHALL BE INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION.
- MINIMUM COVER: MINIMUM COVER, H, IN NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS) IS 12" FROM THE TOP OF PIPE TO GROUND SURFACE. ADDITIONAL COVER MAY BE REQUIRED TO PREVENT FLOTATION. FOR TRAFFIC APPLICATIONS, MINIMUM COVER, H, IS 12" UP TO 48" DIAMETER PIPE AND 24" OF COVER FOR 60" DIAMETER PIPE, MEASURED FROM TOP OF PIPE TO BOTTOM OF FLEXIBLE PAVEMENT OR TO TOP OF RIGID PAVEMENT. FOR TRAFFIC APPLICATIONS WITH LESS THAN FOUR FEET OF COVER, EMBEDMENT OF THE PIPE SHALL BE USING ONLY A CLASS I OR CLASS II BACKFILL.

**C.P.P. (TYPE S) STORM TRENCH**

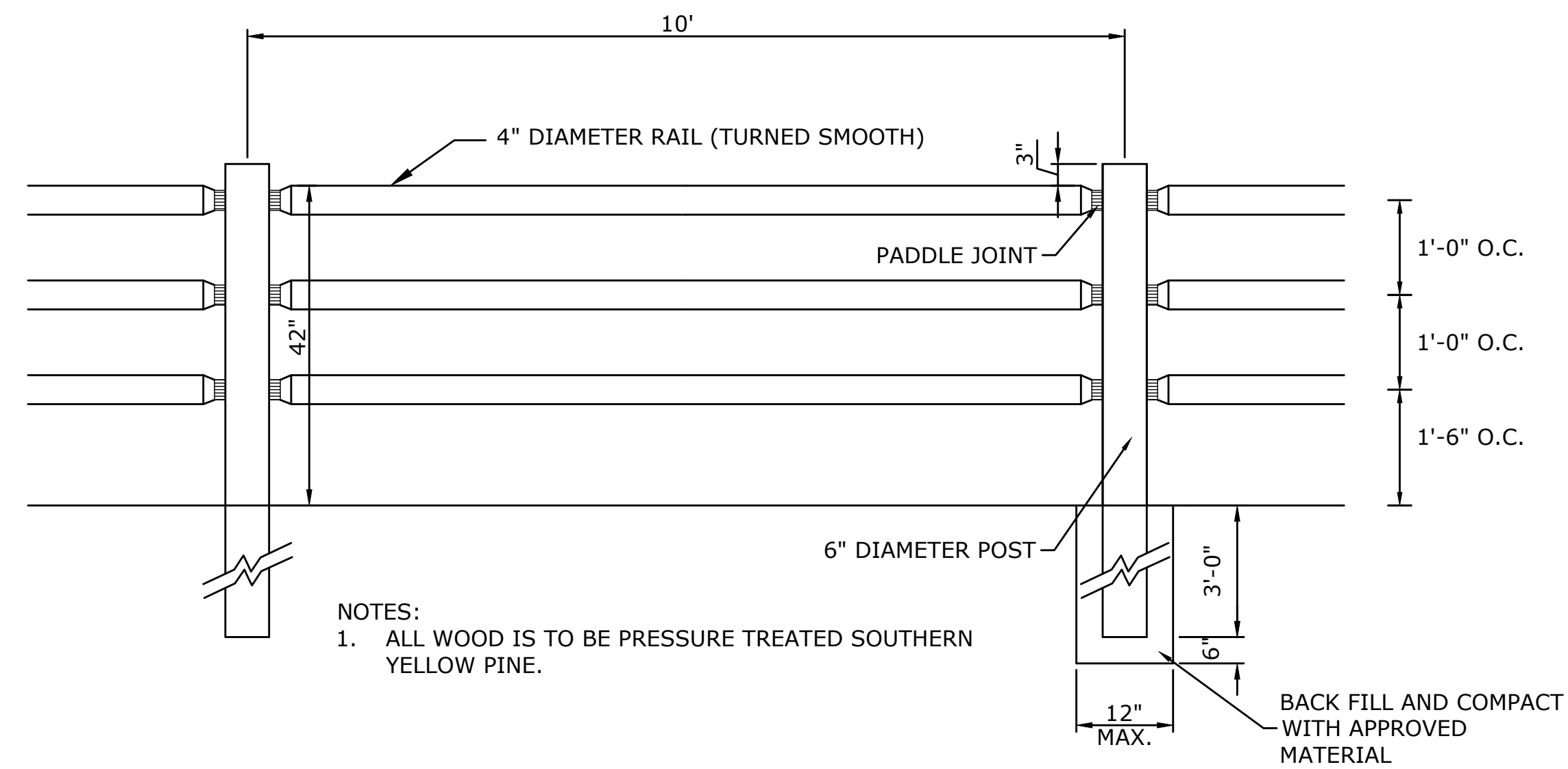


**TEMPORARY PAVEMENT REPAIR**  
NOT TO SCALE

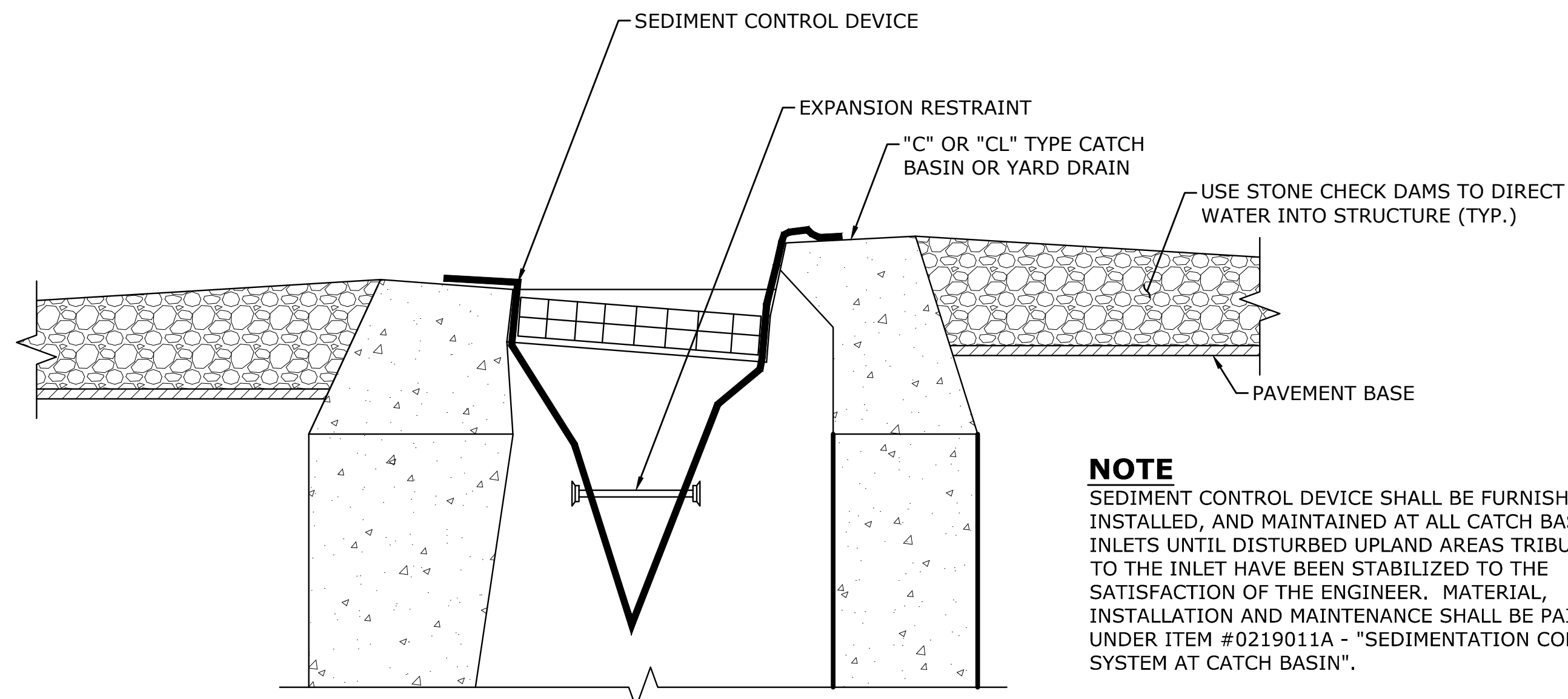


**NOTE**  
PAD SHALL BE INSTALLED AND MAINTAINED DURING OPERATIONS WHICH PROMOTE VEHICULAR TRACKING OF MUD.

**ANTI-TRACKING PAD**

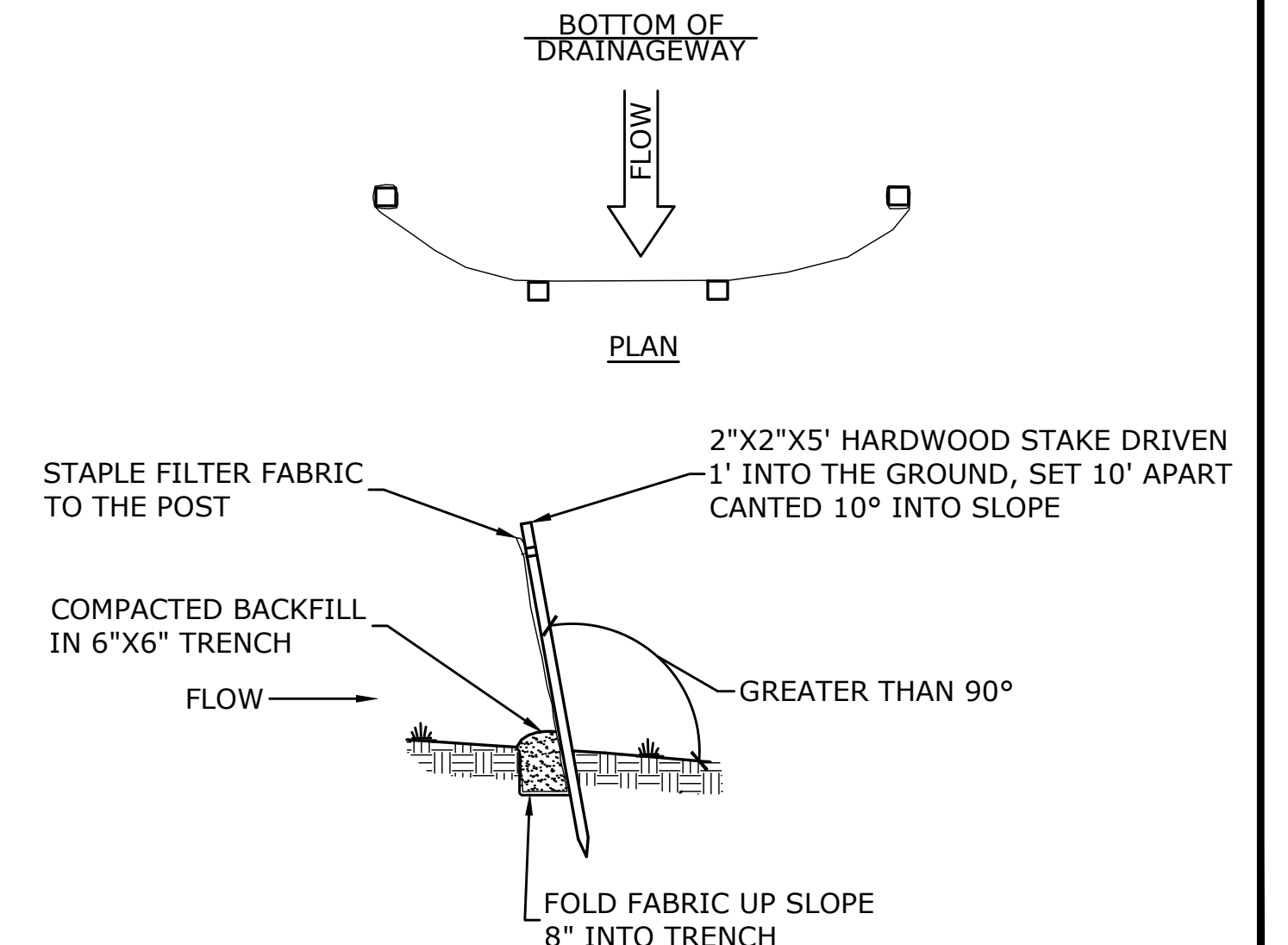
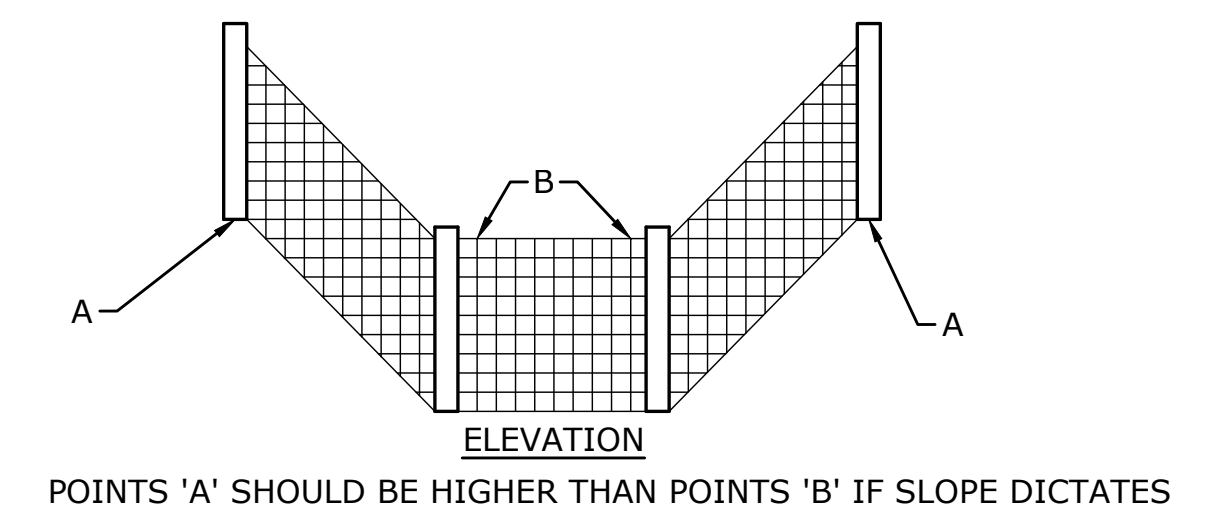


**THREE RAIL WOOD FENCE**

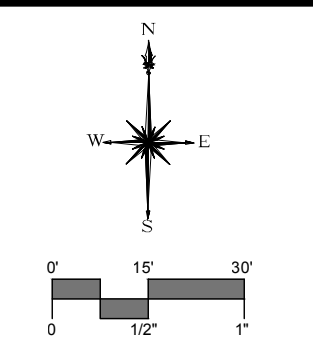


**NOTE**  
SEDIMENT CONTROL DEVICE SHALL BE FURNISHED, INSTALLED, AND MAINTAINED AT ALL CATCH BASIN INLETS UNTIL DISTURBED UPLAND AREAS TRIBUTARY TO THE INLET HAVE BEEN STABILIZED TO THE SATISFACTION OF THE ENGINEER. MATERIAL, INSTALLATION AND MAINTENANCE SHALL BE PAID FOR UNDER ITEM #0219011A - "SEDIMENTATION CONTROL SYSTEM AT CATCH BASIN".

**SEDIMENTATION CONTROL AT CATCH BASIN**



**SEDIMENTATION CONTROL SYSTEM (SILT FENCE)**



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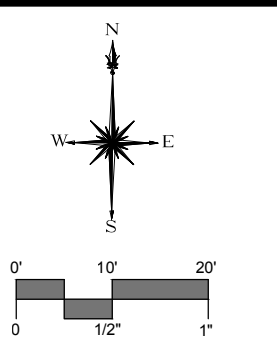
DESCRIPTION	DATE	BY

MISCELLANEOUS DETAILS  
RECONSTRUCTION OF CROSS STREET  
NAUGATUCK, CONNECTICUT

TDS DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE: N.T.S.		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: MDS-06		



MILONE & MACBROOM, INC. 2018

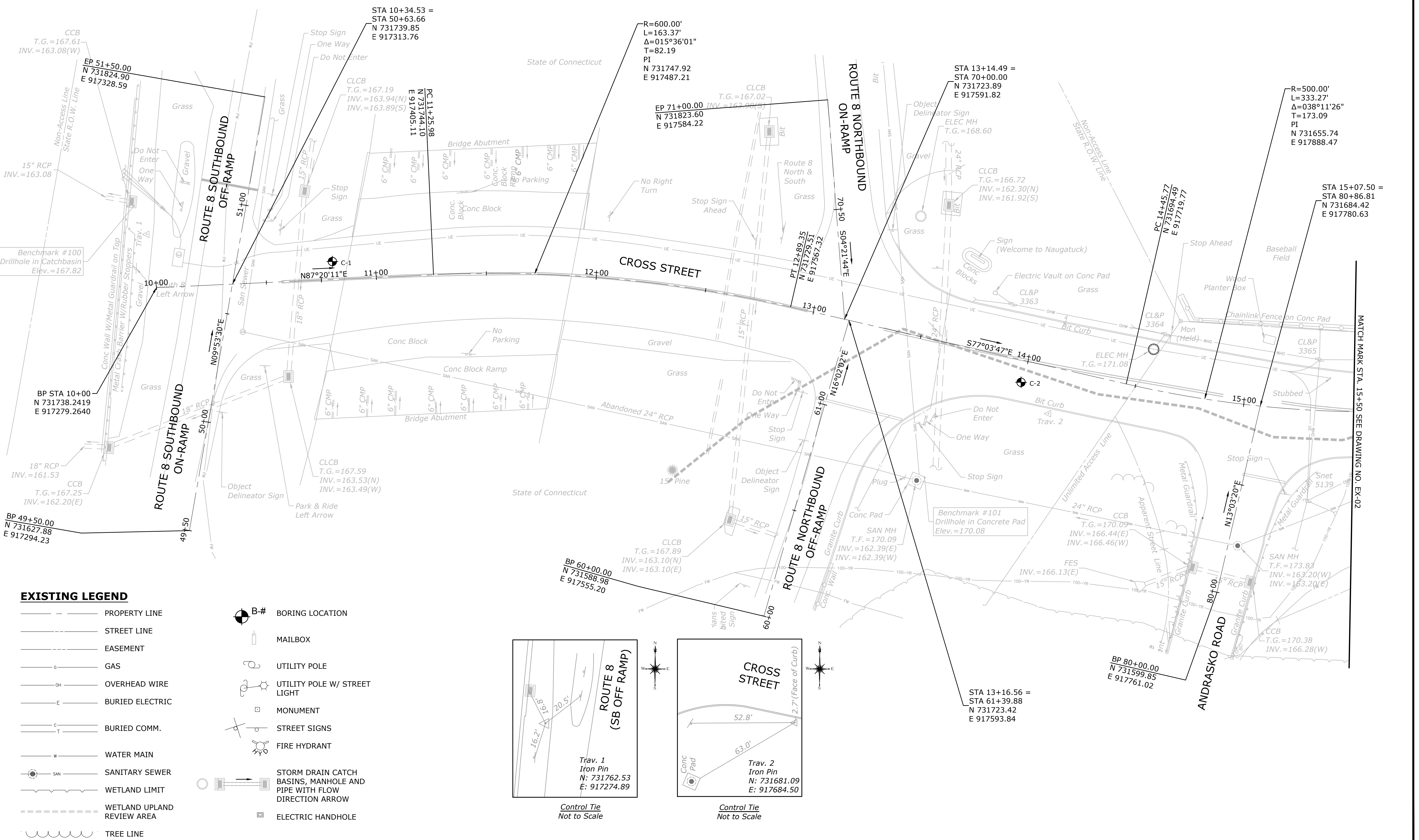


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DESCRIPTION	DATE	BY

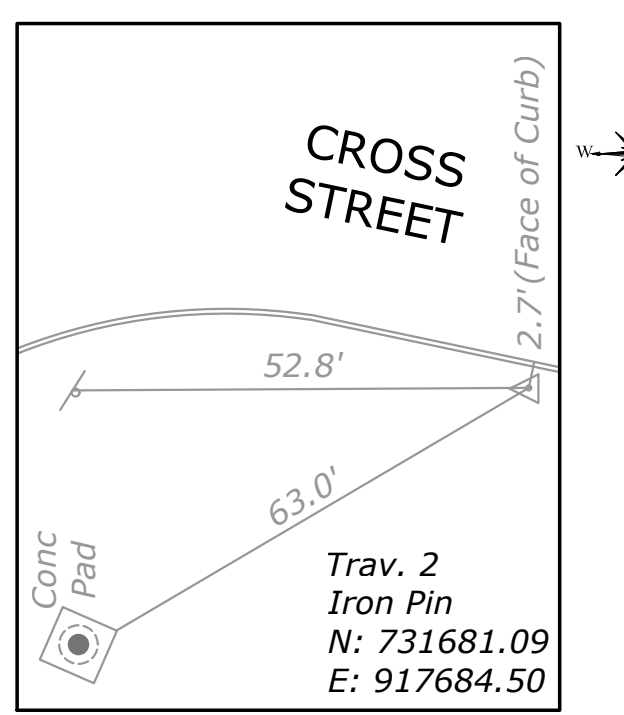
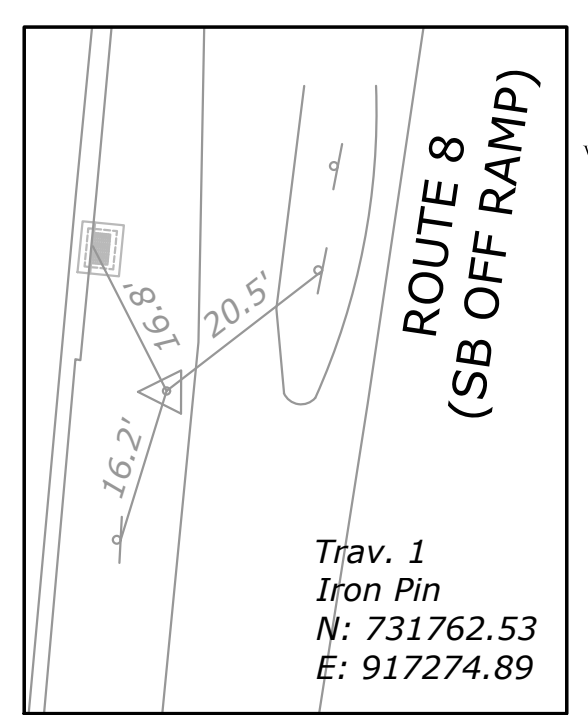
**EXISTING CONDITIONS & BASELINE LAYOUT PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: EX-01		
<b>11</b>		



**EXISTING LEGEND**

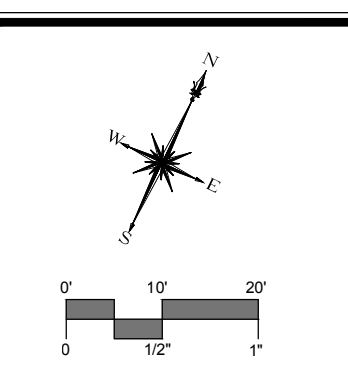
- PROPERTY LINE
- STREET LINE
- EASEMENT
- GAS
- OVERHEAD WIRE
- BURIED ELECTRIC
- BURIED COMM.
- WATER MAIN
- SANITARY SEWER
- WETLAND LIMIT
- WETLAND UPLAND REVIEW AREA
- TREE LINE
- B-# BORING LOCATION
- MAILBOX
- UTILITY POLE
- UTILITY POLE W/ STREET LIGHT
- MONUMENT
- STREET SIGNS
- FIRE HYDRANT
- STORM DRAIN CATCH BASINS, MANHOLE AND PIPE WITH FLOW DIRECTION ARROW
- ELECTRIC HANDHOLE



MATCH MARK STA. 15+50 SEE DRAWING NO. EX-02



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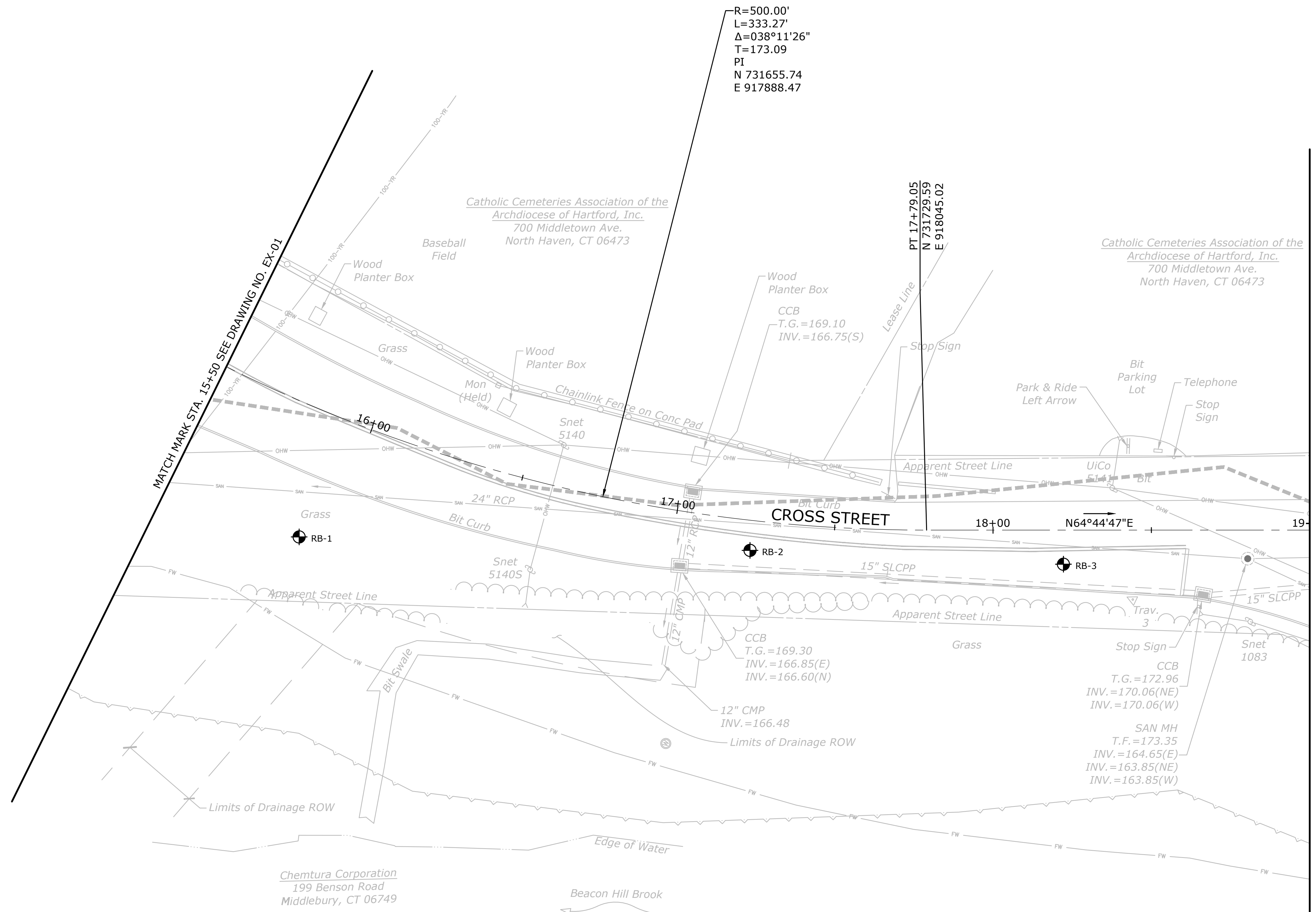
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DESCRIPTION	DATE	BY

**EXISTING CONDITIONS & BASELINE LAYOUT PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. EX-02		
12		

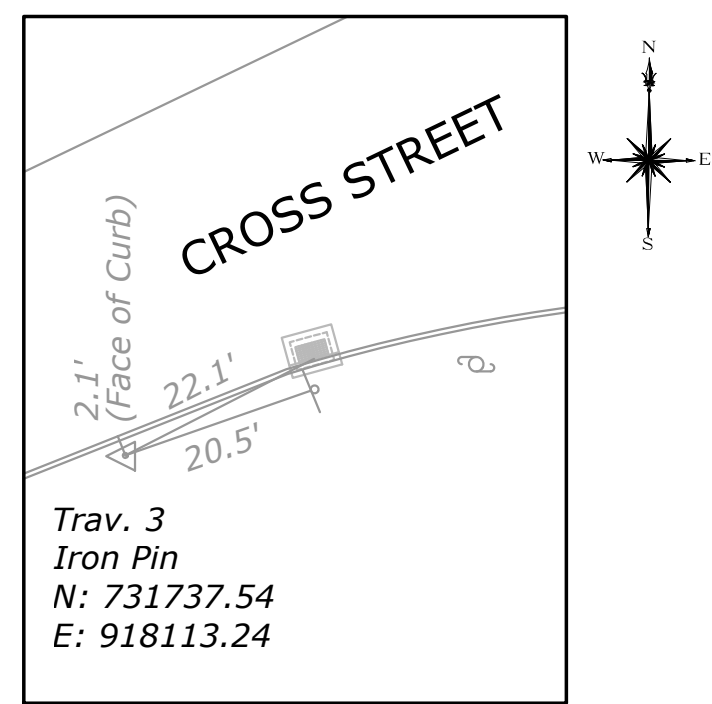
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**EXISTING LEGEND**

- — — — — PROPERTY LINE
- — — — — STREET LINE
- — — — — EASEMENT
- G — — — GAS
- OH — — — OVERHEAD WIRE
- E — — — BURIED ELECTRIC
- C — — — BURIED COMM.
- W — — — WATER MAIN
- SAN — — — SANITARY SEWER
- — — — — WETLAND LIMIT
- — — — — WETLAND UPLAND REVIEW AREA
- — — — — TREE LINE

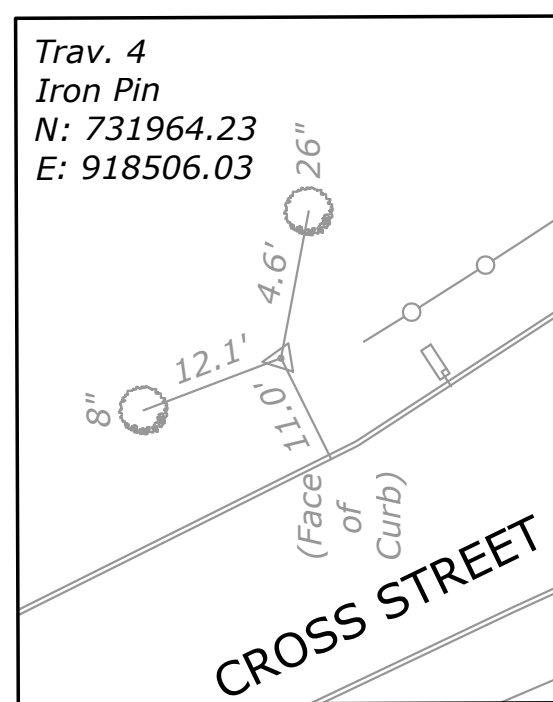
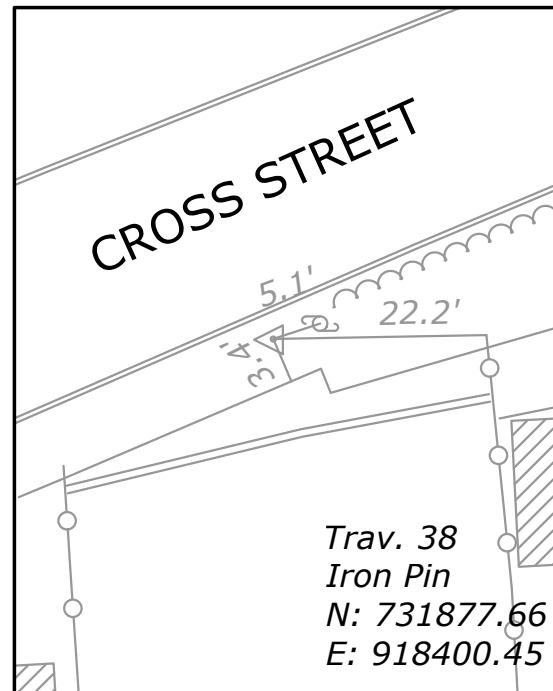
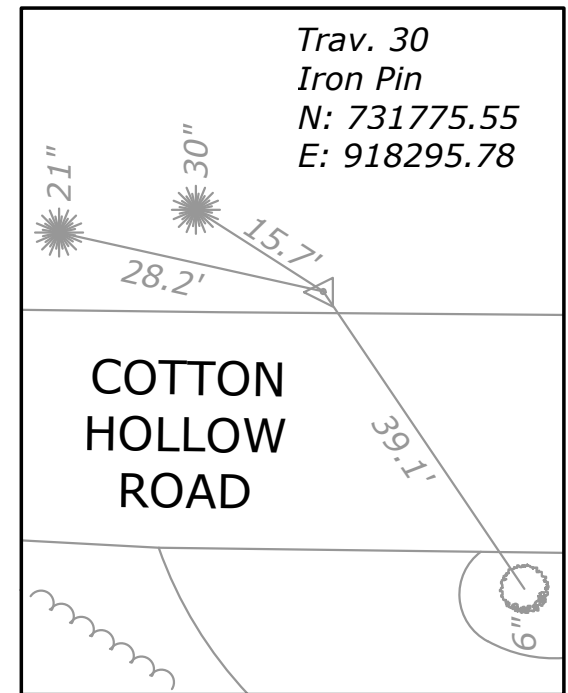
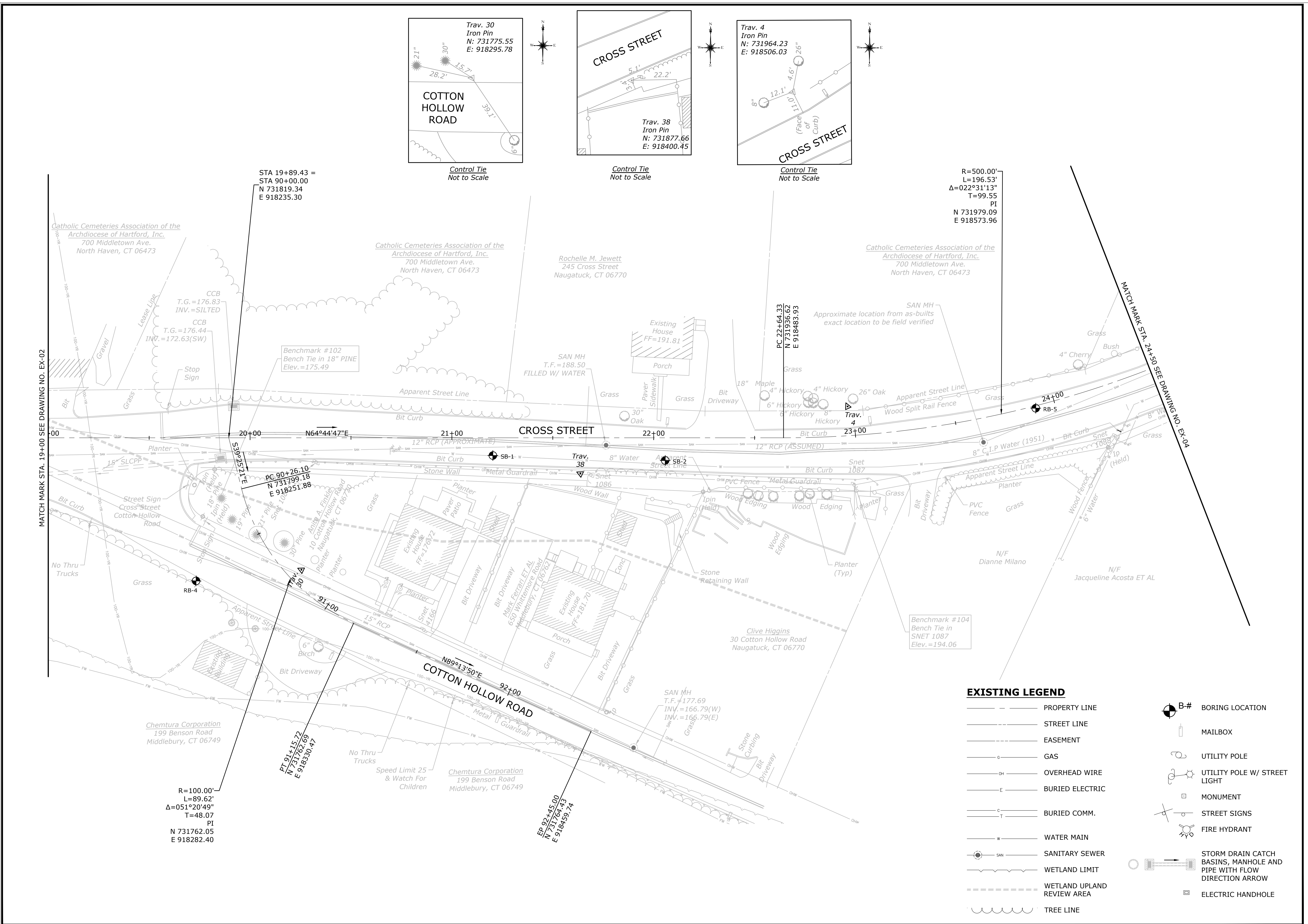
- ⊙ B-# BORING LOCATION
- ☐ MAILBOX
- ⊙ UTILITY POLE
- ⊙ UTILITY POLE W/ STREET LIGHT
- ⊙ MONUMENT
- ⊙ STREET SIGNS
- ⊙ FIRE HYDRANT
- ☐ STORM DRAIN CATCH BASINS, MANHOLE AND PIPE WITH FLOW DIRECTION ARROW
- ☐ ELECTRIC HANDHOLE



Control Tie  
Not to Scale



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R=500.00'  
L=196.53'  
Δ=022°31'13"  
T=99.55  
PI  
N 731979.09  
E 918573.96

R=100.00'  
L=89.62'  
Δ=051°20'49"  
T=48.07  
PI  
N 731762.05  
E 918282.40

STA 19+89.43 =  
STA 90+00.00  
N 731819.34  
E 918235.30

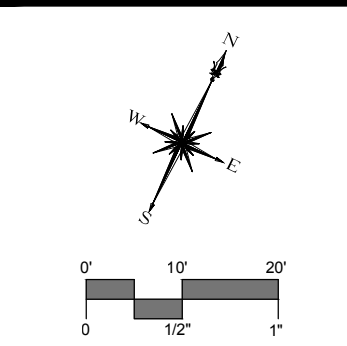
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N 731799.18  
E 918251.88

PT 91+15.72  
N 731762.69  
E 918330.47

EP 92+45.00  
N 731764.43  
E 918459.74

**EXISTING LEGEND**

- — — — — PROPERTY LINE
- — — — — STREET LINE
- — — — — EASEMENT
- G — — — — GAS
- OH — — — — OVERHEAD WIRE
- E — — — — BURIED ELECTRIC
- C — — — — BURIED COMM.
- W — — — — WATER MAIN
- SAN — — — — SANITARY SEWER
- WETLAND LIMIT
- WETLAND UPLAND REVIEW AREA
- — — — — TREE LINE
- ⊙ B-# BORING LOCATION
- MAILBOX
- UTILITY POLE
- UTILITY POLE W/ STREET LIGHT
- MONUMENT
- ⊙ STREET SIGNS
- ⊙ FIRE HYDRANT
- STORM DRAIN CATCH BASINS, MANHOLE AND PIPE WITH FLOW DIRECTION ARROW
- ELECTRIC HANDHOLE



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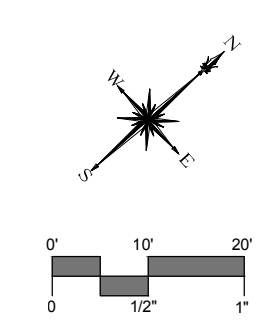
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**EXISTING CONDITIONS & BASELINE LAYOUT PLAN  
RECONSTRUCTION OF CROSS STREET  
NAUGATUCK, CONNECTICUT**

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. EX-03		
SHEET NO. <b>13</b>		



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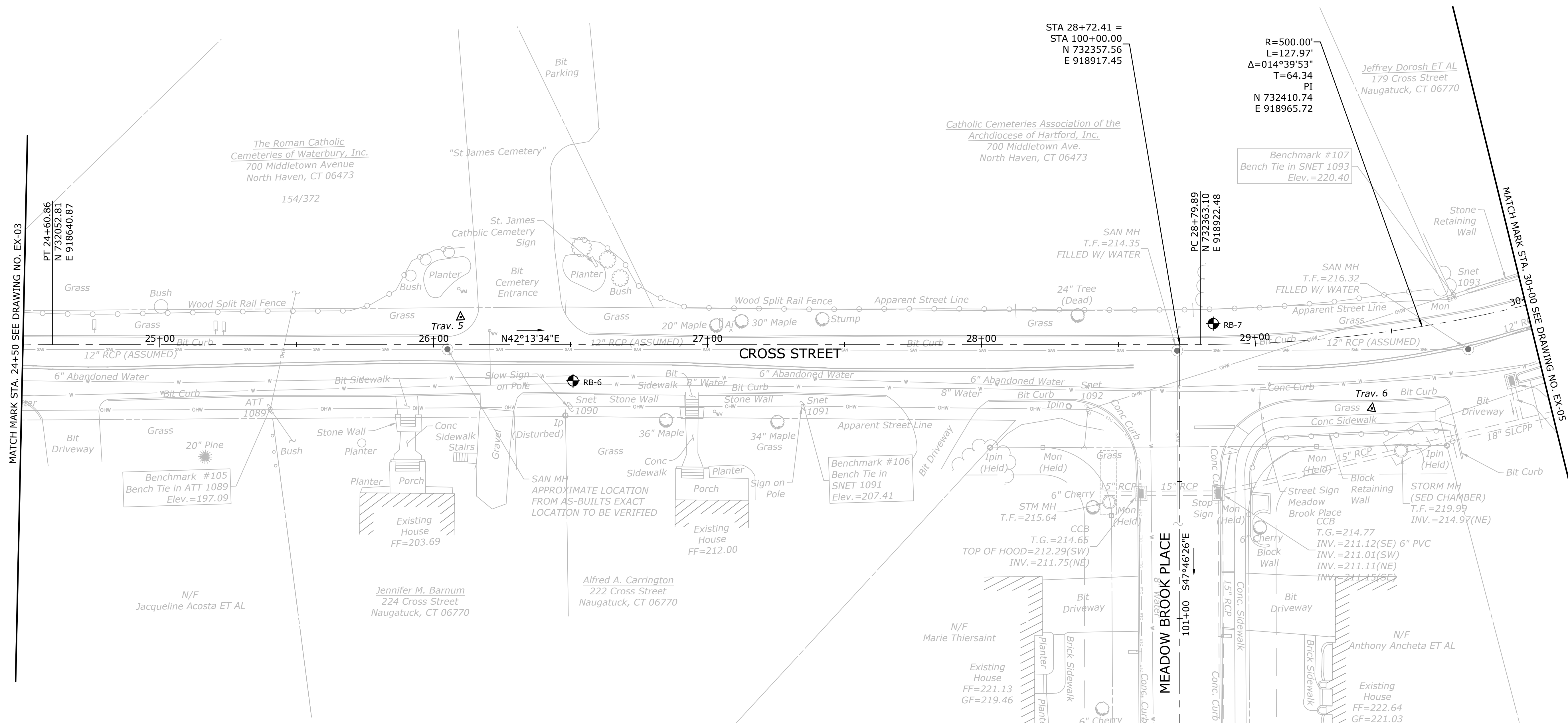


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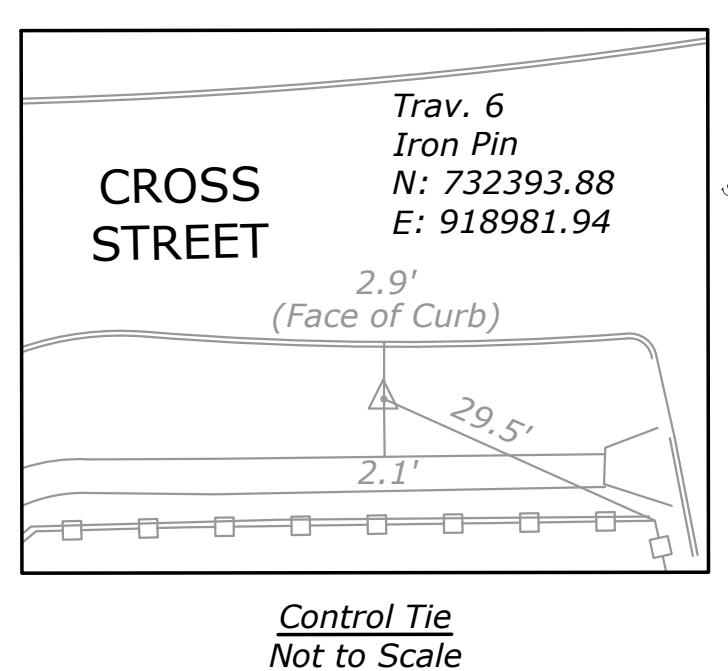
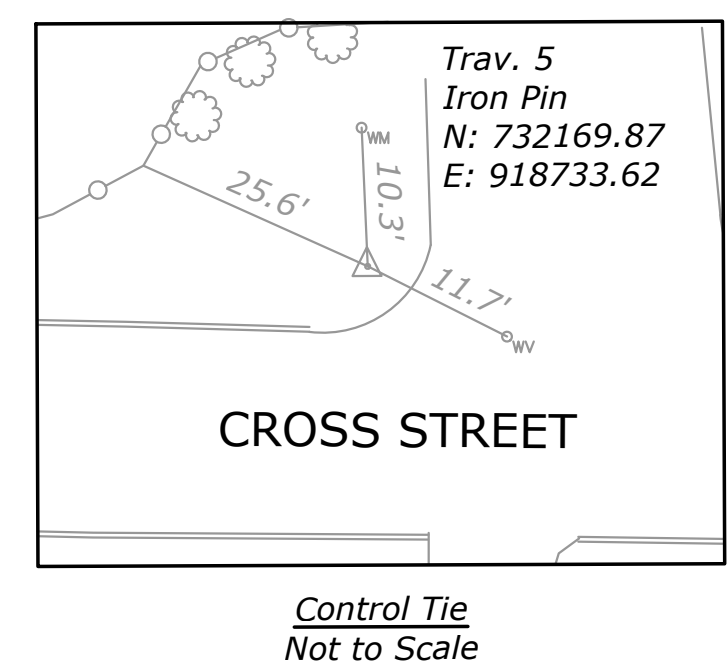
DESCRIPTION	DATE	BY

**EXISTING CONDITIONS & BASELINE LAYOUT PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: EX-04		
<b>14</b>		

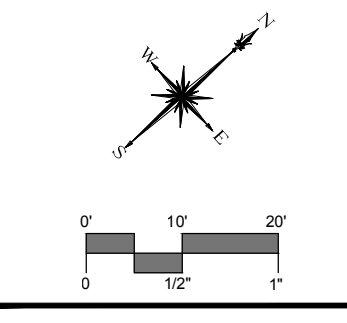


- EXISTING LEGEND**
- PROPERTY LINE
  - STREET LINE
  - EASEMENT
  - GAS
  - OH --- OVERHEAD WIRE
  - E --- BURIED ELECTRIC
  - C --- BURIED COMM.
  - W --- WATER MAIN
  - SAN --- SANITARY SEWER
  - WETLAND LIMIT
  - WETLAND UPLAND REVIEW AREA
  - TREE LINE
- B#** BORING LOCATION
  - MAILBOX
  - UTILITY POLE
  - UTILITY POLE W/ STREET LIGHT
  - MONUMENT
  - STREET SIGNS
  - FIRE HYDRANT
  - STORM DRAIN CATCH BASINS, MANHOLE AND PIPE WITH FLOW DIRECTION ARROW
  - ELECTRIC HANDHOLE





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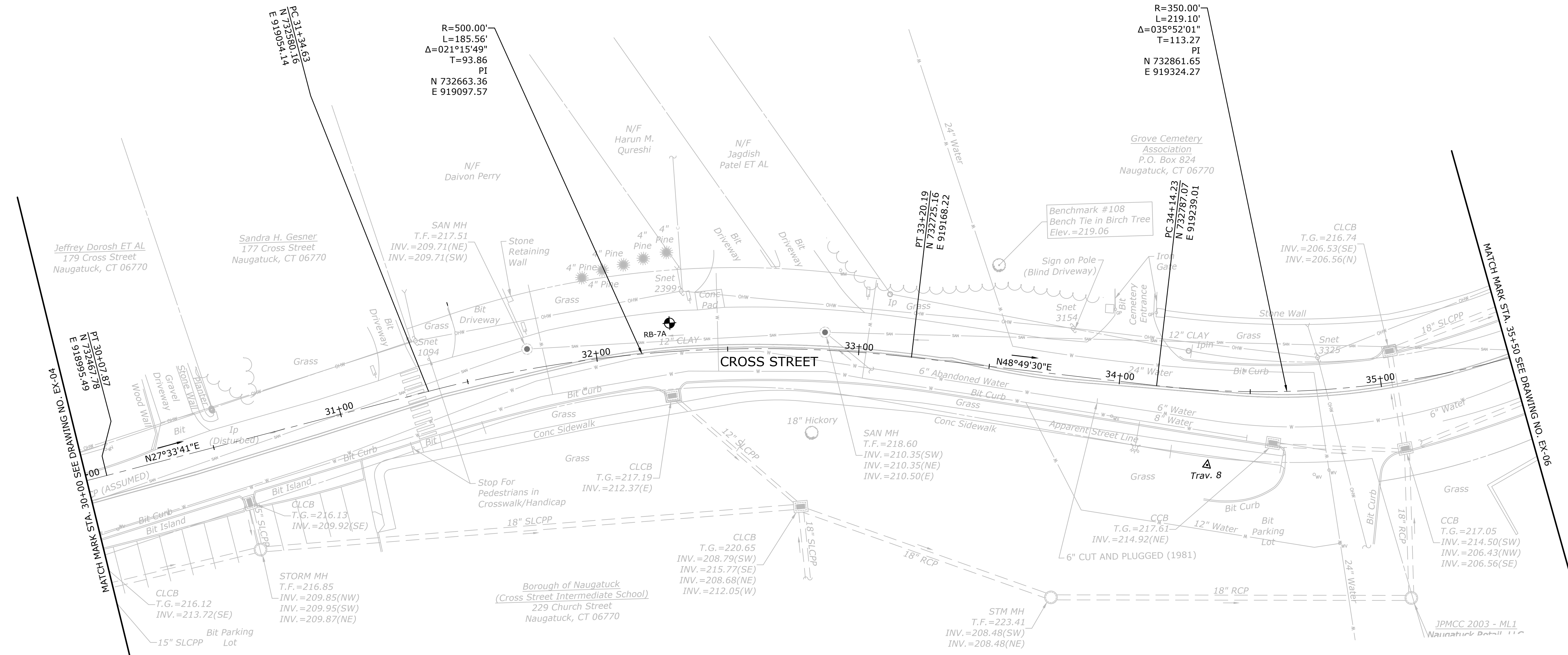


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DESCRIPTION	DATE	BY

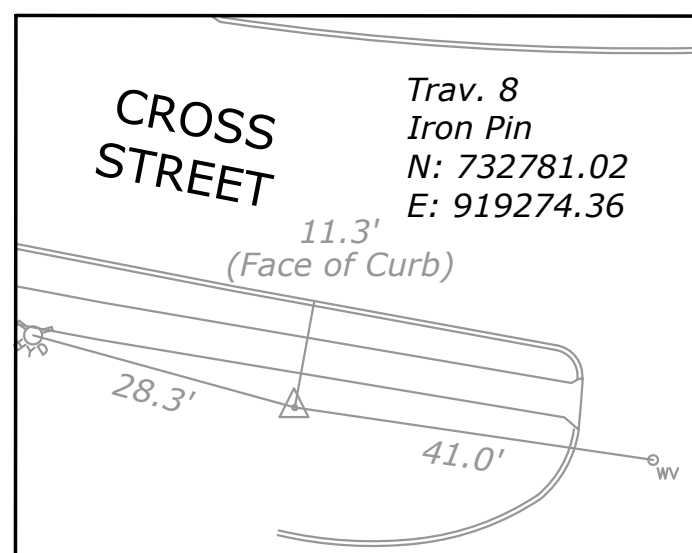
**EXISTING CONDITIONS & BASELINE LAYOUT PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: EX-05		
<b>15</b>		



**EXISTING LEGEND**

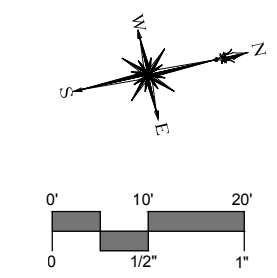
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| —     | PROPERTY LINE              | B-#      | BORING LOCATION  |
| ---   | STREET LINE                | [Symbol] | MAILBOX  |
| ---   | EASEMENT                   | [Symbol] | UTILITY POLE   |
| —G—   | GAS                        | [Symbol] | UTILITY POLE W/ STREET LIGHT   |
| —OH—  | OVERHEAD WIRE              | [Symbol] | MONUMENT   |
| —E—   | BURIED ELECTRIC            | [Symbol] | STREET SIGNS   |
| —C—   | BURIED COMM.               | [Symbol] | FIRE HYDRANT   |
| —W—   | WATER MAIN                 | [Symbol] | STORM DRAIN CATCH BASINS, MANHOLE AND PIPE WITH FLOW DIRECTION ARROW |
| —SAN— | SANITARY SEWER             | [Symbol] | ELECTRIC HANDHOLE  |
| ---   | WETLAND LIMIT              |          |  |
| ---   | WETLAND UPLAND REVIEW AREA |          |  |
| ~~~~~ | TREE LINE                  |          |  |



Control Tie  
Not to Scale



WHERE L, S, OR MAPPING IS THE SAME AS SHOWN ON ANY OF THE ABOVE-REFERENCED DRAWINGS, THE SAME SHALL BE USED UNLESS OTHERWISE SPECIFIED.



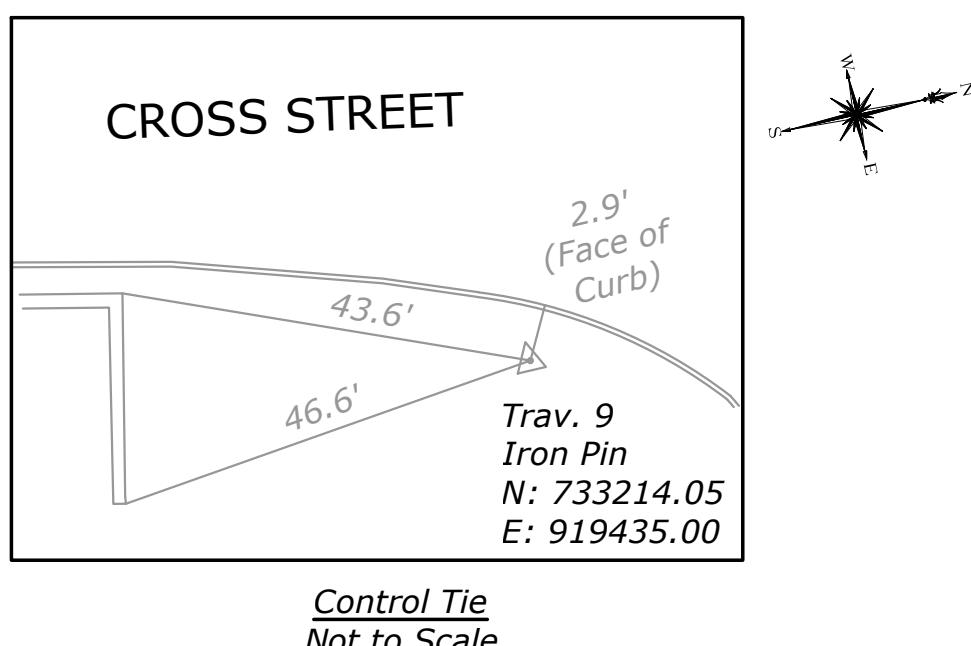
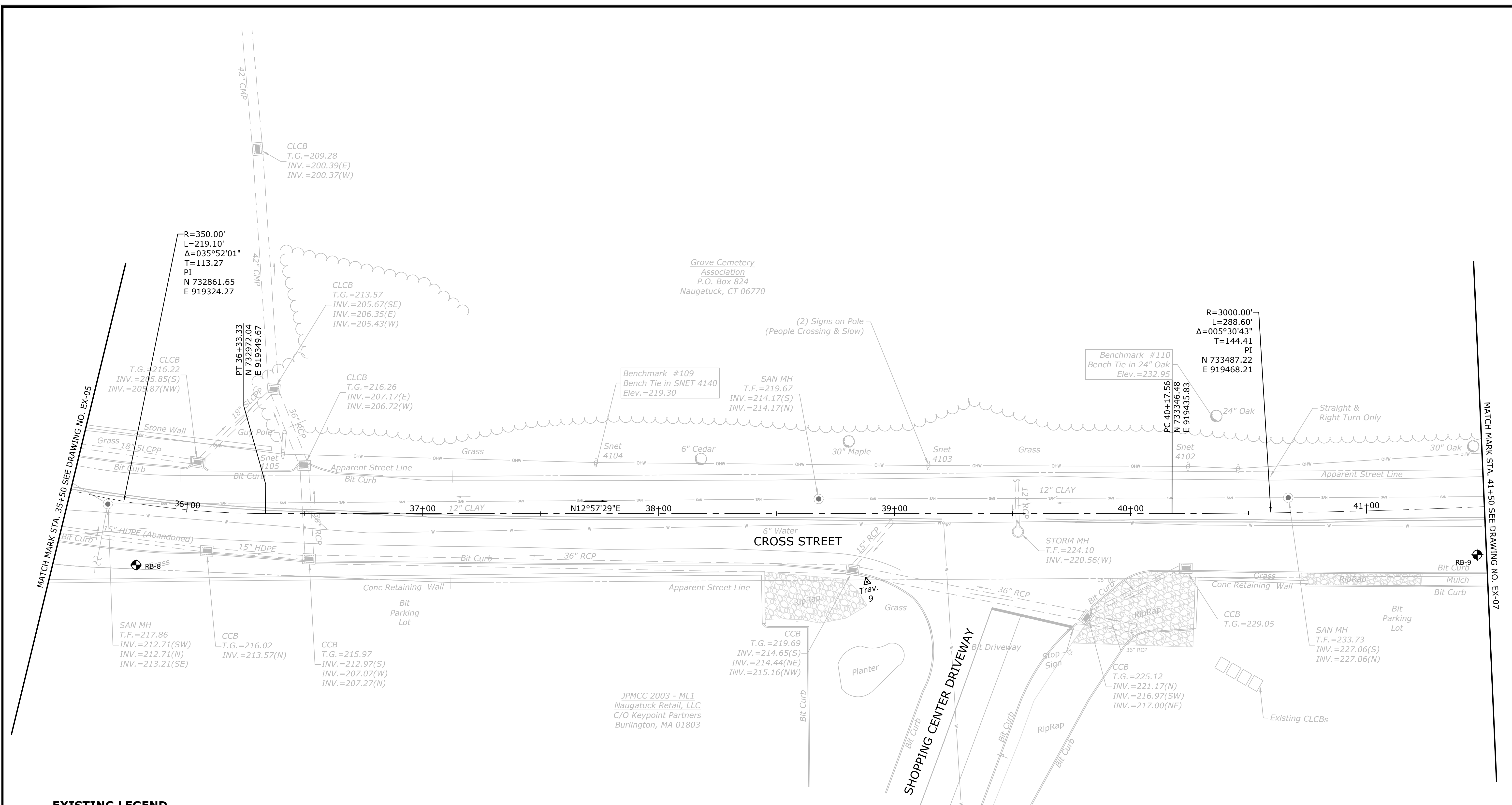
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DESCRIPTION	DATE	BY

**EXISTING CONDITIONS & BASELINE LAYOUT PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

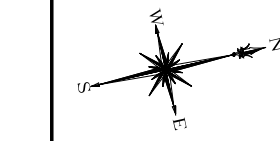
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DESIGNED	DRAWN	CHECKED
SCALE		
1"=20'		
DATE		
SEPTEMBER 7, 2018		
PROJECT NO.		
87-145		
DRAWING NO.		
EX-06		

**16**  
 SHEET NO.



Grove Cemetery  
 Association  
 P.O. Box 824  
 Naugatuck, CT 06770

JPMCC 2003 - MLI  
 Naugatuck Retail, LLC  
 C/O Keypoint Partners  
 Burlington, MA 01803









SHEET 18 - BORING LOGS FOR THE RECONSTRUCTION OF CROSS STREET IN NAUGATUCK, CONNECTICUT  
 PREPARED BY: T. MCGOVERN, ENGINEER  
 DATE: 09/07/2018

Connecticut DOT Boring Report										
Driller: T. McGovern		Town: Naugatuck, Connecticut			Hole No.: C-1					
Inspector: Thiet Ta		Project No.: 2014-0803			Stat./Offset: Northing: 731750.11'					
Engineer: N. Whetten		Route No.: Cross Street			Easting: 917359.17'					
Start Date: 9-29-15		Bridge No.:			Surface Elevation:					
Finish Date: 9-29-15		Project Description: Cross Street Reconstruction								
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID			Core Barrel Type:					
Hammer Wt.: Fall: In.		Hammer Wt.: 140			Fall: 30in.					
Groundwater Observations: @not encountered										
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)		
0	S-1	26 53 35 29	24	12		ASPHALT (3") GRAVEL BASE (6") FILL	Brown, c-f SAND, some gravel, trace silt	0		
5							END OF BORING 2.5ft	2.5		
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%										
Total Penetration in Earth: 2.5ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.					Sheet 1 of 1	
No. of Soil Samples: 1		No. of Core Runs: ---							SM-001-M REV. 1/02	

Connecticut DOT Boring Report										
Driller: T. McGovern		Town: Naugatuck, Connecticut			Hole No.: C-2					
Inspector: Thiet Ta		Project No.: 2014-0803			Stat./Offset: Northing: 731695.25'					
Engineer: N. Whetten		Route No.: Cross Street			Easting: 917672.00'					
Start Date: 9-29-15		Bridge No.:			Surface Elevation:					
Finish Date: 9-29-15		Project Description: Cross Street Reconstruction								
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID			Core Barrel Type:					
Hammer Wt.: Fall: In.		Hammer Wt.: 140			Fall: 30in.					
Groundwater Observations: @not encountered										
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)		
0	S-1	21 42 30 40	24	12		ASPHALT (3") GRAVEL BASE (6") FILL	Brown, c-f GRAVEL and c-f SAND, trace silt	0		
5							END OF BORING 2.5ft	2.5		
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%										
Total Penetration in Earth: 2.5ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.					Sheet 1 of 1	
No. of Soil Samples: 1		No. of Core Runs: ---							SM-001-M REV. 1/02	

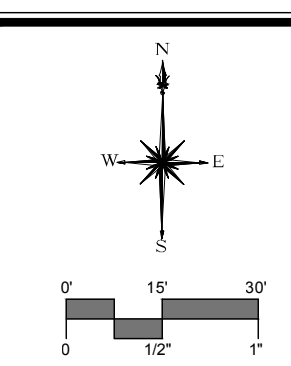
Connecticut DOT Boring Report										
Driller: T. McGovern		Town: Naugatuck, Connecticut			Hole No.: RB-1					
Inspector: Thiet Ta		Project No.: 2014-0803			Stat./Offset: Northing: 731643.12'					
Engineer: N. Whetten		Route No.: Cross Street			Easting: 917866.75'					
Start Date: 9-18-15		Bridge No.:			Surface Elevation:					
Finish Date: 9-18-15		Project Description: Cross Street Reconstruction								
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID			Core Barrel Type:					
Hammer Wt.: Fall: In.		Hammer Wt.: 140			Fall: 30in.					
Groundwater Observations: @not encountered										
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)		
0						TOPSOIL (4") FILL		0		
1	S-1	6 11 16 18	24	16			Brown, c-f SAND, some gravel, little silt	1		
2	S-2	13 36 50 45	24	18			Brown, c-f SAND, little gravel, little silt	2		
3	S-3	50/3"		3			Brown, c-f SAND, some gravel, trace silt, with cobbles and boulders	3		
4	S-4	22 50/5"		9			Brown, c-f SAND, some gravel, trace silt, with cobbles and boulders	4		
5							END OF BORING 8.9ft	8.9		
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%										
Total Penetration in Earth: 8.9ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead. Boring terminated on top of an existing sewer line					Sheet 1 of 1	
No. of Soil Samples: 4		No. of Core Runs: ---							SM-001-M REV. 1/02	

Connecticut DOT Boring Report										
Driller: T. McGovern		Town: Naugatuck, Connecticut			Hole No.: RB-2					
Inspector: Thiet Ta		Project No.: 2014-0803			Stat./Offset: Northing: 731700.01'					
Engineer: N. Whetten		Route No.: Cross Street			Easting: 917997.32'					
Start Date: 9-16-15		Bridge No.:			Surface Elevation:					
Finish Date: 9-16-15		Project Description: Cross Street Reconstruction								
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID			Core Barrel Type:					
Hammer Wt.: Fall: In.		Hammer Wt.: 140			Fall: 30in.					
Groundwater Observations: @not encountered										
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)		
0	S-1	3 10 14 18	24	16		TOPSOIL (4") FILL (5)	Brown, c-f SAND, little gravel, little silt	0		
1	S-2	18 50/4"	10	6			Brown, c-f SAND, some gravel, trace silt, with cobbles and boulders	1		
5	S-3	22 50/4"	10	8		GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, some gravel, trace silt, with cobbles and boulders	5		
8	S-4	50/2"	2	0			Brown, c-f SAND, some gravel, trace silt, with cobbles and boulders	8		
10							END OF BORING 8.2ft	8.2		
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%										
Total Penetration in Earth: 8.2ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead. Encountered frequent cobbles and boulders while drilling					Sheet 1 of 1	
No. of Soil Samples: 4		No. of Core Runs: ---							SM-001-M REV. 1/02	

Connecticut DOT Boring Report										
Driller: T. McGovern		Town: Naugatuck, Connecticut			Hole No.: RB-3					
Inspector: Thiet Ta		Project No.: 2014-0803			Stat./Offset: Northing: 731738.27'					
Engineer: N. Whetten		Route No.: Cross Street			Easting: 918088.68'					
Start Date: 9-29-15		Bridge No.:			Surface Elevation:					
Finish Date: 9-29-15		Project Description: Cross Street Reconstruction								
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID			Core Barrel Type:					
Hammer Wt.: Fall: In.		Hammer Wt.: 140			Fall: 30in.					
Groundwater Observations: @not encountered										
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)		
0						ASHPALT (6") FILL (5)		0		
1	S-1	2 2 3 5	24	14			Brown, c-f SAND, little silt, with organic roots	1		
5	S-2	7 11 48 52	24	12		GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, little gravel, little silt, with organic roots and branches	5		
8	S-3	40 60 90	18	4			Brown, c-f SAND, little gravel, little silt, with cobbles	8		
10							END OF BORING 7.5ft	7.5		
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%										
Total Penetration in Earth: 7.5ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead. Cobbles and boulders directly beneath asphalt					Sheet 1 of 1	
No. of Soil Samples: 3		No. of Core Runs: ---							SM-001-M REV. 1/02	

Connecticut DOT Boring Report										
Driller: T. McGovern		Town: Naugatuck, Connecticut			Hole No.: RB-4					
Inspector: Thiet Ta		Project No.: 2014-0803			Stat./Offset: Northing: 731748.27'					
Engineer: N. Whetten		Route No.: Cotton Hollow Road			Easting: 918250.92'					
Start Date: 9-29-15		Bridge No.:			Surface Elevation:					
Finish Date: 9-29-15		Project Description: Cross Street Reconstruction								
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID			Core Barrel Type:					
Hammer Wt.: Fall: In.		Hammer Wt.: 140			Fall: 30in.					
Groundwater Observations: @not encountered										
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)		
0						ASHPALT (4") FILL (5)		0		
1	S-1	6 5 6 6	24	12			Black to brown, f-SAND, trace silt	1		
2	S-2	5 5 5 7	24	18			Brown, f-SAND, trace silt	2		
5	S-3	50		6		GLACIO-FLUVIAL DEPOSIT	Brown to tan, c-f sand, trace gravel, trace silt, with cobbles and boulders	5		
6							END OF BORING 6ft	6		
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%										
Total Penetration in Earth: 6ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead. Encountered cobbles and boulders at 5 feet Auger refusal on boulders					Sheet 1 of 1	
No. of Soil Samples: 3		No. of Core Runs: ---							SM-001-M REV. 1/02	

NOTE: FOR BORING LOCATIONS SEE DRAWINGS EX-01 TO EX-07



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DESCRIPTION	DATE	BY

**BORING LOGS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

TDS DESIGNED	TDS DRAWN	---	---
SCALE: N.T.S.			
DATE: SEPTEMBER 7, 2018			
PROJECT NO.: 87-145			
DRAWING NO.: BOR-01			
<b>18</b>			

SHEET NO.



SHEET 1 - 19 BORING LOGS FOR THE CROSS STREET RECONSTRUCTION PROJECT, NAUGATUCK, CONNECTICUT  
 DATE: 08/20/2018  
 DRAWN BY: T.M.

Connecticut DOT Boring Report											
Driller: T. MCGOVERN		Town: NAUGATUCK, CONNECTICUT		Hole No.: RB-5							
Inspector: Thiet Ta		Project No.: 2014-0803		Stat./Offset: Northing:732003.20'							
Engineer: N. Whetten		Route No.: Cross Street		Easting:918590.68'							
Start Date: 9-17-15		Bridge No.:		Surface Elevation:							
Finish Date: 9-17-15		Project Description: Cross Street Reconstruction									
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID		Core Barrel Type:							
Hammer Wt.: Fall: In.		Hammer Wt.: 140		Fall: 30in.							
Groundwater Observations: @not encountered											
Depth (ft)	SAMPLES					Generalized Strata Description	Material Description and Notes	Elevation (ft)			
	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RCD %						
0						TOPSOIL (4") FILL (1.5") GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, some gravel, little silt, with cobbles and boulders				
5	S-2	35 50/3"	9	3			Brown, c-f SAND, some gravel, little silt, with cobbles and boulders				
6							END OF BORING 6ft				
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%											
Total Penetration in Earth: 6ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.						Sheet 1 of 1	
No. of Soil Samples: 2		No. of Core Runs: ---		Encountered frequent cobbles and boulders while drilling Auger refusal on boulders							

Connecticut DOT Boring Report											
Driller: T. MCGOVERN		Town: NAUGATUCK, CONNECTICUT		Hole No.: RB-6							
Inspector: Thiet Ta		Project No.: 2014-0803		Stat./Offset: Northing:732184.57'							
Engineer: N. Whetten		Route No.: Cross Street		Easting:918778.42'							
Start Date: 9-17-15		Bridge No.:		Surface Elevation:							
Finish Date: 9-17-15		Project Description: Cross Street Reconstruction									
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID		Core Barrel Type:							
Hammer Wt.: Fall: In.		Hammer Wt.: 140		Fall: 30in.							
Groundwater Observations: @not encountered											
Depth (ft)	SAMPLES					Generalized Strata Description	Material Description and Notes	Elevation (ft)			
	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RCD %						
0						ASHPALT (8") FILL (3")	Brown, c-f SAND, trace gravel, some silt				
5	S-2	20 19 19 18	24	1		GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, little gravel, trace silt, with cobbles				
5	S-3	1 6 50/4"	16	8			Brown, c-f SAND, trace gravel, trace silt				
7							END OF BORING 7ft				
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%											
Total Penetration in Earth: 7ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.						Sheet 1 of 1	
No. of Soil Samples: 3		No. of Core Runs: ---		Auger refusal on boulders							

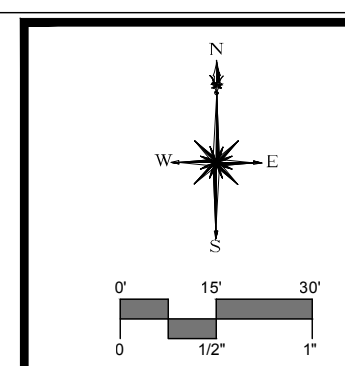
Connecticut DOT Boring Report											
Driller: T. MCGOVERN		Town: NAUGATUCK, CONNECTICUT		Hole No.: RB-7							
Inspector: Thiet Ta		Project No.: 2014-0803		Stat./Offset: Northing:732371.75'							
Engineer: N. Whetten		Route No.: Cross Street		Easting:918920.06'							
Start Date: 9-29-15		Bridge No.:		Surface Elevation:							
Finish Date: 9-29-15		Project Description: Cross Street Reconstruction									
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID		Core Barrel Type:							
Hammer Wt.: Fall: In.		Hammer Wt.: 140		Fall: 30in.							
Groundwater Observations: @not encountered											
Depth (ft)	SAMPLES					Generalized Strata Description	Material Description and Notes	Elevation (ft)			
	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RCD %						
0						ASHPALT (11") FILL (6")	Dark Brown, c-f SAND, little gravel, trace silt, with cobbles and boulders				
5	S-2	20 20 11 8	24	6			Brown, c-f SAND, trace gravel, little silt, with organic roots				
5	S-3	14 17 12 12	24	6		GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, trace gravel, little silt, with organic roots				
5	S-4	12 12 12 18	24	1			Brown, c-f SAND, little gravel, trace silt				
9							END OF BORING 9ft				
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%											
Total Penetration in Earth: 9ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.						Sheet 1 of 1	
No. of Soil Samples: 4		No. of Core Runs: ---		Encountered frequent cobbles while drilling Auger refusal on boulders							

Connecticut DOT Boring Report											
Driller: T. MCGOVERN		Town: NAUGATUCK, CONNECTICUT		Hole No.: RB-7A							
Inspector: Thiet Ta		Project No.: 2014-0803		Stat./Offset: Northing:732685.61'							
Engineer: N. Whetten		Route No.: Cross Street		Easting:919096.59'							
Start Date: 9-29-15		Bridge No.:		Surface Elevation:							
Finish Date: 9-29-15		Project Description: Cross Street Reconstruction									
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID		Core Barrel Type:							
Hammer Wt.: Fall: In.		Hammer Wt.: 140		Fall: 30in.							
Groundwater Observations: @not encountered											
Depth (ft)	SAMPLES					Generalized Strata Description	Material Description and Notes	Elevation (ft)			
	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RCD %						
0						ASHPALT (7") FILL (5")	Brown, c-f SAND, little gravel, little silt (from auger cuttings)				
5	S-1	9 12 18 59	24	0			Brown, c-f SAND, little gravel, little silt, with cobbles and boulders				
5	S-2	50	6	0			Brown, c-f SAND, little gravel, little silt, with cobbles and boulders				
5	S-3	30 47 70	18	12		GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, some gravel, trace silt, with cobbles and boulders				
6.5							END OF BORING 6.5ft				
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%											
Total Penetration in Earth: 6.5ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.						Sheet 1 of 1	
No. of Soil Samples: 3		No. of Core Runs: ---		Encountered frequent cobbles and boulders while drilling							

Connecticut DOT Boring Report											
Driller: T. MCGOVERN		Town: NAUGATUCK, CONNECTICUT		Hole No.: RB-8							
Inspector: Thiet Ta		Project No.: 2014-0803		Stat./Offset: Northing:732913.70'							
Engineer: N. Whetten		Route No.: Cross Street		Easting:919358.62'							
Start Date: 9-17-15		Bridge No.:		Surface Elevation:							
Finish Date: 9-17-15		Project Description: Cross Street Reconstruction									
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID		Core Barrel Type:							
Hammer Wt.: Fall: In.		Hammer Wt.: 140		Fall: 30in.							
Groundwater Observations: @not encountered											
Depth (ft)	SAMPLES					Generalized Strata Description	Material Description and Notes	Elevation (ft)			
	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RCD %						
0						ASHPALT (8") FILL (5")	Brown, c-f SAND, some gravel, little silt				
5	S-1	10 10 10 27	24	12			Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
5	S-2	50/5"	5	0			Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
5	S-3	36 50/1"	7	4		GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
10	S-4	50/2"	2	4			Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
15	S-5	50/3"	3	4			Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
15.3							END OF BORING 15.3ft				
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%											
Total Penetration in Earth: 15.3ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.						Sheet 1 of 1	
No. of Soil Samples: 5		No. of Core Runs: ---		Encountered frequent cobbles and boulders while drilling							

Connecticut DOT Boring Report											
Driller: T. MCGOVERN		Town: NAUGATUCK, CONNECTICUT		Hole No.: RB-9							
Inspector: Thiet Ta		Project No.: 2014-0803		Stat./Offset: Northing:733468.57'							
Engineer: N. Whetten		Route No.: Cross Street		Easting:919481.84'							
Start Date: 9-16-15		Bridge No.:		Surface Elevation:							
Finish Date: 9-16-15		Project Description: Cross Street Reconstruction									
Casing Size/Type: 4 1/4" HSA		Sampler Type/Size: 1-3/8 inch ID		Core Barrel Type:							
Hammer Wt.: Fall: In.		Hammer Wt.: 140		Fall: 30in.							
Groundwater Observations: @not encountered											
Depth (ft)	SAMPLES					Generalized Strata Description	Material Description and Notes	Elevation (ft)			
	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RCD %						
0						ASHPALT (8") FILL (5")	Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
5	S-1	22 50/3"	9	4			Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
5	S-2	24 65 50/3"	15	8			Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
5	S-3	50	6	0		GLACIO-FLUVIAL DEPOSIT	Brown, c-f SAND, and GRAVEL, trace silt, with cobbles and boulders				
6.5							END OF BORING 6.5ft				
Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%											
Total Penetration in Earth: 6.5ft		Rock: ft		NOTES: Samples taken with a 2" O.D. split spoon sampler driven by a hammer operated by a rope and cathead.						Sheet 1 of 1	
No. of Soil Samples: 3		No. of Core Runs: ---		Encountered frequent cobbles and boulders while drilling Auger refusal on boulders							

NOTE: FOR BORING LOCATIONS SEE DRAWINGS EX-01 TO EX-07



**MILONE & MACBROOM**  
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 Cheshire, Connecticut 06410  
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 www.miloneandmacbroom.com

DESCRIPTION	DATE	BY

**BORING LOGS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

TDS DESIGNED	TDS DRAWN	---	CHECKED
SCALE: N.T.S.			
DATE: SEPTEMBER 7, 2018			
PROJECT NO.: 87-145			
DRAWING NO.: BOR-02			

**19**  
 SHEET NO.







MSCALE: 1"=20' (SEE DRAWING NO. 100-02 FOR DETAILS)  
 DATE: 07/2018  
 PROJECT NO.: 87-145  
 DRAWING NO.: PLN-01  
 SHEET NO.: 21

### PLANT SCHEDULE

TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONT.	COMMENTS
AS	9	Acer rubrum 'Red Sunset'	Red Sunset Maple	3.0" Cal.	B & B	
CR	7	Cornus x 'Rutgan'	Stellar Pink Dogwood	2.5" Cal.	B & B	
PO	9	Prunus x okame	Okame Cherry	3.0" Cal.	B & B	
ZS	2	Zelkova serrata 'Village Green'	Village Green Zelkova	3.5" Cal.	B & B	

SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONT.	COMMENTS
IS	14	Ilex glabra 'Shamrock'	Shamrock Holly	#5	CONT.	FULL & DENSE

### PROPOSED LEGEND

- CRUSHED STONE
- CONCRETE SIDEWALK
- CONCRETE SIDEWALK - 8" THICK
- BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
- BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
- CUT BITUMINOUS CONCRETE PAVEMENT
- APPROX. CUT/FILL LIMIT
- SEDIMENTATION CONTROL SYSTEM
- PROPOSED TAKING LINE
- PROPOSED EASEMENT LINE
- PROPOSED STREETLINE
- APPROXIMATE LOCATION OF NEW/RELOCATED UTILITY POLE
- SHADE TREE
- ORNAMENTAL TREE
- THREE RAIL WOOD FENCE
- WETLAND UPLAND REVIEW AREA (100' FROM EDGE OF WETLANDS/WATER COURSES)

**BEGIN FULL-DEPTH CONSTRUCTION**  
 STATION 10+12.57  
 MATCH EXISTING

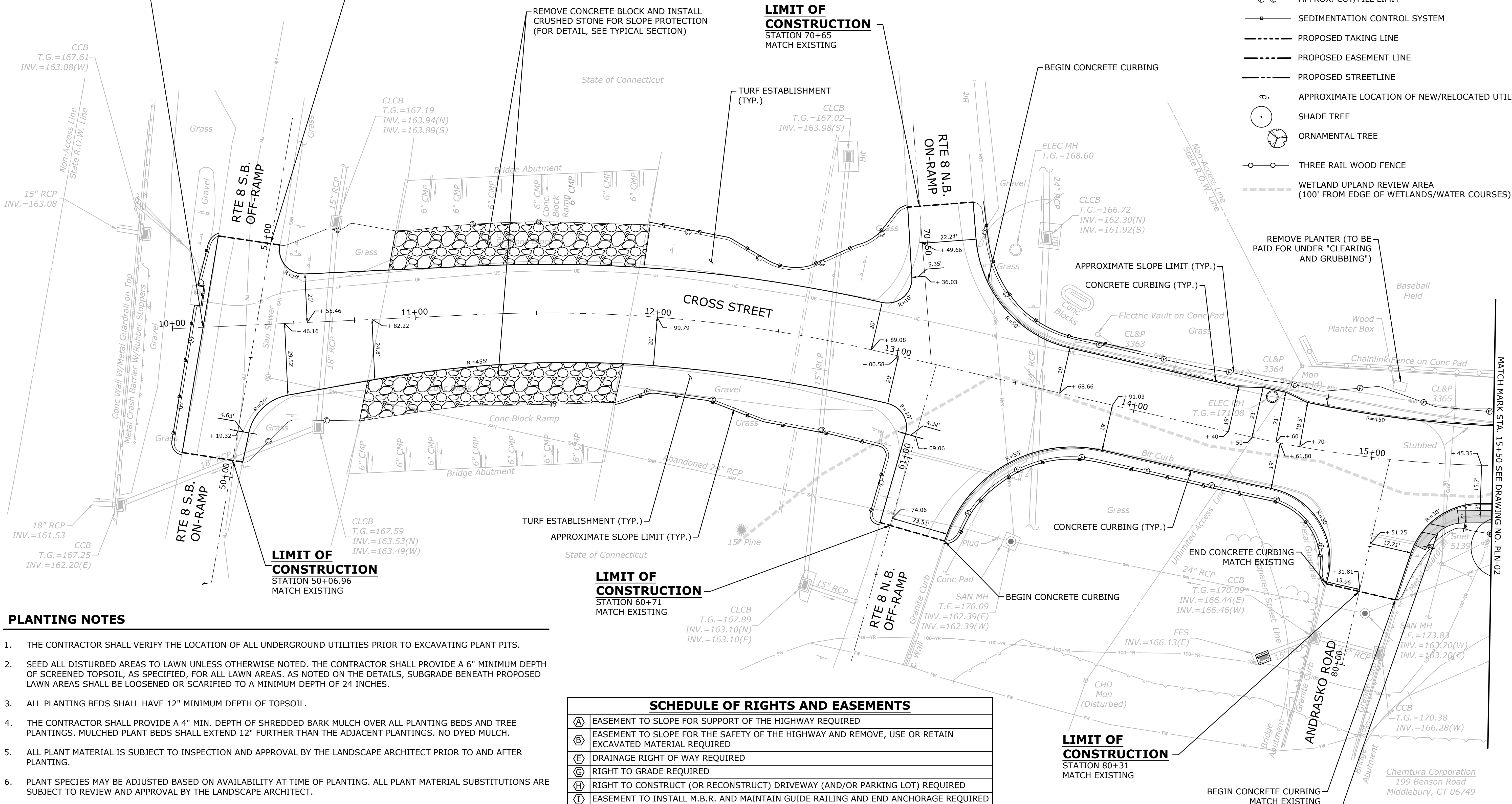
**LIMIT OF CONSTRUCTION**  
 STATION 50+97.60  
 MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
 STATION 70+65  
 MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
 STATION 50+06.96  
 MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
 STATION 60+71  
 MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
 STATION 80+31  
 MATCH EXISTING



### PLANTING NOTES

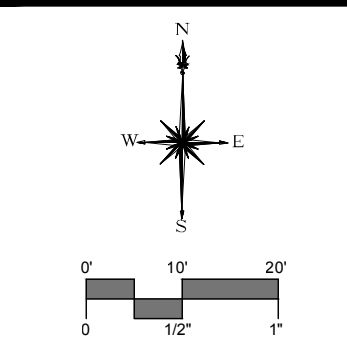
- THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO EXCAVATING PLANT PITS.
- SEED ALL DISTURBED AREAS TO LAWN UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL PROVIDE A 6" MINIMUM DEPTH OF SCREENED TOPSOIL, AS SPECIFIED, FOR ALL LAWN AREAS. AS NOTED ON THE DETAILS, SUBGRADE BENEATH PROPOSED LAWN AREAS SHALL BE LOOSENEED OR SCARIFIED TO A MINIMUM DEPTH OF 24 INCHES.
- ALL PLANTING BEDS SHALL HAVE 12" MINIMUM DEPTH OF TOPSOIL.
- THE CONTRACTOR SHALL PROVIDE A 4" MIN. DEPTH OF SHREDDED BARK MULCH OVER ALL PLANTING BEDS AND TREE PLANTINGS. MULCHED PLANT BEDS SHALL EXTEND 12" FURTHER THAN THE ADJACENT PLANTINGS. NO DYED MULCH.
- ALL PLANT MATERIAL IS SUBJECT TO INSPECTION AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO AND AFTER PLANTING.
- PLANT SPECIES MAY BE ADJUSTED BASED ON AVAILABILITY AT TIME OF PLANTING. ALL PLANT MATERIAL SUBSTITUTIONS ARE SUBJECT TO REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT.
- ALL PLANT MATERIALS SHALL CARRY A FULL GUARANTEE FOR A PERIOD OF ONE YEAR FROM THE DATE OF ACCEPTANCE, TO INCLUDE PROMPT TREATMENT OR REMOVAL AND REPLACEMENT OF ANY PLANTS FOUND TO BE IN AN UNHEALTHY CONDITION BY THE LANDSCAPE ARCHITECT. ALL REPLACEMENTS SHALL BE OF THE SAME KIND AND SIZE OF PLANTS SPECIFIED IN THE PLANT LIST.
- MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER PLANTING AND SHALL CONTINUE UNTIL ACCEPTANCE BY THE LANDSCAPE ARCHITECT AT THE END OF THE WARRANTY PERIOD. MAINTENANCE SHALL INCLUDE WATERING, MULCHING, TIGHTENING & REPLACING OF GUYS, REPLACEMENT OF SICK OR DEAD PLANTS, RESETTING PLANTS TO PROPER GRADE OR UPRIGHT (PLUMB) POSITION, RESTORATION OF SAUCERS, AND ALL OTHER CARE NEEDED FOR PROPER GROWTH OF THE PLANTS.
- CONTRACTOR TO REMOVE TREE STAKES AFTER ONE GROWING SEASON.
- TAKE NOTE TO PROTECT ROOT ZONES OF EXISTING TREES ROOT ZONES DURING CONSTRUCTION.

### SCHEDULE OF RIGHTS AND EASEMENTS

(A)	EASEMENT TO SLOPE FOR SUPPORT OF THE HIGHWAY REQUIRED
(B)	EASEMENT TO SLOPE FOR THE SAFETY OF THE HIGHWAY AND REMOVE, USE OR RETAIN EXCAVATED MATERIAL REQUIRED
(E)	DRAINAGE RIGHT OF WAY REQUIRED
(G)	RIGHT TO GRADE REQUIRED
(H)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) DRIVEWAY (AND/OR PARKING LOT) REQUIRED
(I)	EASEMENT TO INSTALL M.B.R. AND MAINTAIN GUIDE RAILING AND END ANCHORAGE REQUIRED
(J)	RIGHT TO INSTALL SEDIMENTATION CONTROL SYSTEM REQUIRED
(L)	TEMPORARY WORK AREA
(N)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) SIDEWALK REQUIRED
(O)	RIGHT TO CONSTRUCT STEPS AND CONCRETE WALK REQUIRED
(P)	RIGHT TO INSTALL (RESET OR RELOCATE) STONE WALL OR FENCE REQUIRED
(Q)	RIGHT TO INSTALL FENCE REQUIRED
(S)	DEFINED SIGHT LINE EASEMENT REQUIRED
(U)	EASEMENT TO CONSTRUCT AND MAINTAIN RETAINING WALL REQUIRED
(V)	EASEMENT TO CONSTRUCT SIDEWALK REQUIRED
(Z)	RIGHT TO INSTALL CATCH BASIN AND PIPE REQUIRED

### NOTES

- FOR CONSTRUCTION BASELINE GEOMETRY, SEE EXISTING CONDITIONS AND BASE LINE LAYOUT PLAN
- FOR SIGNAGE AND PAVEMENT MARKINGS, SEE SIGNING AND PAVEMENT MARKING PLAN
- FOR PROPOSED DRAINAGE, SEE DRAINAGE PLAN



**MILONE & MACBROOM**  
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 (203) 271-1773 Fax (203) 272-9733  
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DESCRIPTION	DATE	BY

**ROADWAY PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: PLN-01		
<b>21</b>		





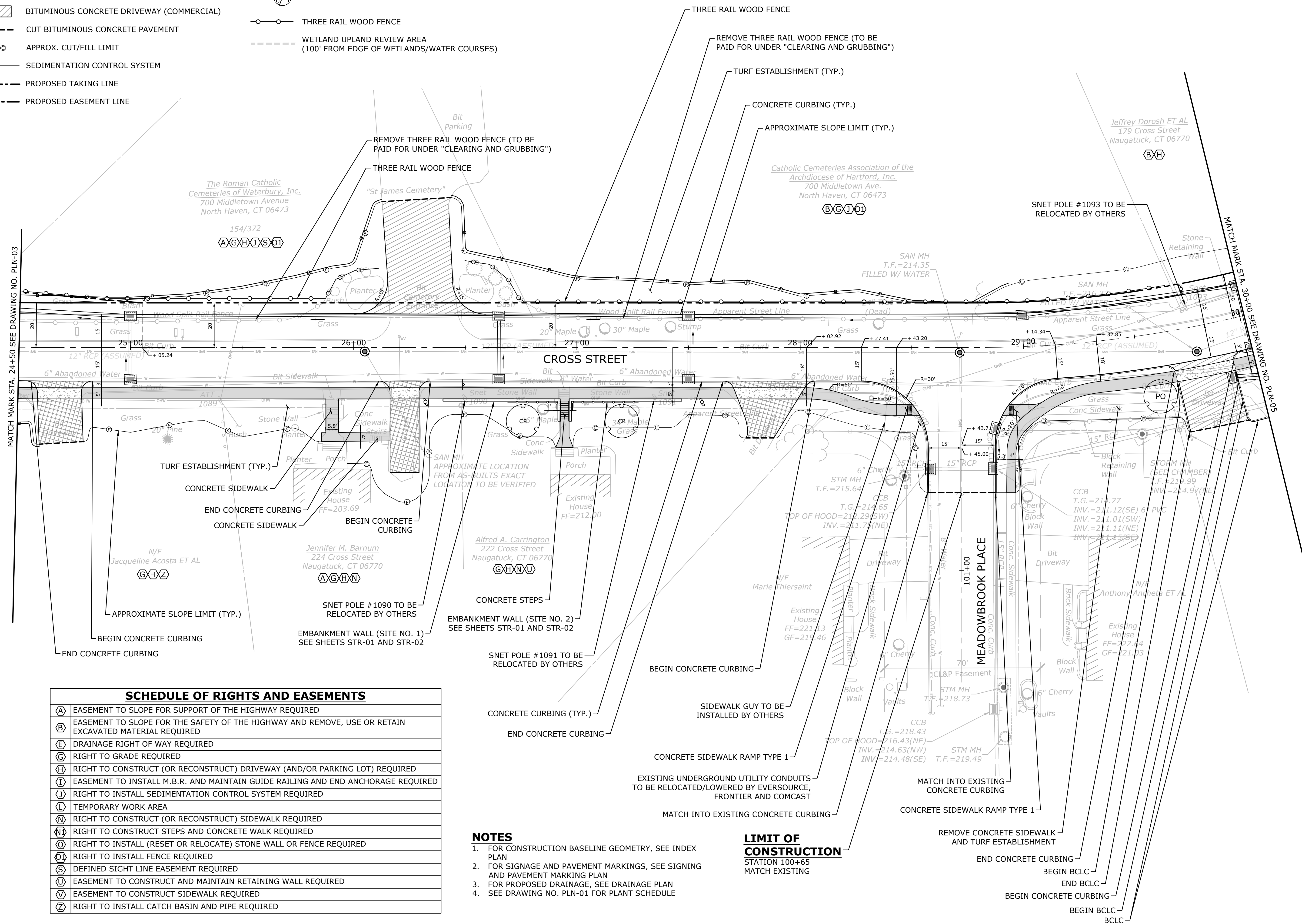






**PROPOSED LEGEND**

- CRUSHED STONE
- CONCRETE SIDEWALK
- CONCRETE SIDEWALK - 8" THICK
- BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
- BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
- CUT BITUMINOUS CONCRETE PAVEMENT
- APPROX. CUT/FILL LIMIT
- SEDIMENTATION CONTROL SYSTEM
- PROPOSED TAKING LINE
- PROPOSED EASEMENT LINE
- PROPOSED STREETLINE
- APPROXIMATE LOCATION OF NEW/RELOCATED UTILITY POLE
- SHADE TREE
- ORNAMENTAL TREE
- THREE RAIL WOOD FENCE
- WETLAND UPLAND REVIEW AREA (100' FROM EDGE OF WETLANDS/WATER COURSES)



**SCHEDULE OF RIGHTS AND EASEMENTS**

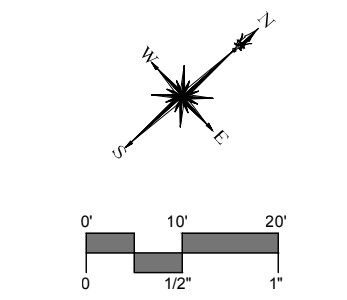
(A)	EASEMENT TO SLOPE FOR SUPPORT OF THE HIGHWAY REQUIRED
(B)	EASEMENT TO SLOPE FOR THE SAFETY OF THE HIGHWAY AND REMOVE, USE OR RETAIN EXCAVATED MATERIAL REQUIRED
(E)	DRAINAGE RIGHT OF WAY REQUIRED
(G)	RIGHT TO GRADE REQUIRED
(H)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) DRIVEWAY (AND/OR PARKING LOT) REQUIRED
(I)	EASEMENT TO INSTALL M.B.R. AND MAINTAIN GUIDE RAILING AND END ANCHORAGE REQUIRED
(J)	RIGHT TO INSTALL SEDIMENTATION CONTROL SYSTEM REQUIRED
(L)	TEMPORARY WORK AREA
(N)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) SIDEWALK REQUIRED
(N)	RIGHT TO CONSTRUCT STEPS AND CONCRETE WALK REQUIRED
(O)	RIGHT TO INSTALL (RESET OR RELOCATE) STONE WALL OR FENCE REQUIRED
(S)	RIGHT TO INSTALL FENCE REQUIRED
(D)	DEFINED SIGHT LINE EASEMENT REQUIRED
(U)	EASEMENT TO CONSTRUCT AND MAINTAIN RETAINING WALL REQUIRED
(V)	EASEMENT TO CONSTRUCT SIDEWALK REQUIRED
(Z)	RIGHT TO INSTALL CATCH BASIN AND PIPE REQUIRED

**NOTES**

- FOR CONSTRUCTION BASELINE GEOMETRY, SEE INDEX PLAN
- FOR SIGNAGE AND PAVEMENT MARKINGS, SEE SIGNING AND PAVEMENT MARKING PLAN
- FOR PROPOSED DRAINAGE, SEE DRAINAGE PLAN
- SEE DRAWING NO. PLN-01 FOR PLANT SCHEDULE

**LIMIT OF CONSTRUCTION**

STATION 100+65  
MATCH EXISTING



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DESCRIPTION	DATE	BY



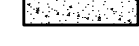


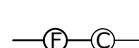
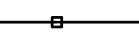







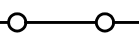

**ROADWAY PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
NAUGATUCK, CONNECTICUT

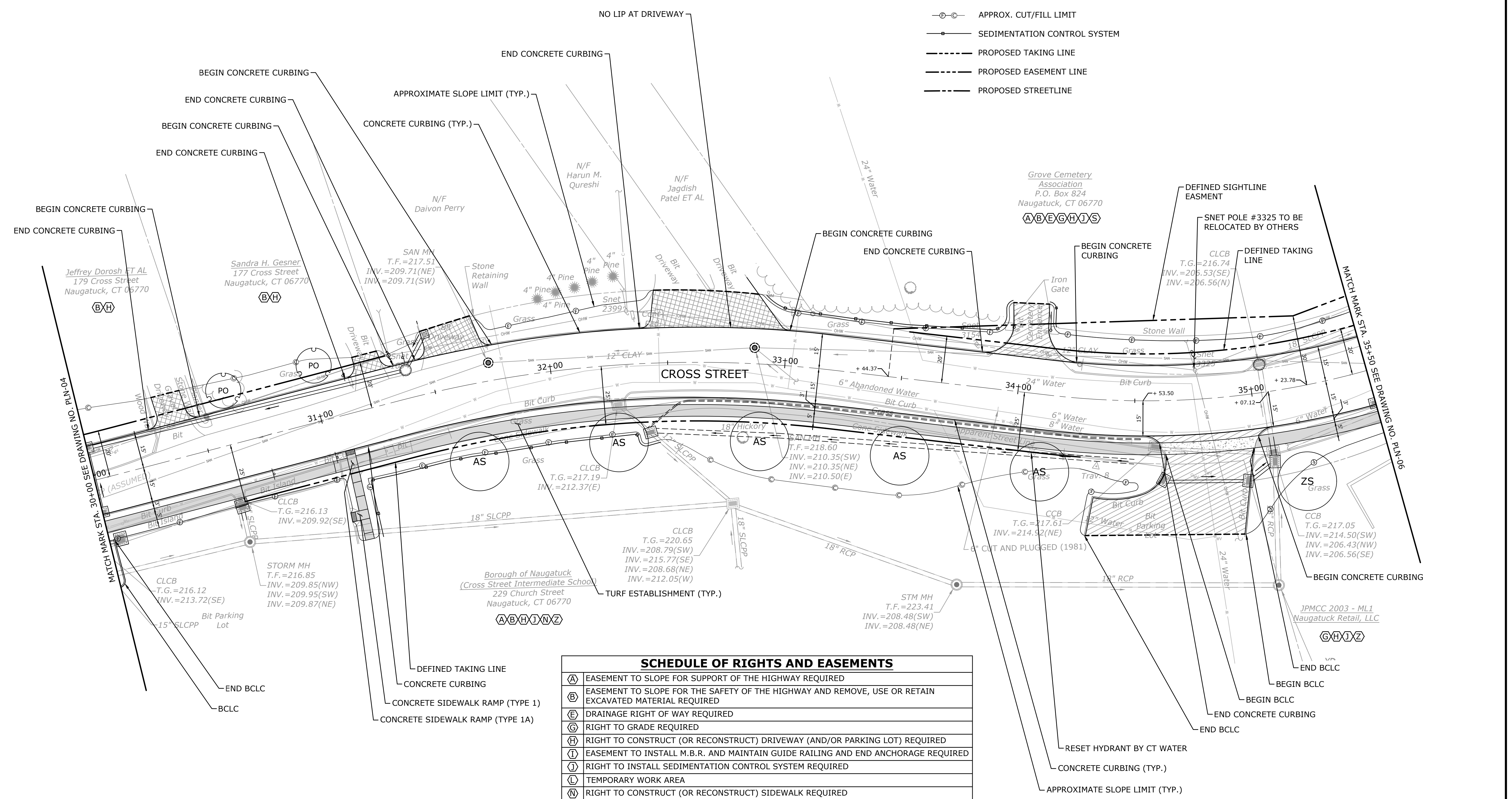
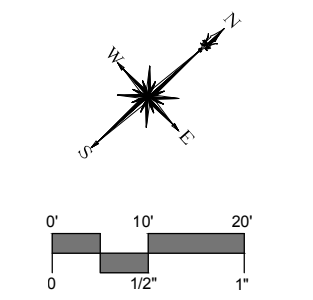
DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: PLN-04		
<b>24</b>		



MILONE & MACBROOM, INC. 2015

**PROPOSED LEGEND**

-  CRUSHED STONE
-  CONCRETE SIDEWALK
-  CONCRETE SIDEWALK - 8" THICK
-  BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
-  BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
-  CUT BITUMINOUS CONCRETE PAVEMENT
-  APPROX. CUT/FILL LIMIT
-  SEDIMENTATION CONTROL SYSTEM
-  PROPOSED TAKING LINE
-  PROPOSED EASEMENT LINE
-  PROPOSED STREETLINE
-  APPROXIMATE LOCATION OF NEW/RELOCATED UTILITY POLE
-  SHADE TREE
-  ORNAMENTAL TREE
-  THREE RAIL WOOD FENCE
-  WETLAND UPLAND REVIEW AREA (100' FROM EDGE OF WETLANDS/WATER COURSES)



SCHEDULE OF RIGHTS AND EASEMENTS	
(A)	EASEMENT TO SLOPE FOR SUPPORT OF THE HIGHWAY REQUIRED
(B)	EASEMENT TO SLOPE FOR THE SAFETY OF THE HIGHWAY AND REMOVE, USE OR RETAIN EXCAVATED MATERIAL REQUIRED
(E)	DRAINAGE RIGHT OF WAY REQUIRED
(G)	RIGHT TO GRADE REQUIRED
(H)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) DRIVEWAY (AND/OR PARKING LOT) REQUIRED
(I)	EASEMENT TO INSTALL M.B.R. AND MAINTAIN GUIDE RAILING AND END ANCHORAGE REQUIRED
(J)	RIGHT TO INSTALL SEDIMENTATION CONTROL SYSTEM REQUIRED
(K)	TEMPORARY WORK AREA
(N)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) SIDEWALK REQUIRED
(N1)	RIGHT TO CONSTRUCT STEPS AND CONCRETE WALK REQUIRED
(O)	RIGHT TO INSTALL (RESET OR RELOCATE) STONE WALL OR FENCE REQUIRED
(O1)	RIGHT TO INSTALL FENCE REQUIRED
(S)	DEFINED SIGHT LINE EASEMENT REQUIRED
(U)	EASEMENT TO CONSTRUCT AND MAINTAIN RETAINING WALL REQUIRED
(V)	EASEMENT TO CONSTRUCT SIDEWALK REQUIRED
(Z)	RIGHT TO INSTALL CATCH BASIN AND PIPE REQUIRED

- NOTES**
- FOR CONSTRUCTION BASELINE GEOMETRY, SEE INDEX PLAN
  - FOR SIGNAGE AND PAVEMENT MARKINGS, SEE SIGNING AND PAVEMENT MARKING PLAN
  - FOR PROPOSED DRAINAGE, SEE DRAINAGE PLAN
  - SEE DRAWING NO. PLN-01 FOR PLANT SCHEDULE

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DESCRIPTION	DATE	BY

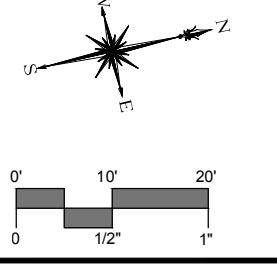
**ROADWAY PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: PLN-05		

**25**  
 SHEET NO.



SHEET 1 OF 26 - SEE DRAWING NO. PLN-05 TO THE LEFT AND DRAWING NO. PLN-07 TO THE RIGHT.  
 ALL RIGHTS AND EASEMENTS ARE TO BE OBTAINED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS.  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS.  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS.



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DESCRIPTION	DATE	BY

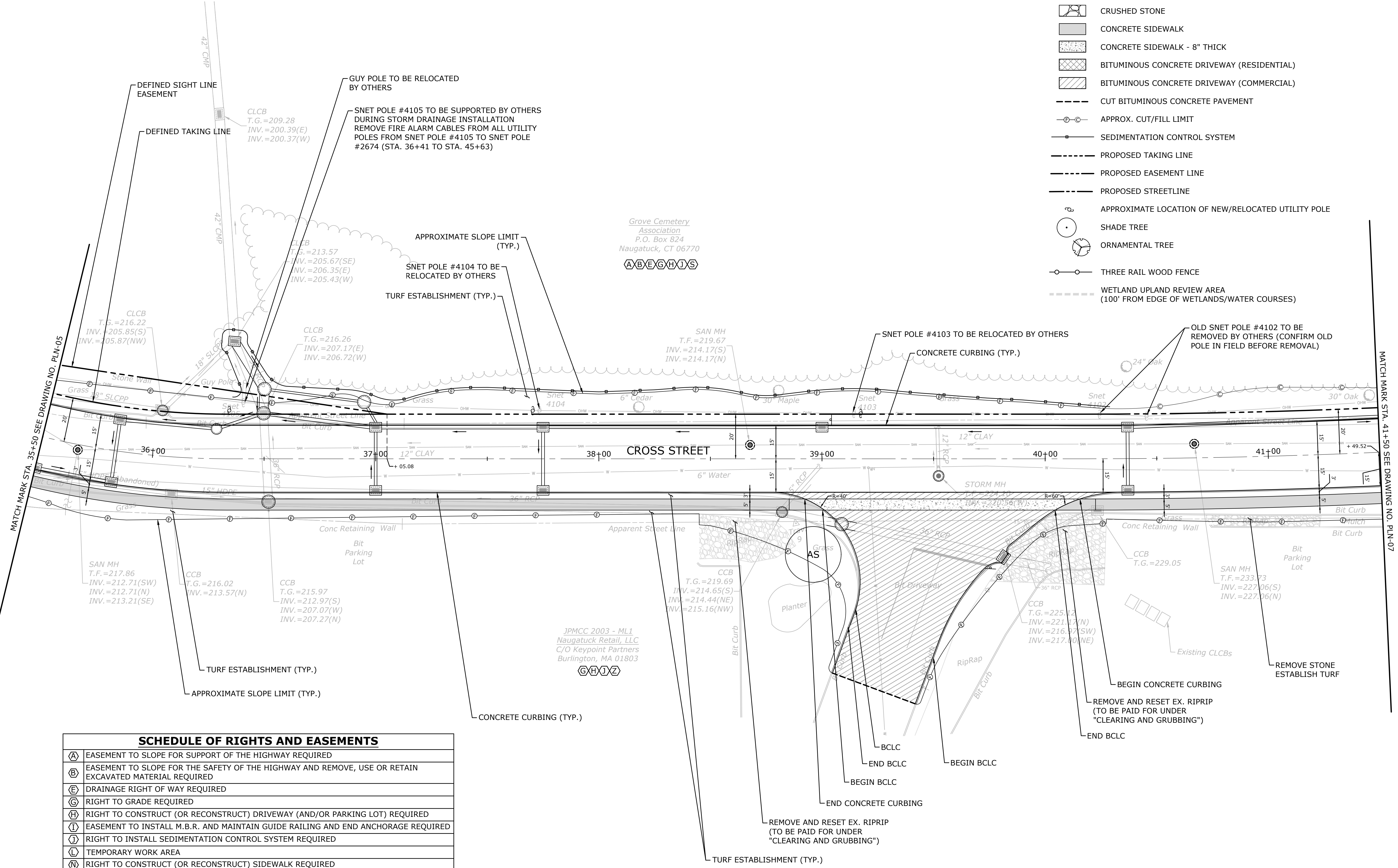
**ROADWAY PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE		
1"=20'		
DATE		
SEPTEMBER 7, 2018		
PROJECT NO.		
87-145		
DRAWING NO.		
PLN-06		

**26**  
SHEET NO.  
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**PROPOSED LEGEND**

- CRUSHED STONE
- CONCRETE SIDEWALK
- CONCRETE SIDEWALK - 8" THICK
- BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
- BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
- CUT BITUMINOUS CONCRETE PAVEMENT
- APPROX. CUT/FILL LIMIT
- SEDIMENTATION CONTROL SYSTEM
- PROPOSED TAKING LINE
- PROPOSED EASEMENT LINE
- PROPOSED STREETLINE
- APPROXIMATE LOCATION OF NEW/RELOCATED UTILITY POLE
- SHADE TREE
- ORNAMENTAL TREE
- THREE RAIL WOOD FENCE
- WETLAND UPLAND REVIEW AREA (100' FROM EDGE OF WETLANDS/WATER COURSES)



Grove Cemetery  
 Association  
 P.O. Box 824  
 Naugatuck, CT 06770  
**ABEGHDS**

JPMCC 2003 - ML1  
 Naugatuck Retail, LLC  
 C/O Keypoint Partners  
 Burlington, MA 01803  
**GHI Z**

**SCHEDULE OF RIGHTS AND EASEMENTS**

(A)	EASEMENT TO SLOPE FOR SUPPORT OF THE HIGHWAY REQUIRED
(B)	EASEMENT TO SLOPE FOR THE SAFETY OF THE HIGHWAY AND REMOVE, USE OR RETAIN EXCAVATED MATERIAL REQUIRED
(E)	DRAINAGE RIGHT OF WAY REQUIRED
(G)	RIGHT TO GRADE REQUIRED
(H)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) DRIVEWAY (AND/OR PARKING LOT) REQUIRED
(I)	EASEMENT TO INSTALL M.B.R. AND MAINTAIN GUIDE RAILING AND END ANCHORAGE REQUIRED
(J)	RIGHT TO INSTALL SEDIMENTATION CONTROL SYSTEM REQUIRED
(L)	TEMPORARY WORK AREA
(N)	RIGHT TO CONSTRUCT (OR RECONSTRUCT) SIDEWALK REQUIRED
(NI)	RIGHT TO CONSTRUCT STEPS AND CONCRETE WALK REQUIRED
(O)	RIGHT TO INSTALL (RESET OR RELOCATE) STONE WALL OR FENCE REQUIRED
(OI)	RIGHT TO INSTALL FENCE REQUIRED
(S)	DEFINED SIGHT LINE EASEMENT REQUIRED
(U)	EASEMENT TO CONSTRUCT AND MAINTAIN RETAINING WALL REQUIRED
(V)	EASEMENT TO CONSTRUCT SIDEWALK REQUIRED
(Z)	RIGHT TO INSTALL CATCH BASIN AND PIPE REQUIRED

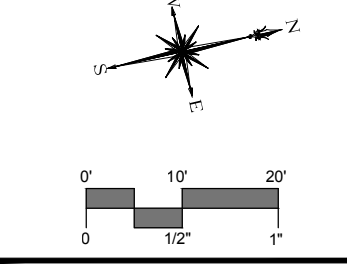
**NOTES**

1. FOR CONSTRUCTION BASELINE GEOMETRY, SEE INDEX PLAN
2. FOR SIGNAGE AND PAVEMENT MARKINGS, SEE SIGNING AND PAVEMENT MARKING PLAN
3. FOR PROPOSED DRAINAGE, SEE DRAINAGE PLAN
4. SEE DRAWING NO. PLN-01 FOR PLANT SCHEDULE



**SCHEDULE OF RIGHTS AND EASEMENTS**

(A)	EASEMENT TO SLOPE FOR SUPPORT OF THE HIGHWAY REQUIRED
(B)	EASEMENT TO SLOPE FOR THE SAFETY OF THE HIGHWAY AND REMOVE, USE OR RETAIN EXCAVATED MATERIAL REQUIRED
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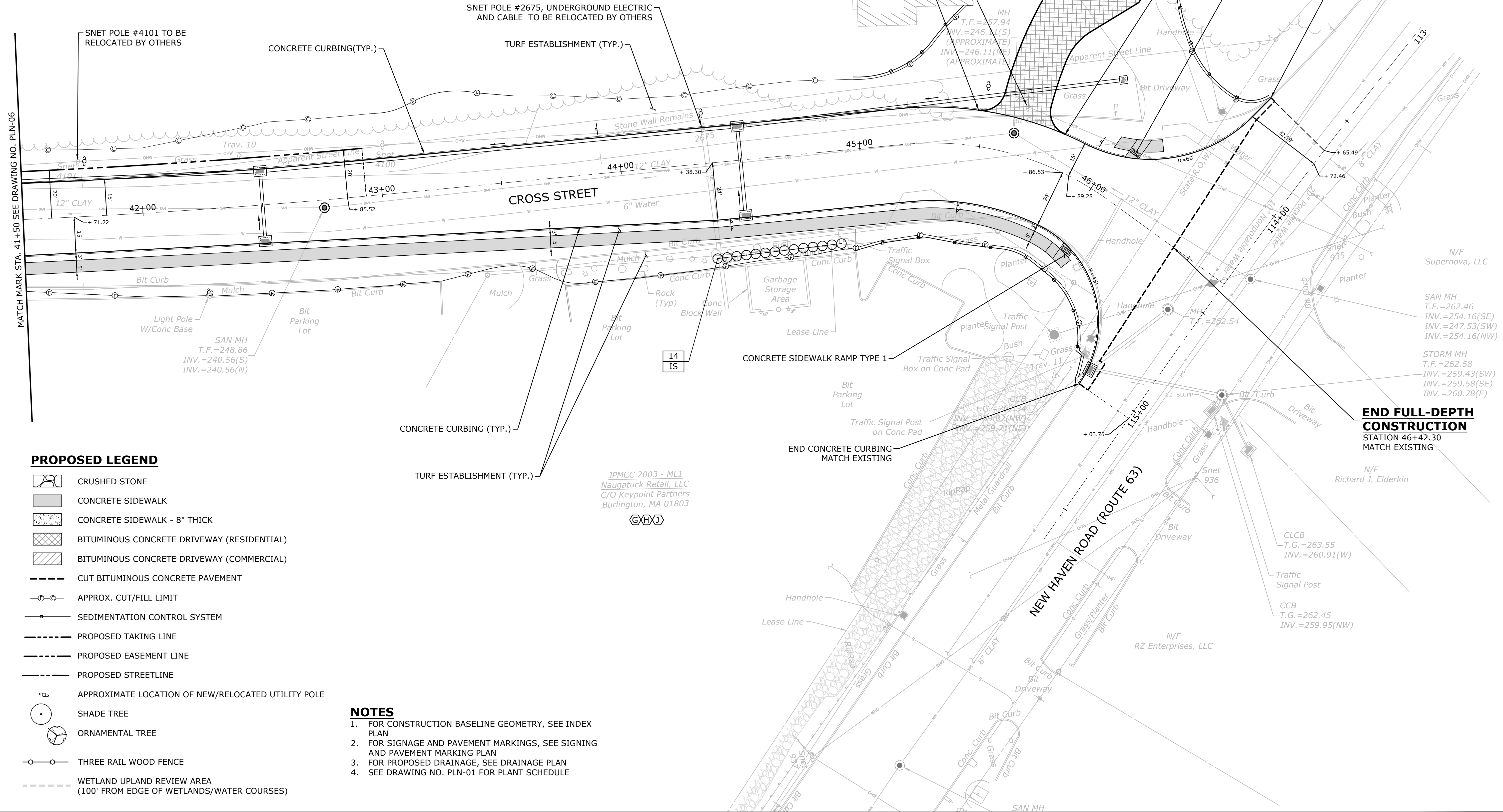


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DESCRIPTION	DATE	BY

**ROADWAY PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: PLN-07		
<b>27</b>		



- PROPOSED LEGEND**
- CRUSHED STONE
  - CONCRETE SIDEWALK
  - CONCRETE SIDEWALK - 8" THICK
  - BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
  - BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
  - CUT BITUMINOUS CONCRETE PAVEMENT
  - APPROX. CUT/FILL LIMIT
  - SEDIMENTATION CONTROL SYSTEM
  - PROPOSED TAKING LINE
  - PROPOSED EASEMENT LINE
  - PROPOSED STREETLINE
  - APPROXIMATE LOCATION OF NEW/RELOCATED UTILITY POLE
  - SHADE TREE
  - ORNAMENTAL TREE
  - THREE RAIL WOOD FENCE
  - WETLAND UPLAND REVIEW AREA (100' FROM EDGE OF WETLANDS/WATER COURSES)

- NOTES**
- FOR CONSTRUCTION BASELINE GEOMETRY, SEE INDEX PLAN
  - FOR SIGNAGE AND PAVEMENT MARKINGS, SEE SIGNING AND PAVEMENT MARKING PLAN
  - FOR PROPOSED DRAINAGE, SEE DRAINAGE PLAN
  - SEE DRAWING NO. PLN-01 FOR PLANT SCHEDULE

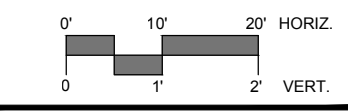
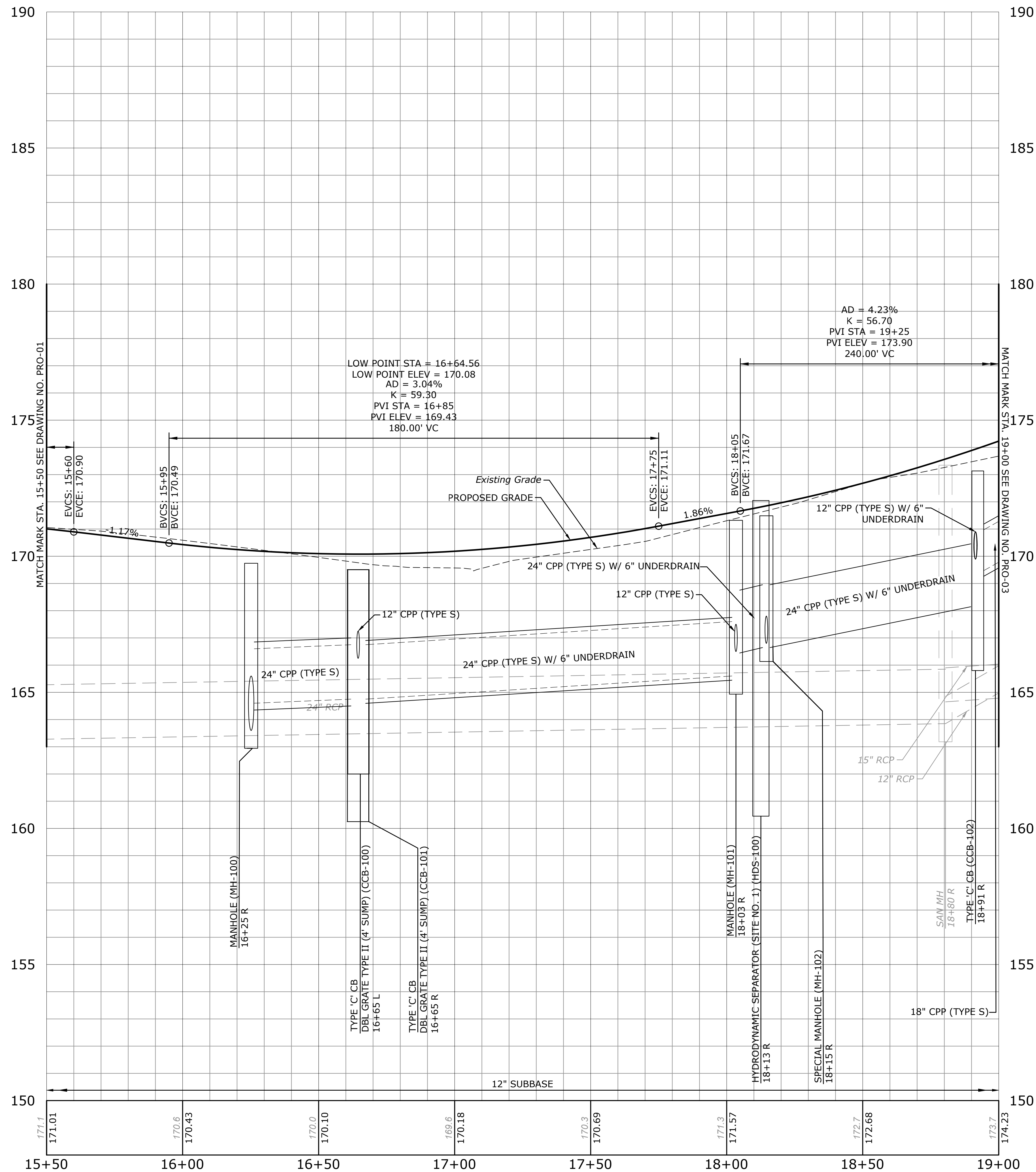
JPMCC 2003 - MLI  
 Naugatuck Retail, LLC  
 C/O Keypoint Partners  
 Burlington, MA 01803  
 (G)(H)(J)







SHEET 29 - ROADWAY PROFILE - CROSS STREET  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT



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DESCRIPTION	DATE	BY

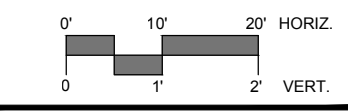
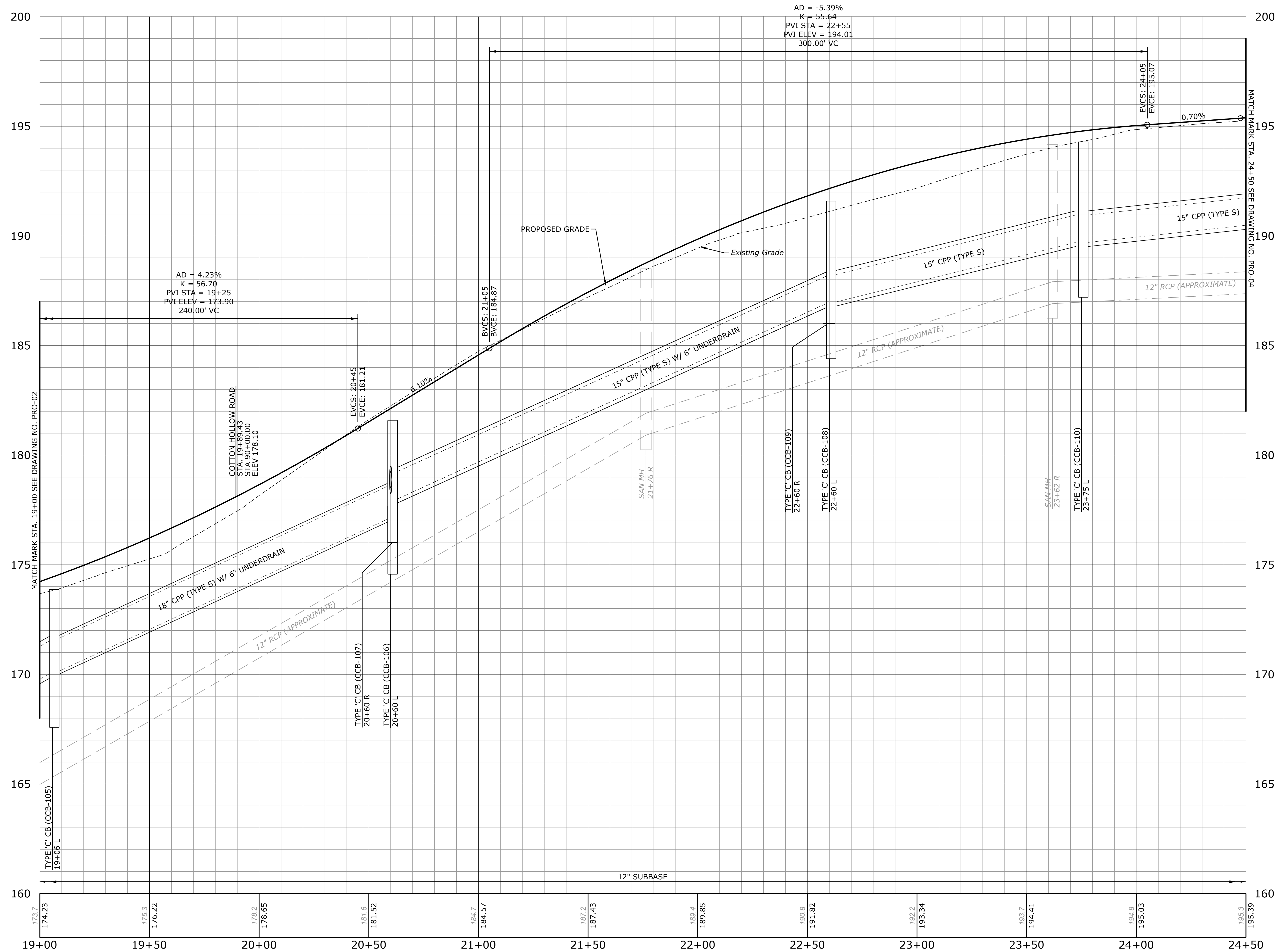
**ROADWAY PROFILE - CROSS STREET**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=20' HORIZ. 1"=2' VERT.		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. PRO-02		

**29**  
 SHEET NO.



MILE & MACBROOM ENGINEERS, INC. 1000 WEST MAIN STREET, SUITE 200, WESTPORT, CT 06880  
 TEL: (203) 271-1773 FAX: (203) 272-9733 WWW.MILEANDMACBROOM.COM



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DESCRIPTION	DATE	BY

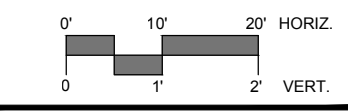
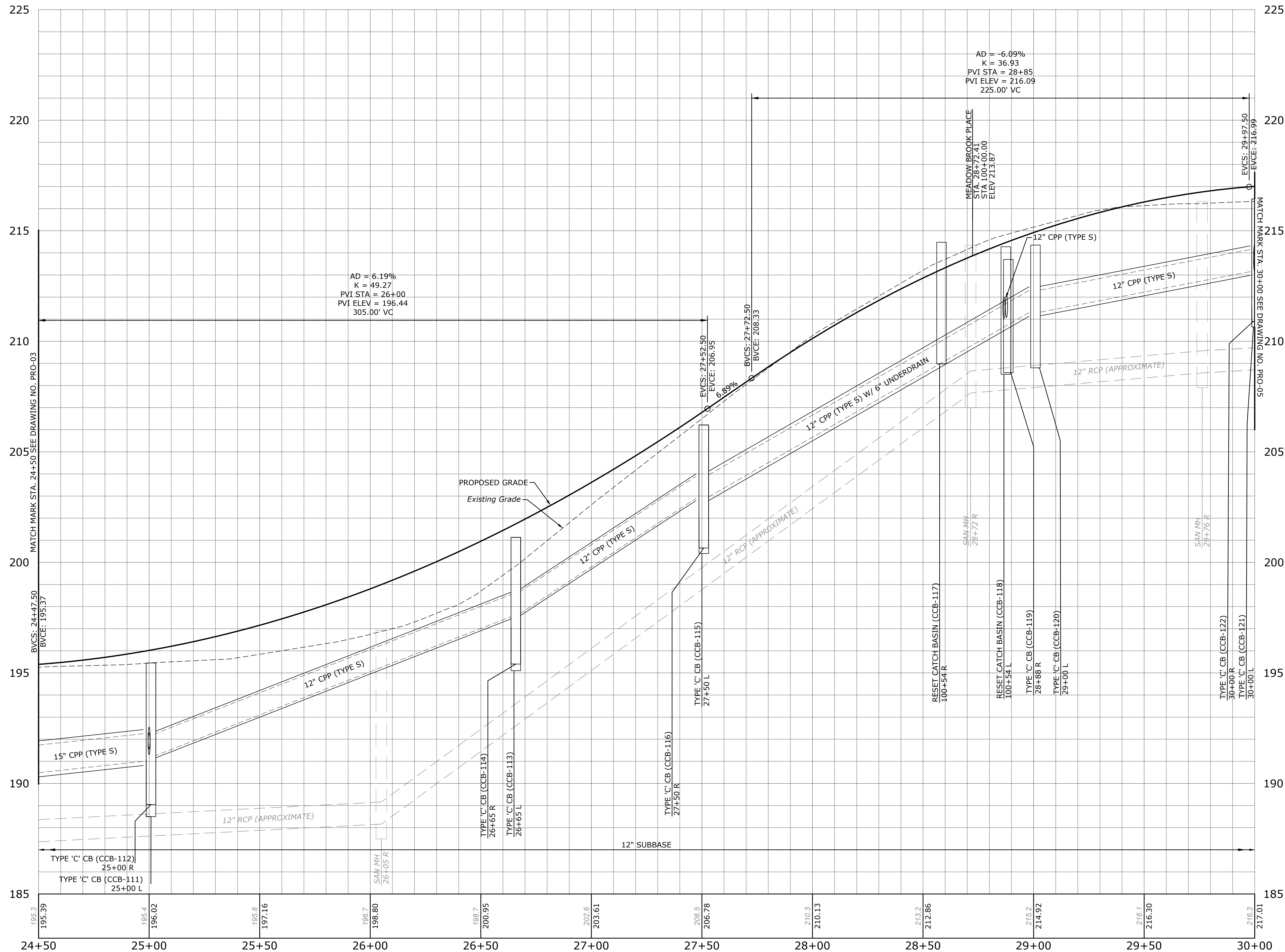
**ROADWAY PROFILE - CROSS STREET**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=20' HORIZ. 1"=2' VERT.		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. PRO-03		

**30**



MILONE & MACBROOM ENGINEERS, INC. 99 REEDY DRIVE, CHESHIRE, CONNECTICUT 06410



DESCRIPTION	DATE	BY

**ROADWAY PROFILE - CROSS STREET  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT**

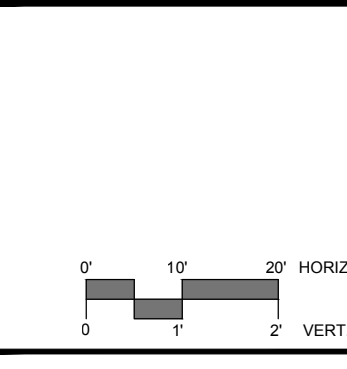
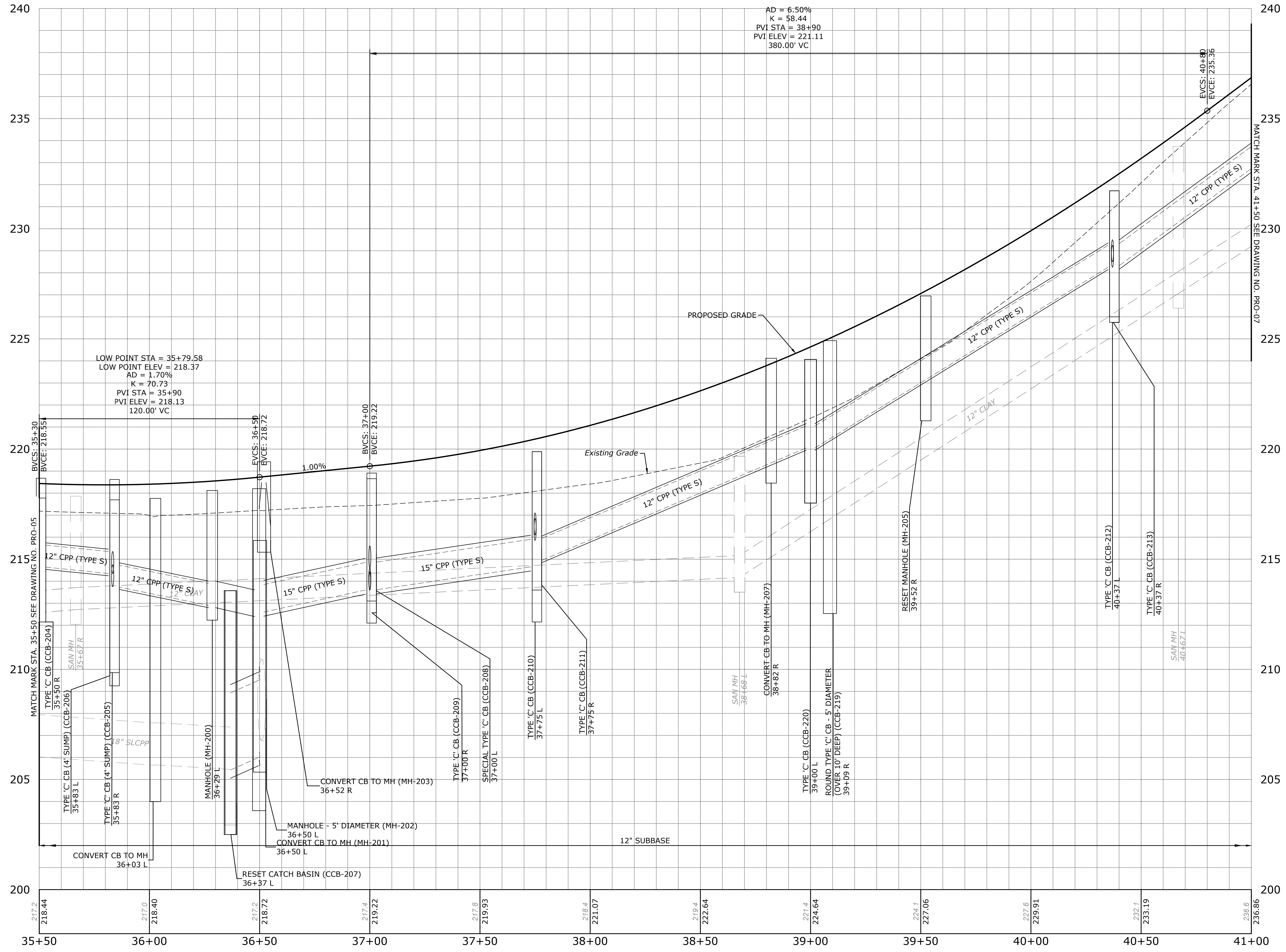
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SCALE 1"=20' HORIZ. 1"=2' VERT.		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. PRO-04		







MILEONE & MACBROOM, INC. 99 REALTY DRIVE, CHELSEA, CONNECTICUT 06010  
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 Chelsea, Connecticut 06010  
 (203) 271-1773 Fax (203) 272-9733  
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DESCRIPTION	DATE	BY

**ROADWAY PROFILE - CROSS STREET**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

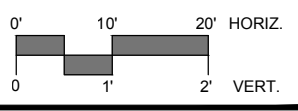
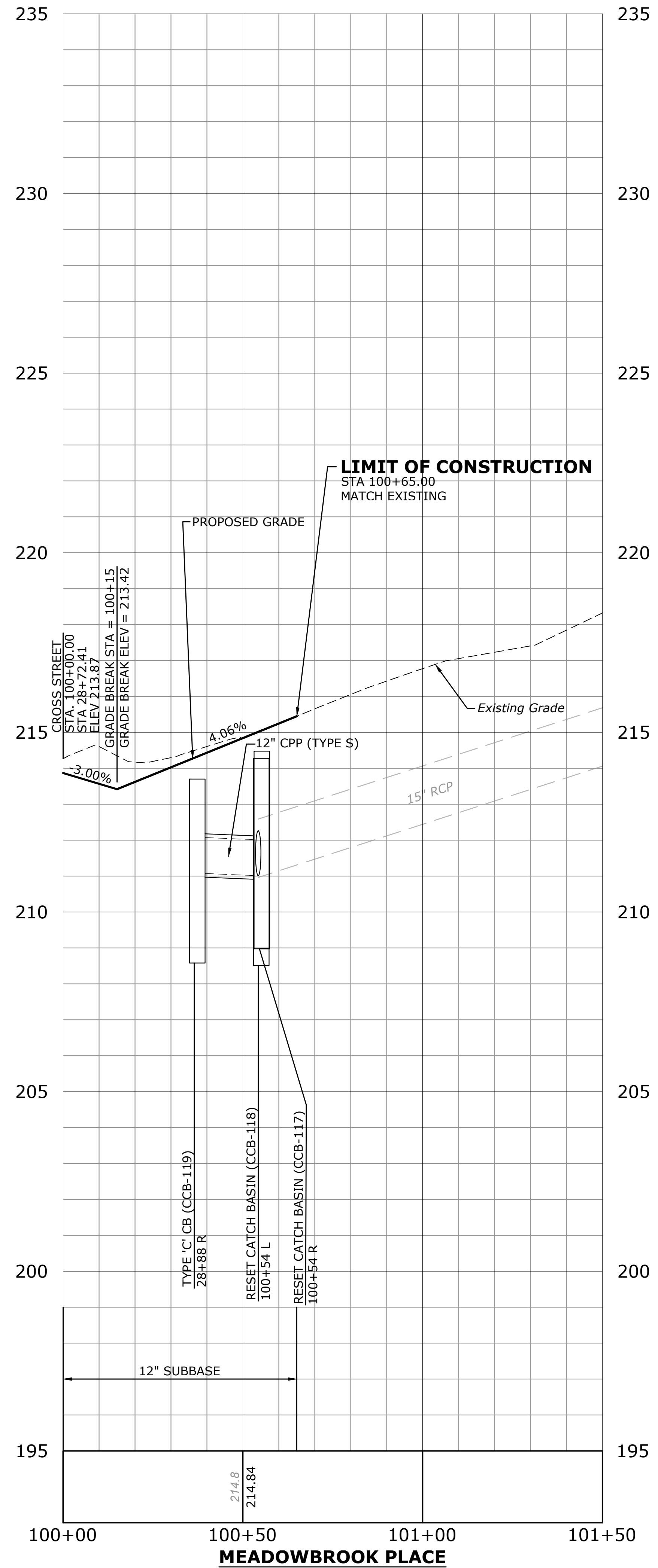
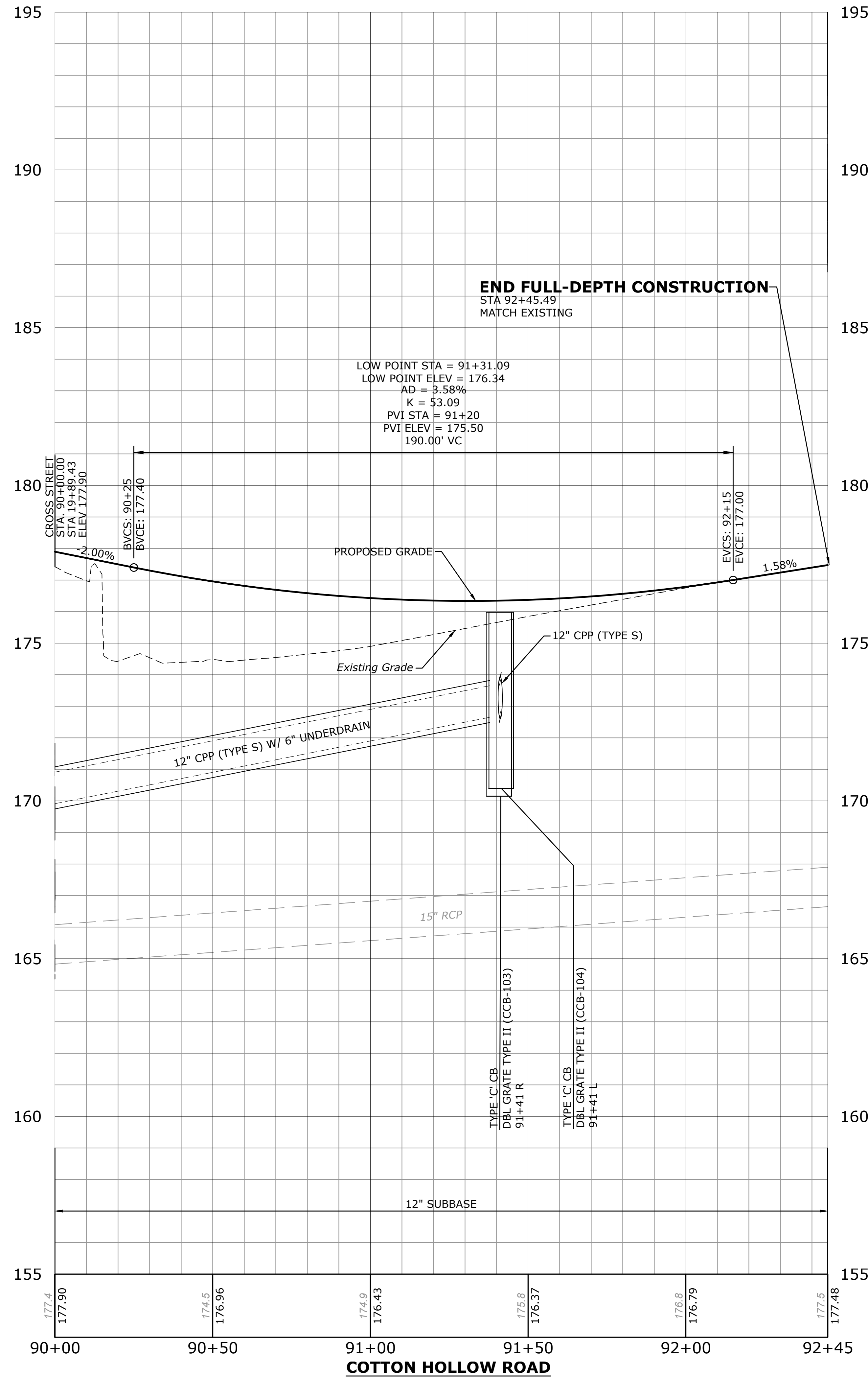
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DESIGNED	DRAWN	CHECKED
1"=20' HORIZ.		
SCALE 1"=2' VERT.		
SEPTEMBER 7, 2018		
DATE		
PROJECT NO. 87-145		
DRAWING NO. PRO-06		







MILEAGE - 1.3 MILE LONG. SEE THE PLAN FOR THE LOCATION OF THIS SHEET IN THE PROJECT.



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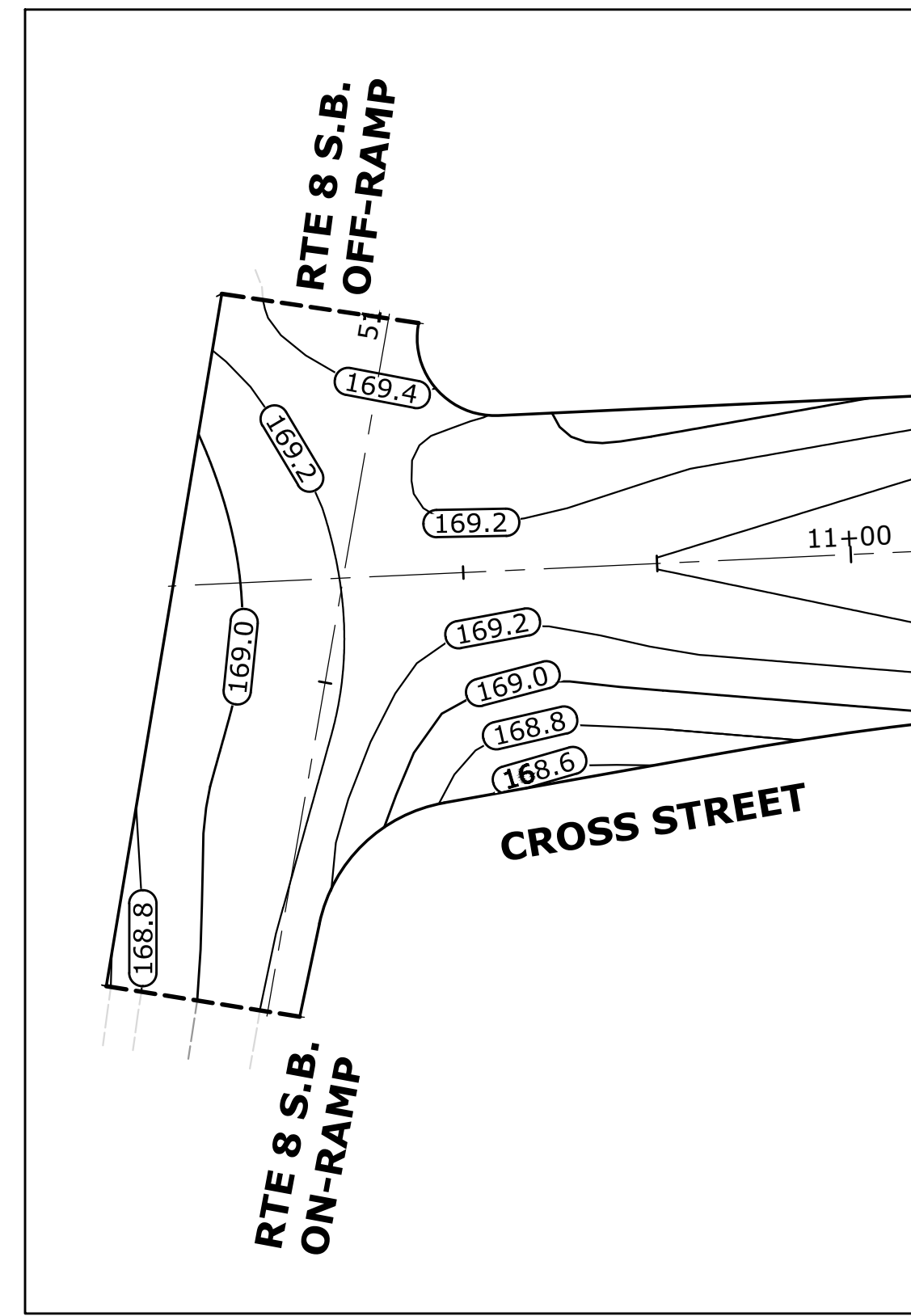
DESCRIPTION	DATE	BY

**ROADWAY PROFILE**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

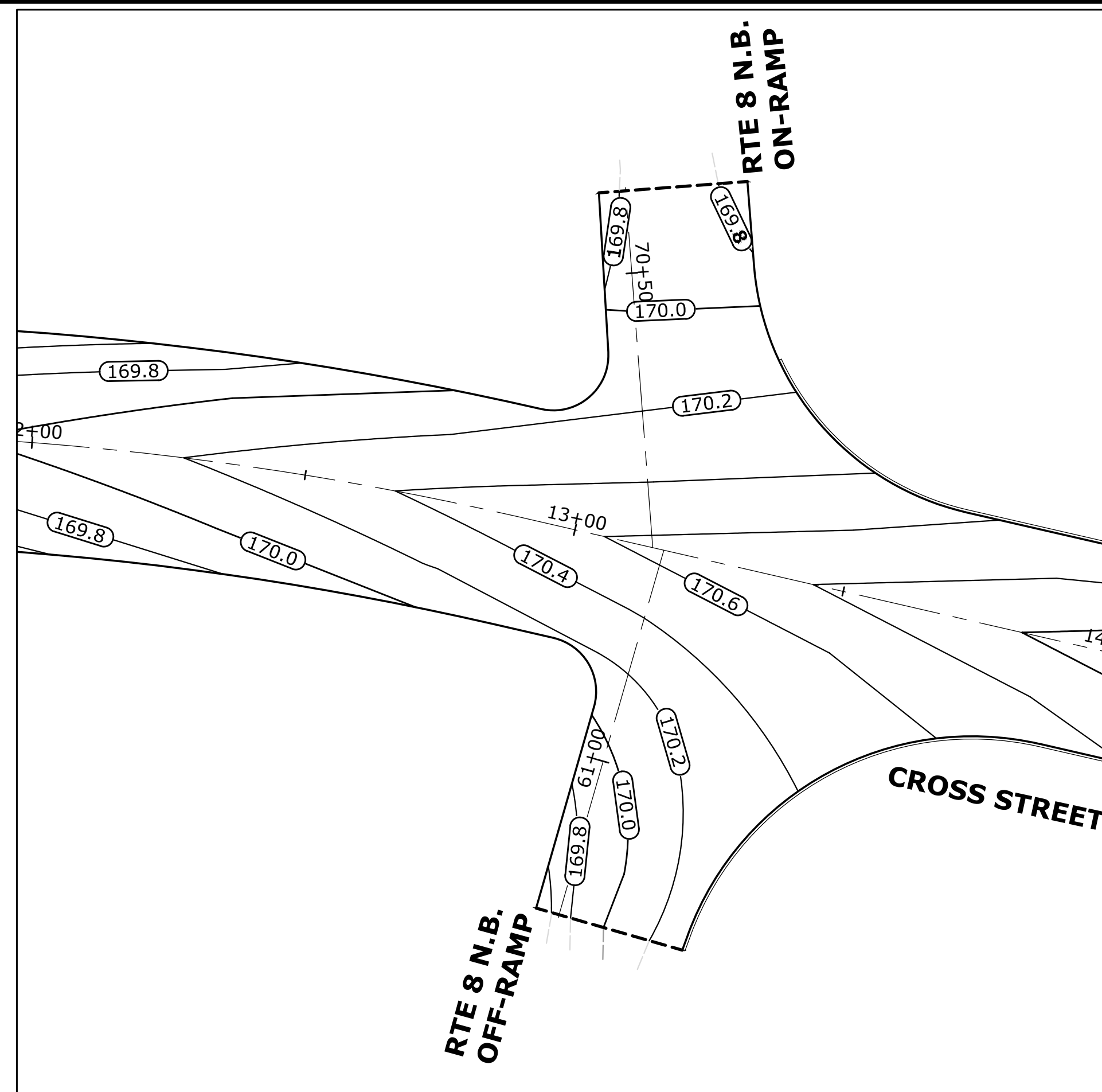
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DESIGNED	DRAWN	CHECKED
1"=20' HORIZ.		
SCALE 1"=2' VERT.		
SEPTEMBER 7, 2018		
DATE		
PROJECT NO. 87-145		
DRAWING NO. PRO-08		

**35**

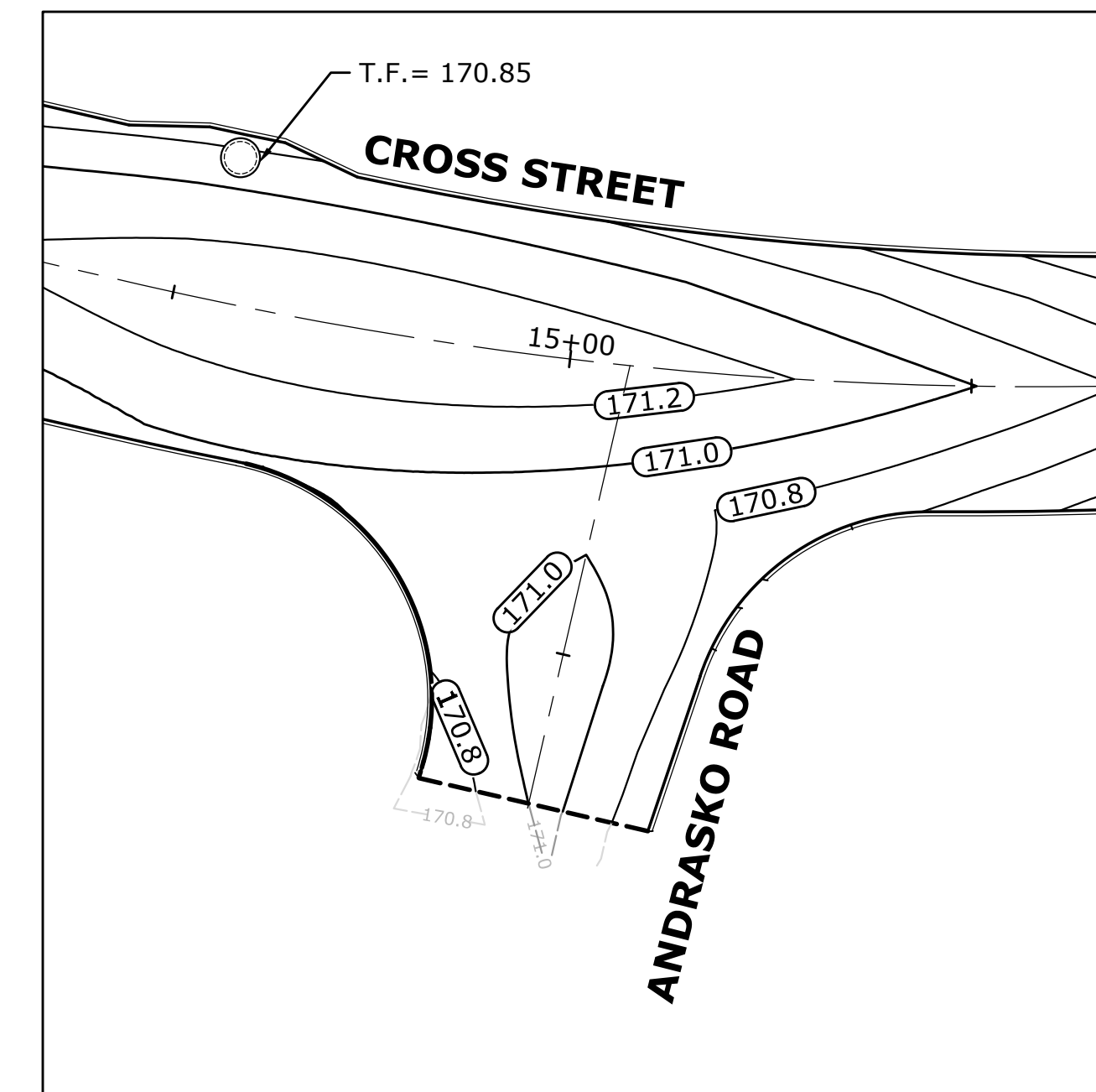




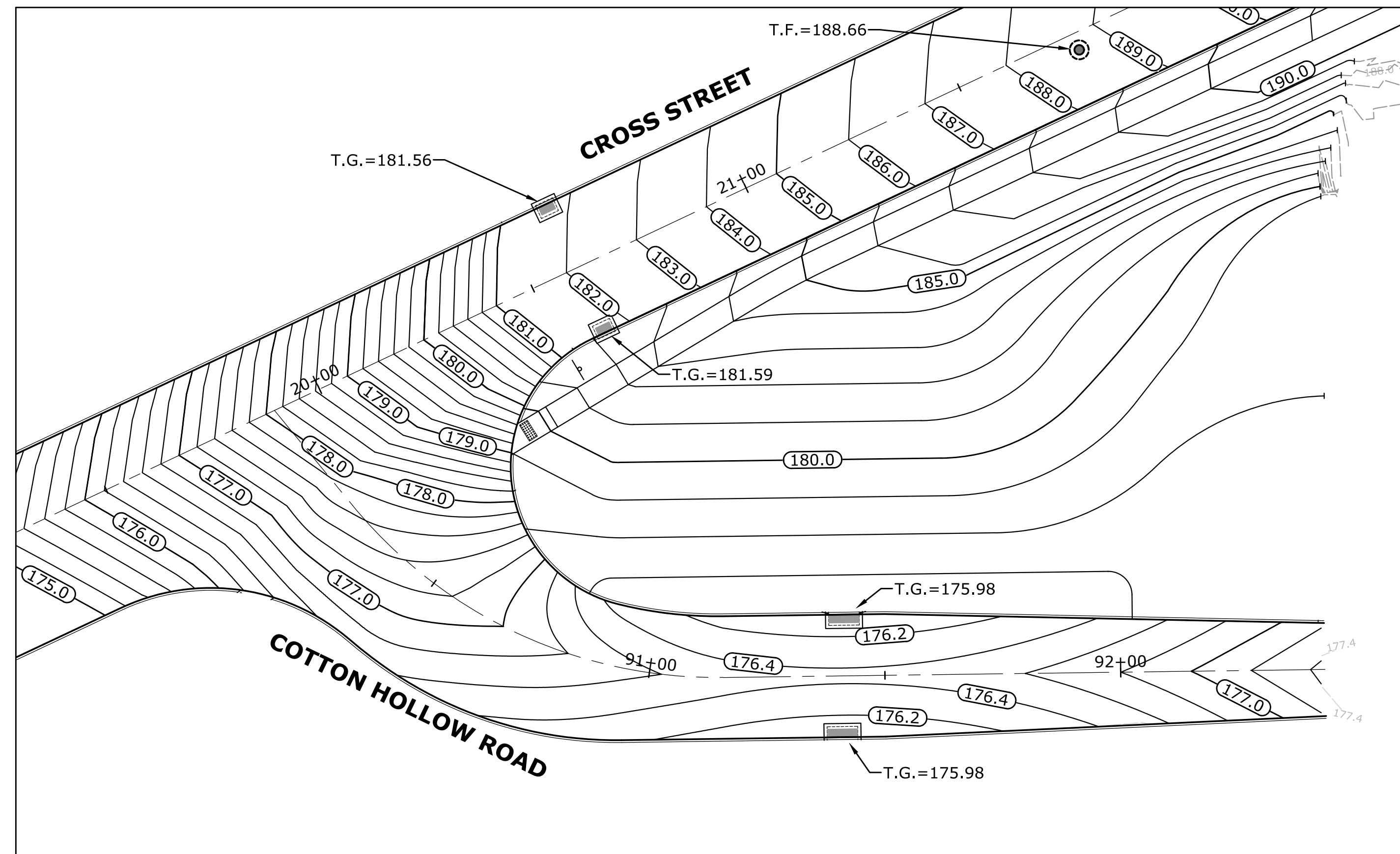
ROUTE 8 RAMPS AND CROSS STREET



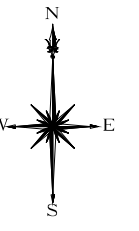
ROUTE 8 NORTH RAMPS AND CROSS STREET



ANDRASKO ROAD AND CROSS STREET



COTTON HOLLOW ROAD AND CROSS STREET



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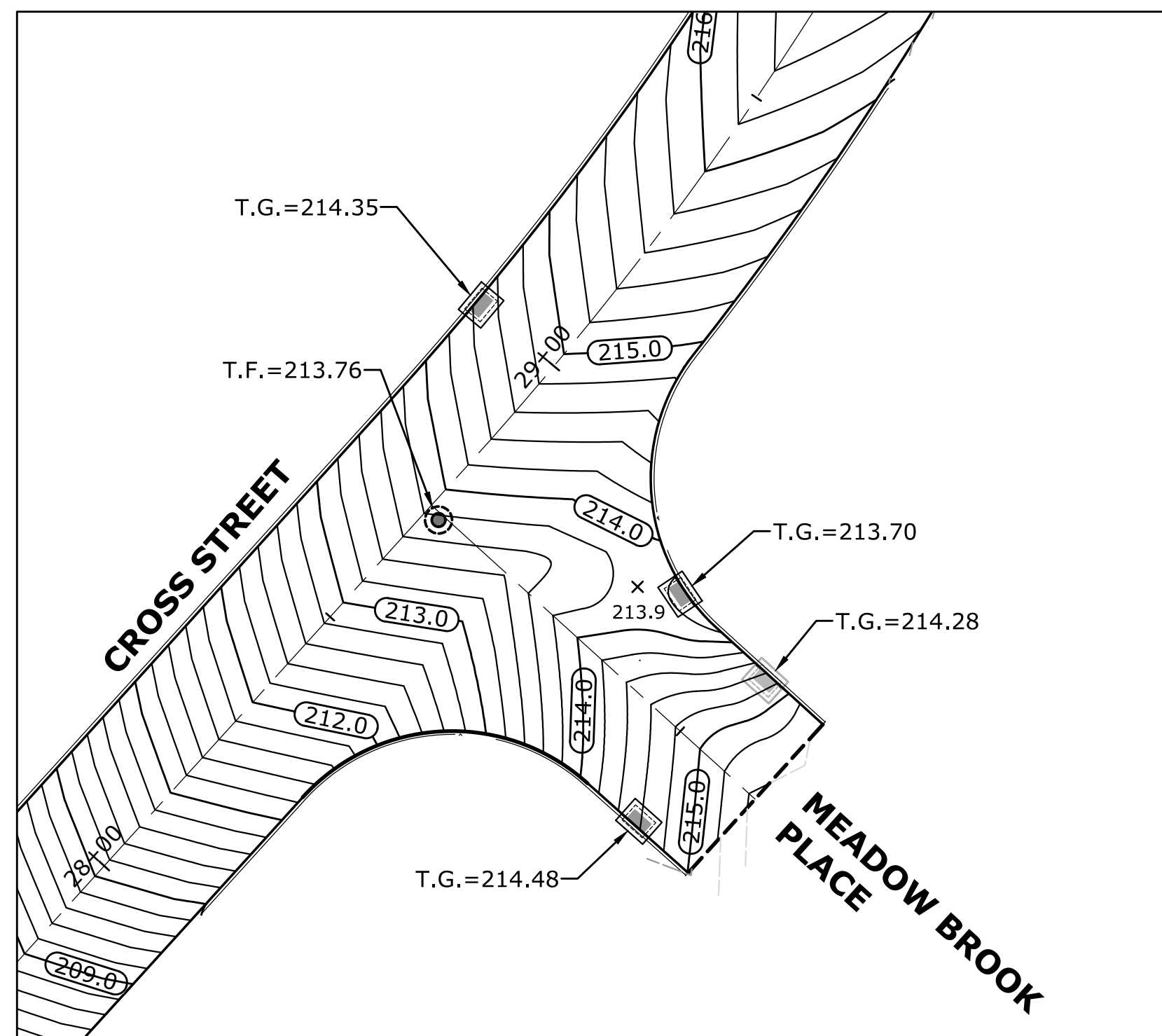
DESCRIPTION	DATE	BY

INTERSECTION GRADING PLAN  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT

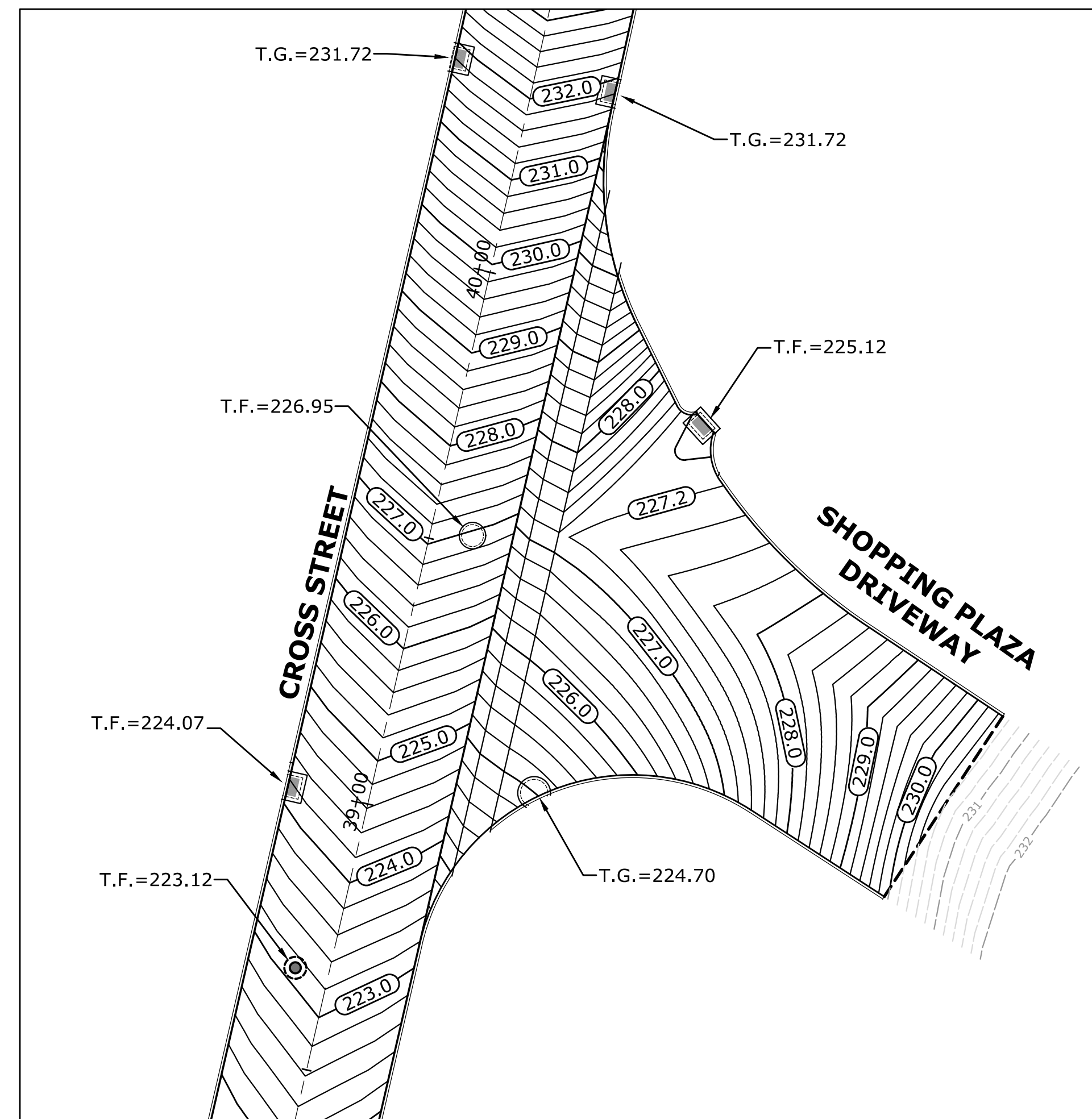
DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 2129-28		
DRAWING NO. GRA-01		

SHEET NO.  
**36**

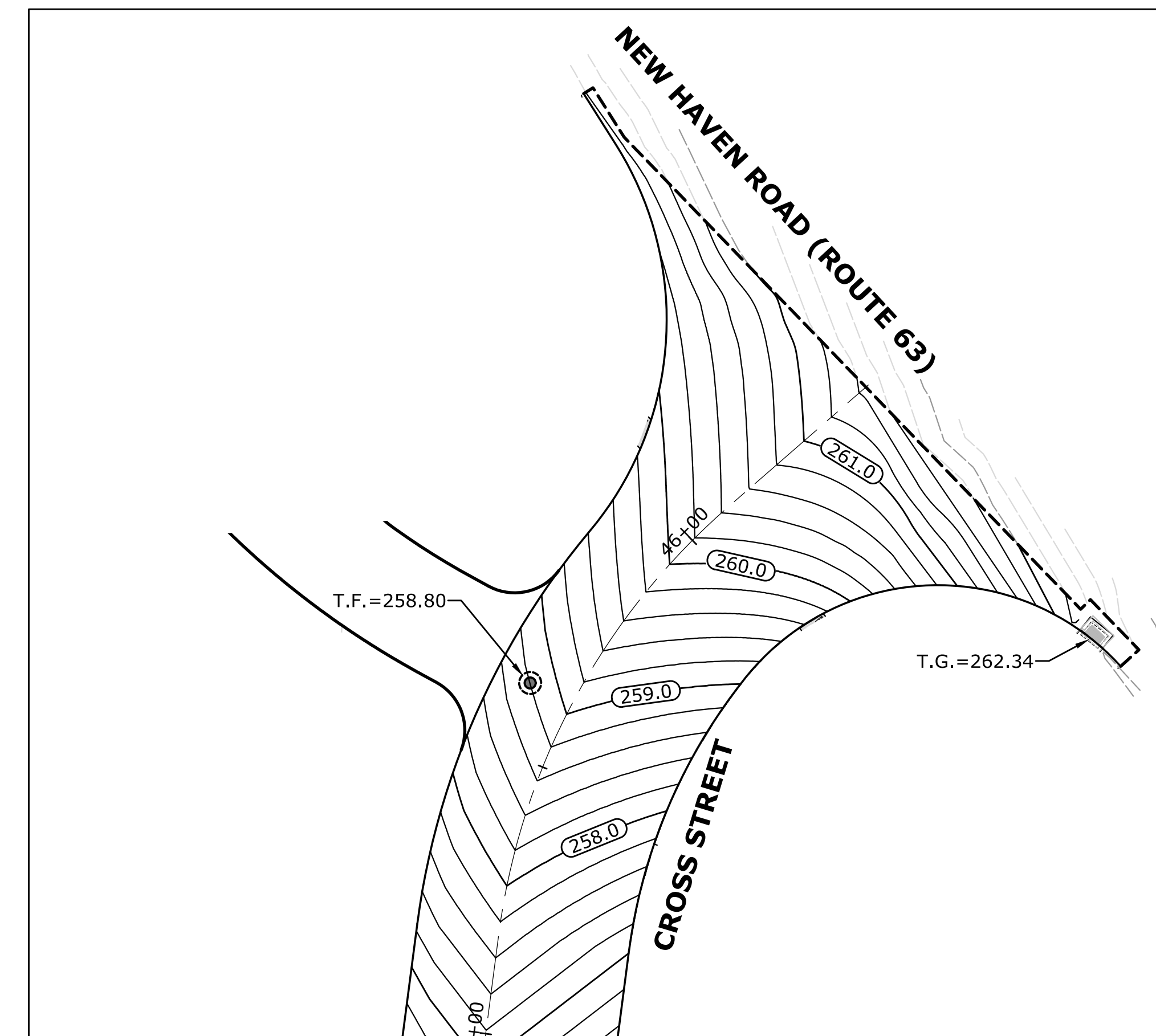




MEADOW BROOK PLACE AND CROSS STREET



SHOPPING CENTER DRIVEWAY AND CROSS STREET



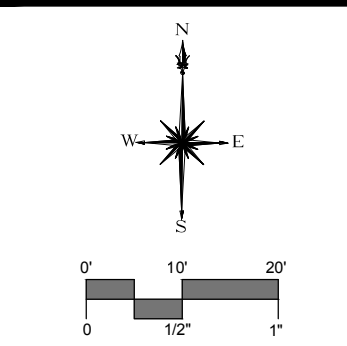
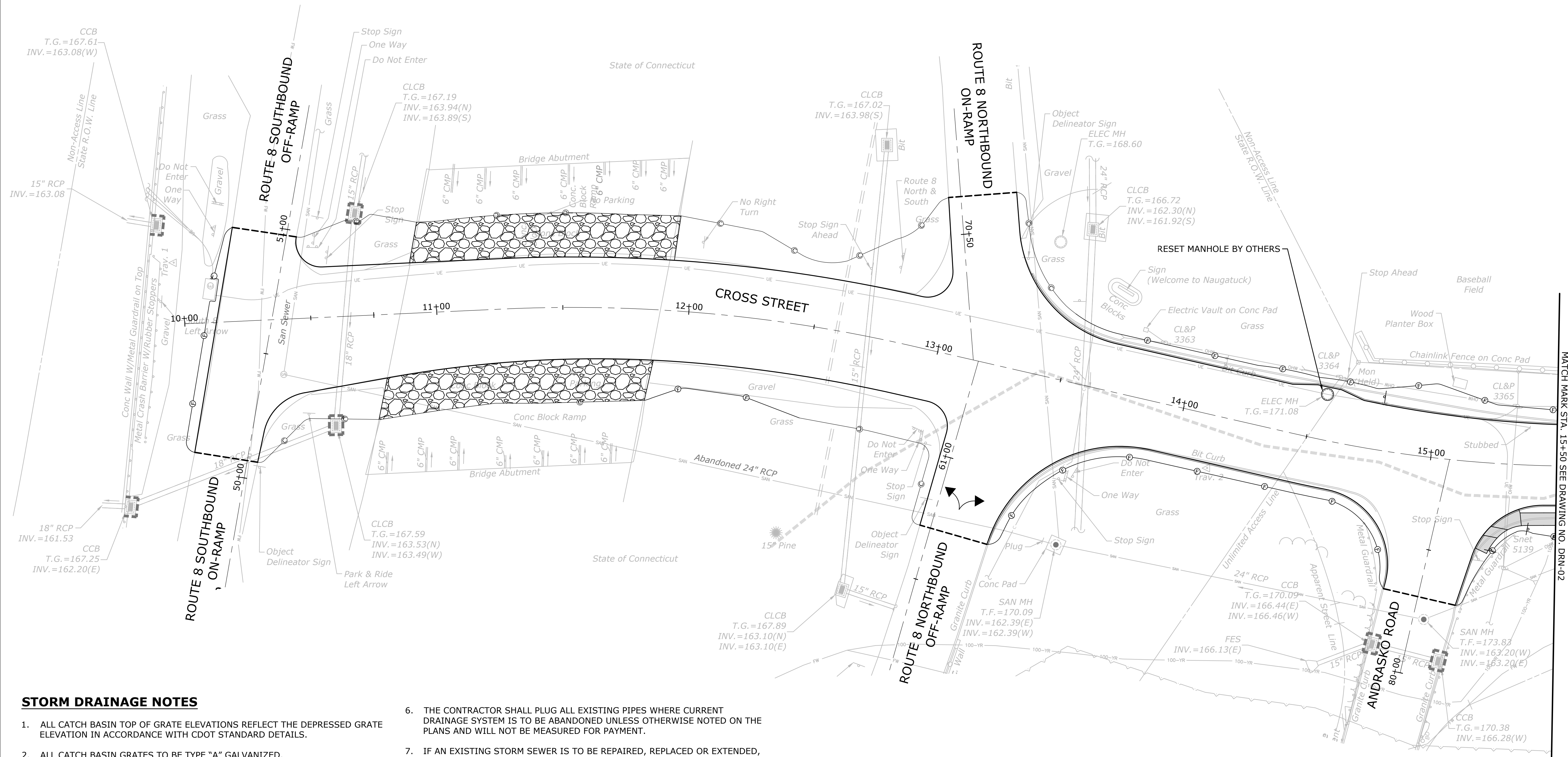
NEW HAVEN ROAD (ROUTE 63) AND CROSS STREET

DESCRIPTION	DATE	BY

INTERSECTION GRADING PLAN  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 2129-28		
DRAWING NO. GRA-02		





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DESCRIPTION	DATE	BY

MATCH MARK STA. 15+50 SEE DRAWING NO. DRN-02  
 DRAINAGE PLAN  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT

**STORM DRAINAGE NOTES**

- ALL CATCH BASIN TOP OF GRATE ELEVATIONS REFLECT THE DEPRESSED GRATE ELEVATION IN ACCORDANCE WITH CDOT STANDARD DETAILS.
- ALL CATCH BASIN GRATES TO BE TYPE "A" GALVANIZED.
- SILT SACKS MUST BE INSTALLED AT ALL EXISTING AND NEW CATCH BASINS WITHIN AND IMMEDIATELY ADJACENT TO THE PROJECT AREA AND SHALL BE PAID FOR UNDER "SEDIMENTATION CONTROL SYSTEM AT CATCH BASIN".
- ANY EXISTING PROPERTY DRAINS OR FOOTING DRAINS ENCOUNTERED SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION AND RECONNECTED TO NEW DRAINAGE STRUCTURES. THE COST OF THIS WORK SHALL BE PAID FOR UNDER ITEM NO. 0751501A "DRAINAGE PIPE LATERAL".
- EXISTING PIPE OR OTHER DRAINAGE STRUCTURES DESIGNATED FOR REMOVAL ON THE PLANS SHALL BE PAID FOR UNDER THE APPROPRIATE "TRENCH EXCAVATION" AND/OR "ROCK IN TRENCH EXCAVATION" ITEM PER CDOT FORM 817. REMOVAL OF EXISTING DRAINAGE PIPE AND STRUCTURES WHEN ENCOUNTERED DURING EXCAVATION FOR NEW DRAINAGE PIPE AND STRUCTURES SHALL NOT BE MEASURED FOR PAYMENT.
- THE CONTRACTOR SHALL PLUG ALL EXISTING PIPES WHERE CURRENT DRAINAGE SYSTEM IS TO BE ABANDONED UNLESS OTHERWISE NOTED ON THE PLANS AND WILL NOT BE MEASURED FOR PAYMENT.
- IF AN EXISTING STORM SEWER IS TO BE REPAIRED, REPLACED OR EXTENDED, IT IS A WORKING LINE AND MUST BE OPERATIONAL (CONTINUE TO FUNCTION) DURING EVENINGS AND WEEKENDS AS WELL AS ANY OTHER NON-WORKING HOURS.

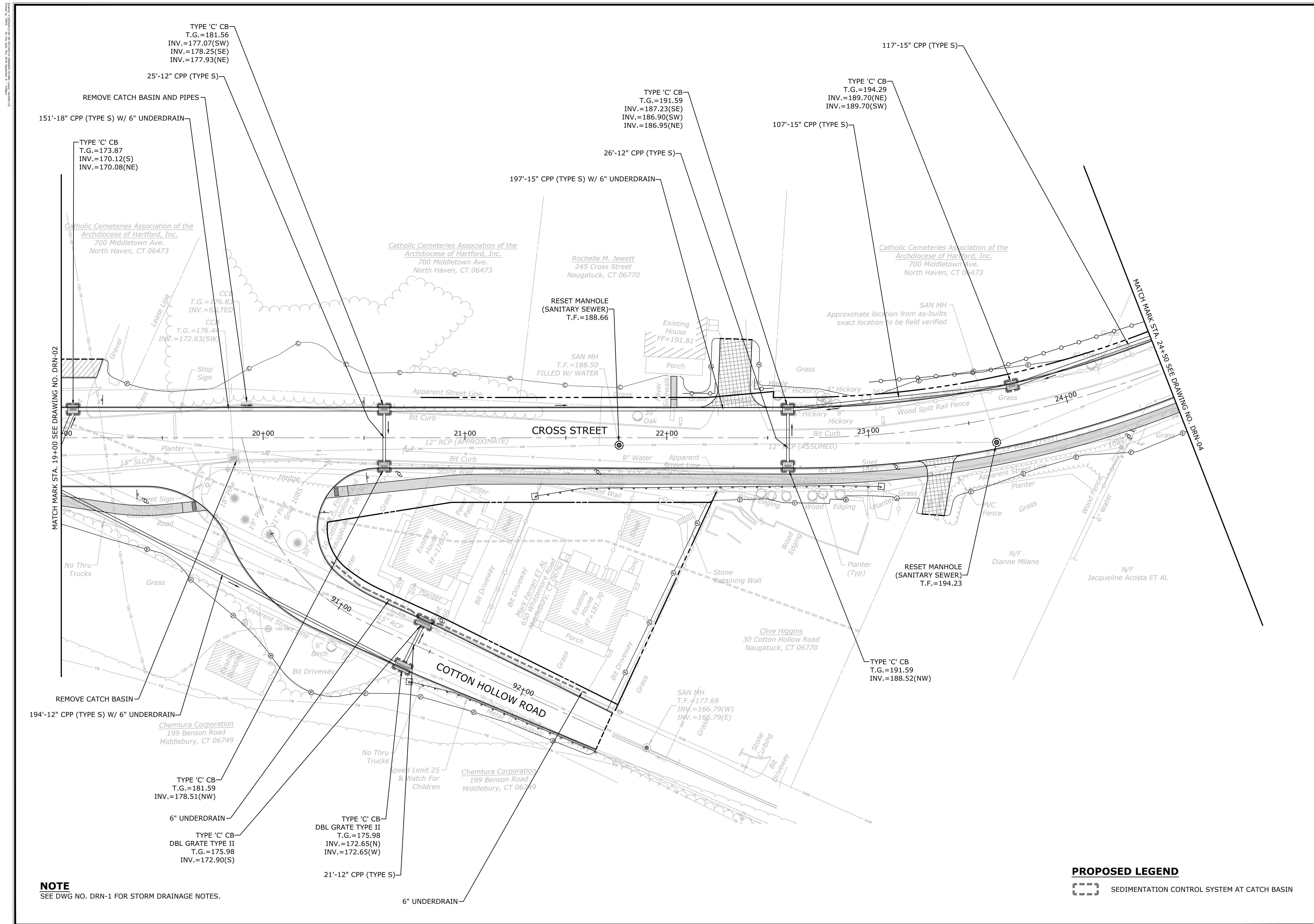
**PROPOSED LEGEND**

SEDIMENTATION CONTROL SYSTEM AT CATCH BASIN

DP	TDS	DP
DESIGNED	DRAWN	CHECKED
SCALE		
1"=20'		
DATE		
SEPTEMBER 7, 2018		
PROJECT NO.		
87-145		
DRAWING NO.		
DRN-01		
SHEET NO.		
38		

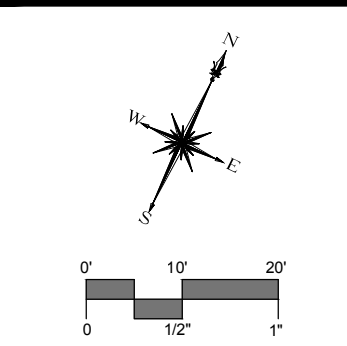






**NOTE**  
SEE DWG NO. DRN-1 FOR STORM DRAINAGE NOTES.

**PROPOSED LEGEND**  
 SEDIMENTATION CONTROL SYSTEM AT CATCH BASIN



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DESCRIPTION	DATE	BY

**DRAINAGE PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	DP
DESIGNED	DRAWN	CHECKED
SCALE		
1"=20'		
DATE		
SEPTEMBER 7, 2018		
PROJECT NO.		
87-145		
DRAWING NO.		
DRN-03		

**40**

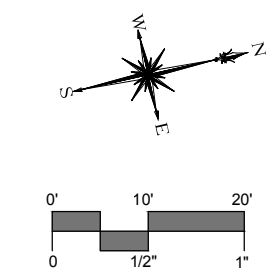








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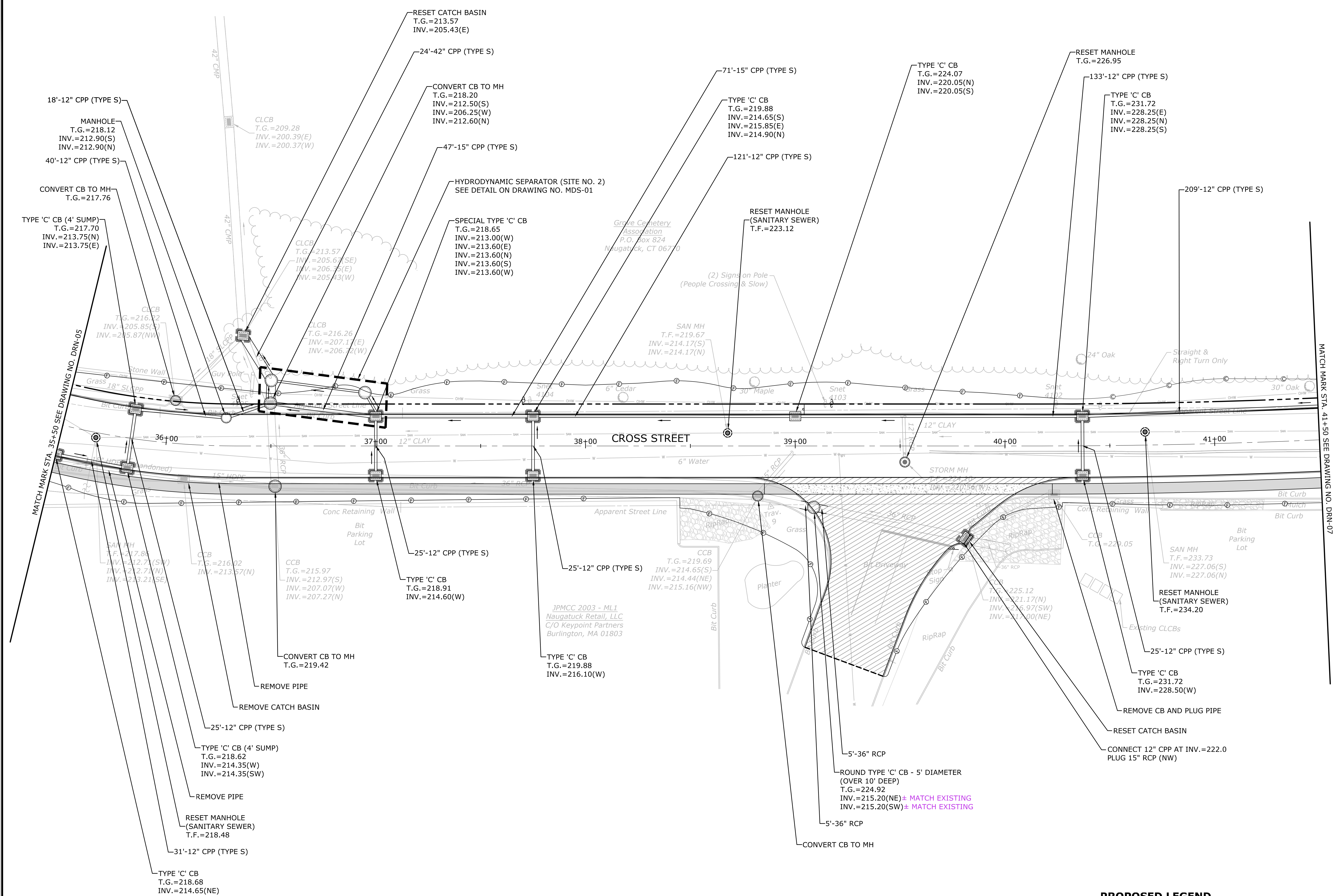
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DESCRIPTION	DATE	BY

**DRAINAGE PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	DP CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: DRN-06		

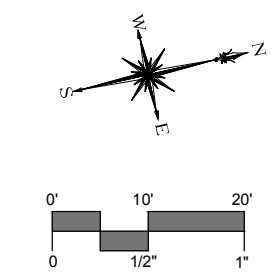
SHEET NO. **43**  
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**NOTE**  
 SEE DWG NO. DRN-1 FOR STORM DRAINAGE NOTES.

**PROPOSED LEGEND**  
 SEDIMENTATION CONTROL SYSTEM AT CATCH BASIN





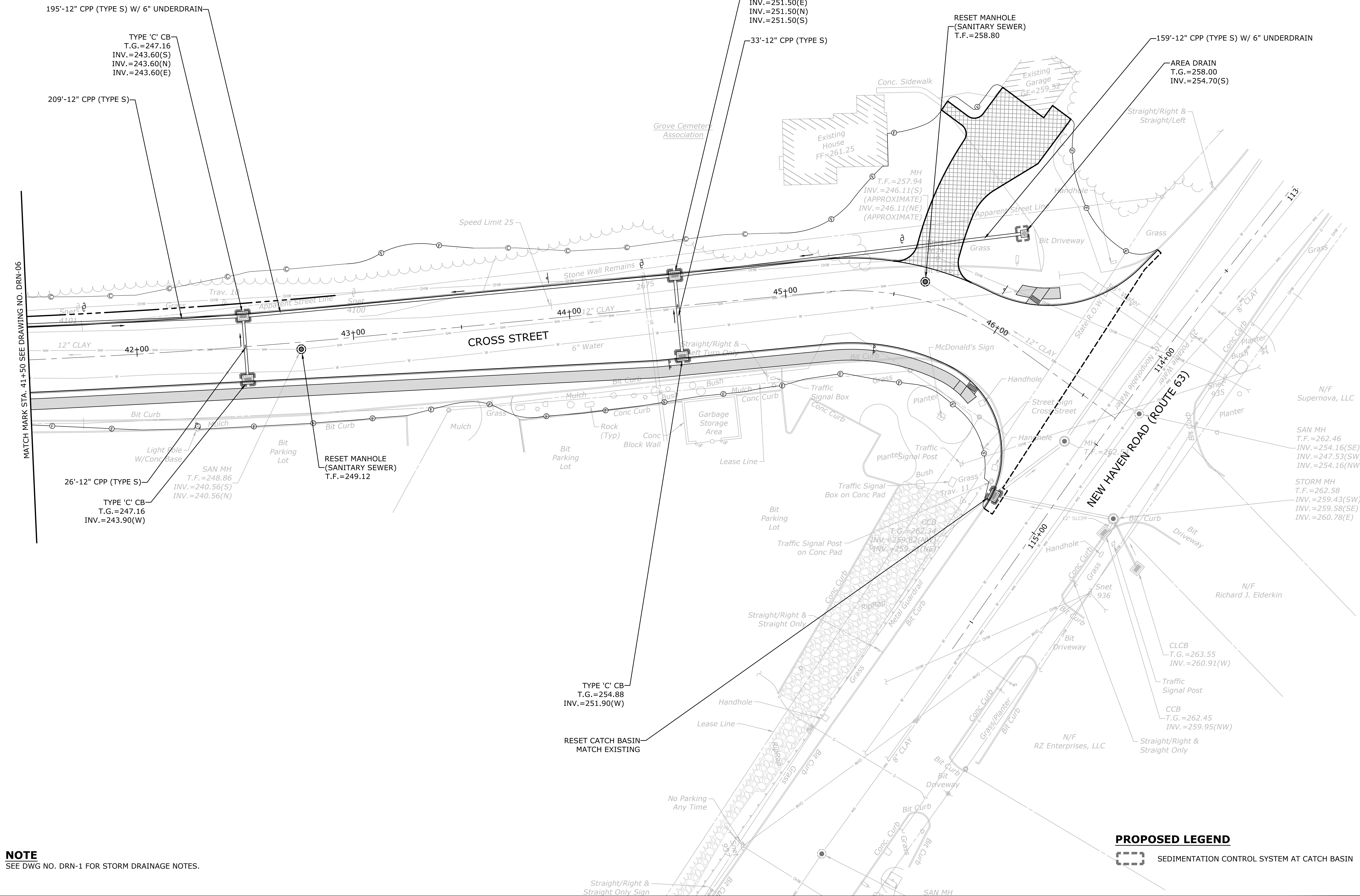
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DESCRIPTION	DATE	BY

**DRAINAGE PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	DP
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. DRN-07		

**44**  
 SHEET NO.



**NOTE**  
 SEE DWG NO. DRN-1 FOR STORM DRAINAGE NOTES.

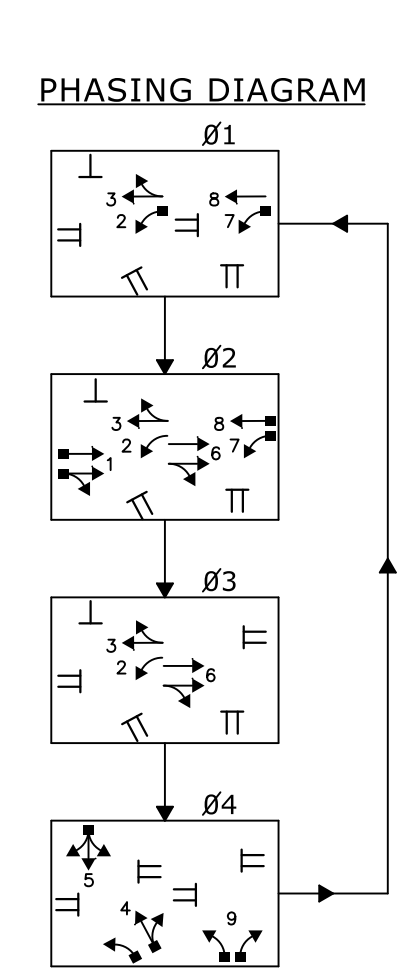
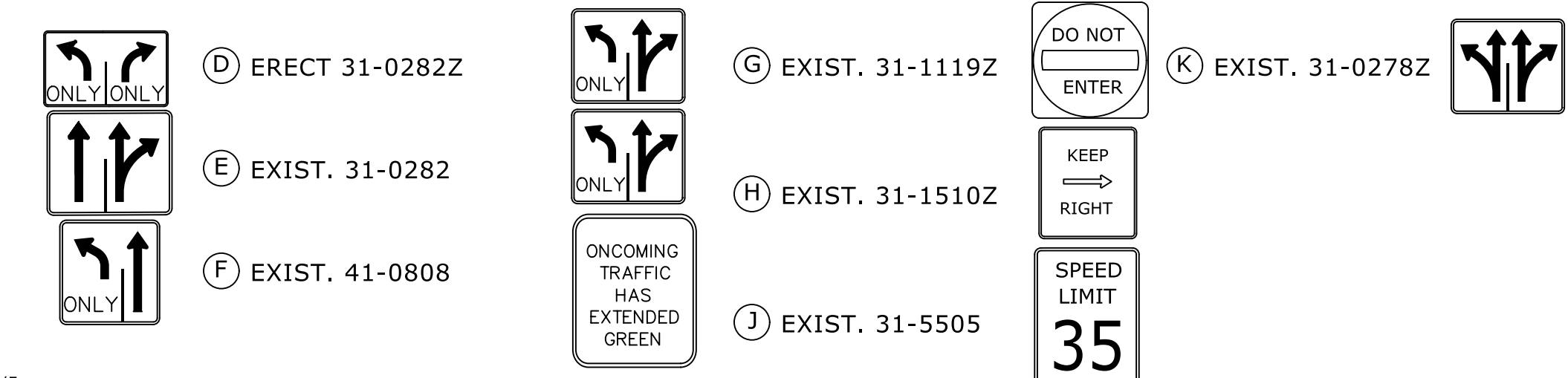
**PROPOSED LEGEND**  
 SEDIMENTATION CONTROL SYSTEM AT CATCH BASIN

MOVEMENT DIAGRAM									
NTOR	PHASE 1	PHASE 2	PHASE 3	PHASE 4	PHASE 5	PHASE 6	PHASE 7	PHASE 8	
NONE									
	FLASH	GRN	CL	CL	GRN	CL	CL	GRN	CL
F A C E #	1	Y	R	R	R	R	R	R	R
	2	Y	G	Y	G	G	G	G	G
	3	Y	G	G	G	G	G	G	G
	4	R	R	R	R	R	R	R	R
	5	R	R	R	R	R	R	R	R
	6	Y	R	R	R	R	R	R	R
	7	Y	G	Y	G	G	G	G	G
	8	Y	G	Y	G	G	G	G	G
	9	R	R	R	R	R	R	R	R
I N T E R V A L S	MIN GRN	5			6			9	
	WALK							16	
	PED CLR							1	
	VEH EXT	2						2	
	MAX 1	5						20	
	MAX 2	8						20	
	YELLOW							20	
	RED	3	1.0	4	2.0	3.8	2.0	3.3	1.7
	ADD INIT								
	MAX INIT								
TBR									
TTR									
MIN GAP									
MODE	MIN. RECALL		MIN. RECALL		NON-LOCK		NON-LOCK		OFF
INT START	THIS PHASE		THIS PHASE						

TECHNICAL NOTES		OFFICE RECORD	
STANDARD OVERLAP SKIP FEATURES APPLY		REV #	TIR # N/A
DETECTORS D6 & D9 TO EXTEND BUT NOT CALL PHASE 1.		SM # N/A	SIGNAL REVISED:
		INSTALLED NEW LOOPS ON CROSS STREET NORTHBOUND APPROACH, INSTALLED NEW CONDUIT AND 14/2 CABLE ACROSS CROSS STREET, AND REMOVED PROGRAMMED FLASH OPERATION.	

IDENT	SIZE	LOOP X TURNS	MODE	SYSTEM LOC MASTER	COORDINATION TYPE - NONE	PROGRAM	FUNCTION	TIME	DAYS	CYCLE	OFFSET
D1	6' x 6'	3	2" DELAY				FLASH	NONE			
D2	6' x 6'	3	PRESENCE				MAX 2	0500-1000	DAILY		
D3	8' x 6'	3	8" DELAY				MAX 1	1500-1900	DAILY		
D4	6' x 6'	3	PRESENCE				MAX 1	ALL OTHER TIMES			
D5	6' x 6'	3	8" DELAY								
D6	8' x 6'	4	PRESENCE								
D7	6' x 12'	4	PRESENCE								
D8	6' x 12'	4	PRESENCE								
D9	8' x 6'	4	PRESENCE								

- SIGN LEGEND:**
- (A) EXIST. 31-0290Z
  - (B) EXIST. 31-0219
  - (C) EXIST. 31-0222
  - (D) ERECT 31-0282Z
  - (E) EXIST. 31-0282
  - (F) EXIST. 41-0808
  - (G) EXIST. 31-1119Z
  - (H) EXIST. 31-1510Z
  - (J) EXIST. 31-5505
  - (K) EXIST. 31-0278Z



**CONSTRUCTION NOTES:**

COORDINATE WITH UTILITY COMPANY REPRESENTATIVES LISTED IN THE SPECIAL PROVISION, 1.07 - LEGAL RELATIONS AND RESPONSIBILITIES.

THE LOCATION OF TRAFFIC SIGNAL FOUNDATIONS WHEN IN OR ADJACENT TO SIDEWALKS SHALL BE VERIFIED PRIOR TO INSTALLATION TO PROVIDE A FREE PATH OF NOT LESS THAN 4 FEET. IF A MINIMUM 4 FOOT FREE PATH IS UNAVAILABLE NOTIFY THE ENGINEER AND CONTACT THE DIVISION OF TRAFFIC ENGINEERING.

ALL TRAFFIC SIGNAL EQUIPMENT IS EXISTING EXCEPT AS NOTED.

STAKE ALL R.O.W. PRIOR TO EXCAVATION.

INSTALL NEW D2 AND D3 LOOPS.

INSTALL LOOP DETECTORS CENTERED IN LANE AND 8 FEET APART.

(W) INSTALL 30" X 30" HANDHOLE. ALL OTHERS TYPE II.

(X) ABANDON EXISTING 2" RMC CONDUIT FROM ACROSS CROSS STREET AND INSTALL NEW CONDUIT AND WIRING AS SHOWN.

CLEAN EXISTING CONDUIT WHEN INSTALLING NEW CABLE WITHIN THE CONDUIT.

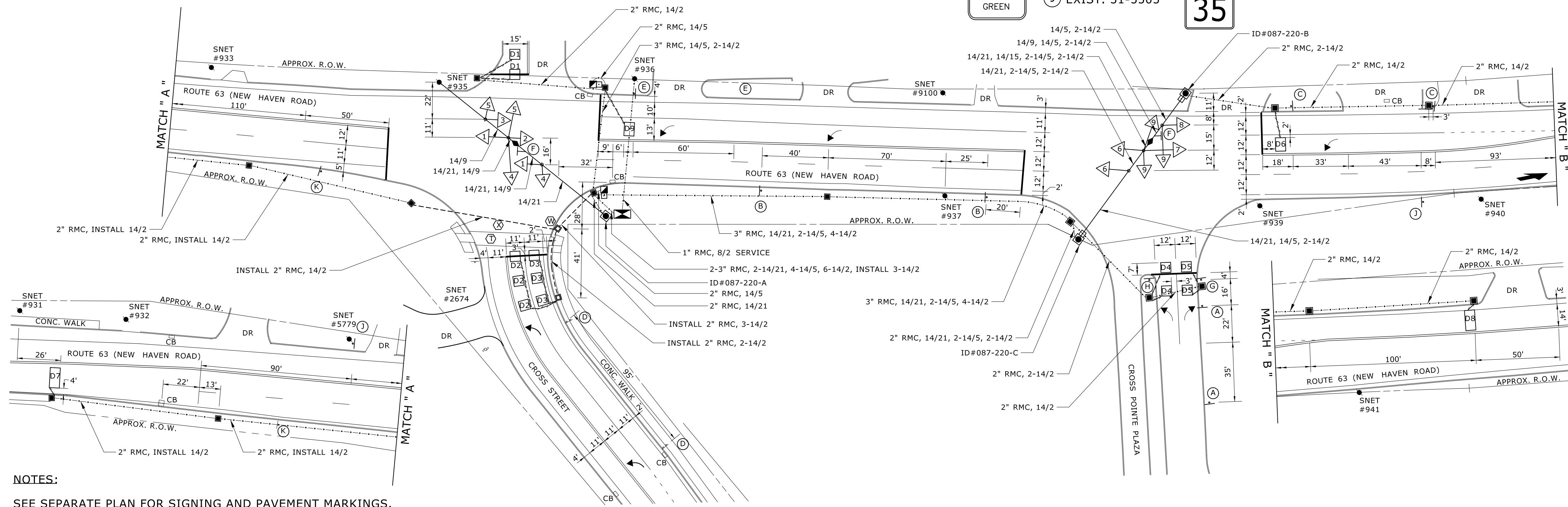
INSTALL NEW 14/2 LEAD IN CABLE WITHOUT SPLICES TO CONTROLLER.

SERIES SPLICE SEGMENTED LOOPS PER LANE.

ANY PROPOSED REVISIONS TO THE LOCATION OF THE APPURTENANCES SHOWN ON THE PLAN MUST BE SUBMITTED FOR REVIEW AND APPROVAL BY THE DIVISION OF TRAFFIC ENGINEERING PRIOR TO INSTALLATION.

MODIFY EXISTING CONTROLLER TO ACCOMMODATE CHANGES. SUPPLY 5 COPIES OF REVISED CABINET WIRING DIAGRAMS.

REMOVE ALL ABANDONED TRAFFIC SIGNAL EQUIPMENT PER SPECIAL PROVISION.



**2015 VOLUMES**

3	1	0	37	4	3
23	4	14	34	0	5
10			15		16
466	430	518	547	772	734
587			196	31	31
47	140	48	43		
29			25	0	36
			41	1	59

XXX AM PEAK HOUR  
XXX PM PEAK HOUR

**2035 DESIGN YEAR VOLUMES**

5	0	0	45	5	5
30	5	20	45	0	5
15			20		20
570	525	635	685	955	895
720			40	40	40
60	240	170	55		
35			40	5	190
			50	15	430
			30	0	45
			50	0	75

XXX AM PEAK HOUR  
XXX PM PEAK HOUR

**NOTES:**

SEE SEPARATE PLAN FOR SIGNING AND PAVEMENT MARKINGS.

STATE TO MAINTAIN ALL SIGNS AND PAVEMENT MARKINGS ON ROUTE 63 AND ALL STOP BARS.

BOROUGH OF NAUGATUCK TO MAINTAIN ALL SIGNS AND ALL OTHER PAVEMENT MARKINGS ON CROSS STREET.

OTHERS TO MAINTAIN ALL SIGNS AND ALL OTHER PAVEMENT MARKINGS ON PRIVATE DRIVEWAY.

(T) BAR TYPE CROSSWALK (16" BARS x 24" SPACES x 8' MIN) - TOWN MAINTAINED

(W) 30" X 30" CONCRETE HANDHOLE.

**SPAN POLE INFORMATION:**

087-220-A: HEIGHT 32', B.C. 22", LOAD AT YIELD 10,000 LBS, INSTALLED 1991.

087-220-B: HEIGHT 30', B.C. 20", LOAD AT YIELD 7,800 LBS, INSTALLED 1991.

087-220-C: HEIGHT 30', B.C. 20", LOAD AT YIELD 7,800 LBS, INSTALLED 1991.

ENGINEER	TRAFFIC DATE	ELECTRICAL DATE	REV #	INTERSECTION # 087-220	
DRAWN BY	MILONE & MACBROOM		ENERGY BY - TOWN		
CHECKED BY	99 Realty Drive Cheshire, Connecticut 06014 (203) 271-1775 Fax (203) 271-0773 www.miloneandmacbroom.com		ADDRESS #		
SUBMITTED BY			MAINT LEVEL - 5		
APPROVED BY			SERVICE POLE # SNET 936		
APPROVED DATE			UNMETERED SERVICE		
			TOWN:	PROJECT NO.	
			BOROUGH OF NAUGATUCK		87-145
			DRAWING TITLE:		DRAWING NO.
			TRAFFIC CONTROL SIGNAL PLAN		TCS-01
					SHEET NO.
					45

SCALE 1" = 40'

<p>LEGEND:</p> <p>R RED</p> <p>Y YELLOW</p> <p>G GREEN</p> <p>← RED ARROW</p> <p>↔ YELLOW ARROW</p> <p>↔ GREEN ARROW</p> <p>W/W WALK/PED. CLR</p> <p>D.W. DON'T WALK</p> <p>FL. FLASHING</p>	<p>○ PROPOSED WOOD SPAN POLE</p> <p>● EXISTING WOOD SPAN POLE</p> <p>○ PROPOSED STEEL SPAN POLE</p> <p>● EXISTING STEEL SPAN POLE</p> <p>○ PROPOSED UTILITY POLE</p> <p>● EXISTING UTILITY POLE</p> <p>□ PEDESTAL MOUNTING</p> <p>□ PEDESTAL PUSH BUTTON &amp; SIGN</p> <p>□ DIRECTIONAL ARW. FOR PUSH BUTTON</p>	<p>□ TRAFFIC SIGNAL FACE</p> <p>□ PEDESTRIAN SIGNAL FACE</p> <p>□ DET. LEADS IN SAW CUT</p> <p>□ PROPOSED RMC (RIGID METAL CONDUIT)</p> <p>□ EXISTING RMC (RIGID METAL CONDUIT)</p> <p>□ AUXILIARY TERMINATION CABINET</p> <p>□ AUXILIARY EQUIPMENT CABINET</p> <p>□ VIDEO DETECTION ZONE</p>	<p>□ PROPOSED CONTROLLER</p> <p>□ EXISTING CONTROLLER</p> <p>□ LOOP DETECTOR</p> <p>□ SD SYSTEM DETECTOR</p> <p>□ MAGNETIC DETECTOR</p> <p>□ OPTICAL DETECTOR</p> <p>□ VIDEO DETECTOR</p> <p>□ AUDIO DETECTOR</p>	<p>□ SIDEWALK RAMP</p> <p>□ CABLE CLOSURE</p> <p>□ WIRELESS SENSOR</p> <p>□ WIRELESS RECEIVER</p> <p>□ WIRELESS TRANSMITTER</p> <p>□ GUY WIRE</p> <p>□ PROPOSED HANDHOLE</p> <p>□ EXISTING HANDHOLE</p>
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DATE PLOTTED : 9/6/2018 V:\Design\2129-28-DE\CAD\CSS-TCS.dgn

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

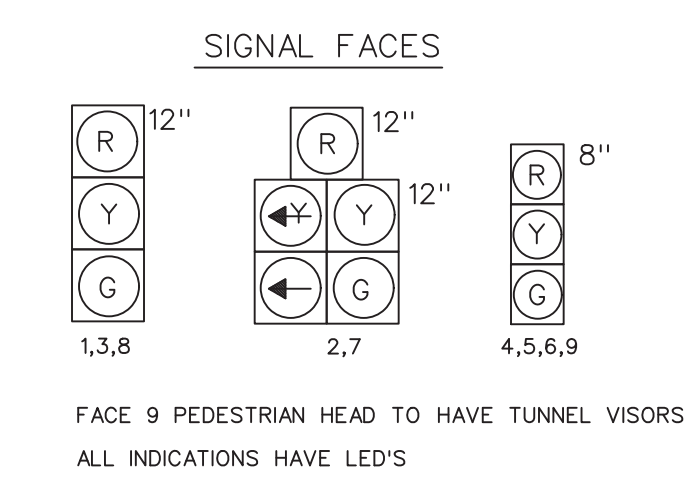


MOVEMENT DIAGRAM																												
NTOR	PHASE 1			PHASE 2			PHASE 3			PHASE 4			PHASE 5			PHASE 6			PHASE 7			PHASE 8						
NONE																												
F A C E #	FLASH	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL			
	1	Y	R	R	R	G	G	Y	R	R	R	R	R	R														
	2	Y	R	G	G	G	G	G	R	R	R	R	R	R														
	3	Y	G	G	G	G	G	G	Y	R	R	R	R	R														
	4	R	R	R	R	R	R	R	R	R	R	R	R	R														
	5	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R												
	6	Y	R	R	R	R	R	R	R	R	R	R	R	R														
	7	Y	R	G	G	G	G	G	Y	R	R	R	R	R	R	R												
	8	Y	G	G	G	G	G	G	Y	R	R	R	R	R	R	R												
	9	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R												
I N T E R V A L S	MIN GRN	4			20			6			9																	
	WALK										19																	
	PED CLR										1																	
	VEH EXT	2			5			2			2																	
	MAX 1	5			35			6			20																	
	MAX 2	8			40			6			20																	
	YELLOW				3.0			1.0			4.0			2.0			3.8			2.0			3.3			1.7		
	RED																											
	ADD INIT				1.6																							
	MAX INIT				35																							
TBR																												
TTR																												
MIN GAP																												
MODE	MIN RECALL			MIN RECALL			NON-LOCK			NON-LOCK			OFF			OFF			OFF			OFF						
INT START				THIS PHASE																								

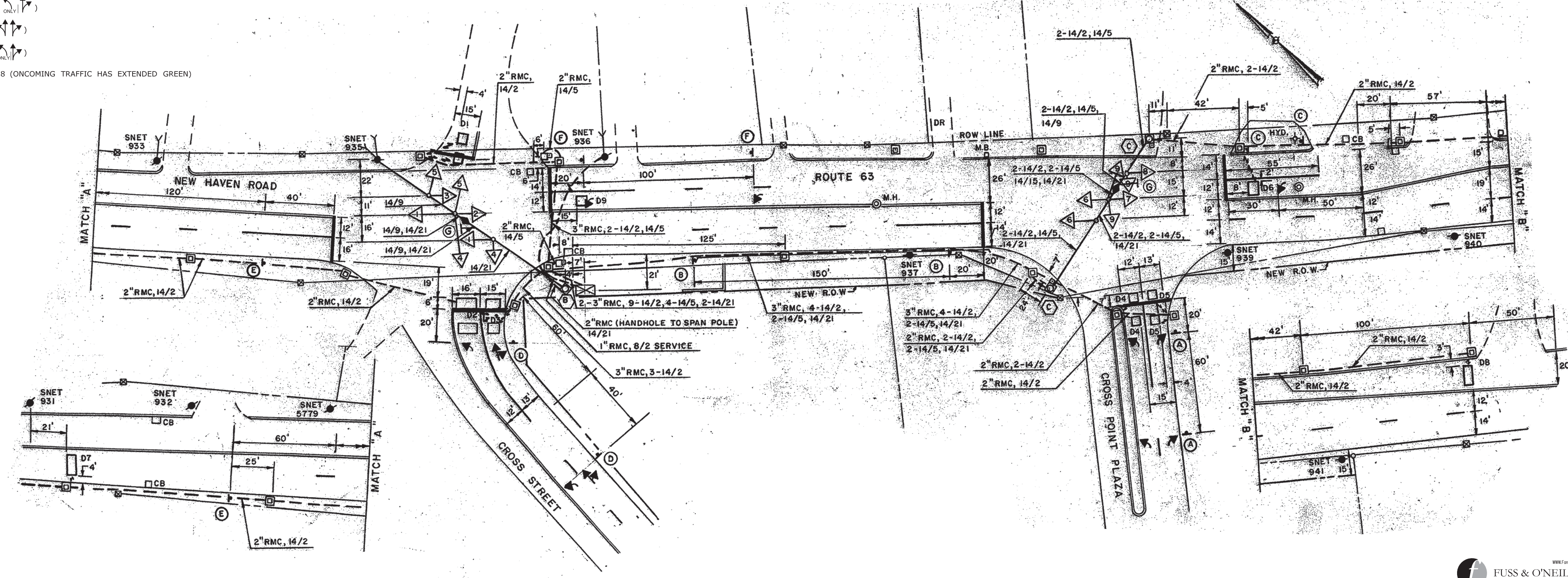
TECHNICAL NOTES  
 STANDARD OVERLAP SKIP FEATURES APPLY  
 PHASE 2 ON TO OMIT PHASE 1  
 PHASE 3 ALWAYS PRECEDES PHASE 4  
 PHASE 1 ALWAYS PRECEDES PHASE 2  
 DETECTORS D6 & D9 TO EXTEND BUT NOT CALL PHASE 1  
 PHASE 3 ON TO CALL PHASE 4

OFFICE RECORD  
 REV # 11 TIR # N/A SM # 100624 SIGNAL REVISED: 03/25/2016

IDENT	SIZE (WXL)	TURNS	MODE	SYSTEM LOC	COORDINATION TYPE: NONE			
					FUNCTION	PROGRAM TIME	DAYS	OFFSET
D1	6'X6'	3	2" DELAY		FLASH	2300-0500	DAILY	
D2	12'X6'	3	2" DELAY		MAX 1	ALL OTHER TIMES		
D3	8'X6'	3	8" DELAY		MAX 2	0500-1000	DAILY	
D4	6'X6'	3	PRESENCE			1500-1900		
D5	6'X6'	3	8" DELAY					
D6	8'X6'	4	PRESENCE					
D7	6'X12'	4	PRESENCE					
D8	6'X12'	4	PRESENCE					
D9	6'X6'	4	PRESENCE					



- SIGN LEGEND:
- (A) EXIST 31-0290Z (one-way)
  - (B) EXIST 31-0219 (two-way)
  - (C) EXIST 31-0222 (one-way)
  - (D) EXIST 31-0282Z (two-way)
  - (E) EXIST 31-0278 (two-way)
  - (F) EXIST 31-0282 (two-way)
  - (G) SPAN MTD. 41-4638 (ONCOMING TRAFFIC HAS EXTENDED GREEN)



SPAN POLE INFORMATION

- (B) 32", 22" BOLT CIRCLE, 10,000 LB LOAD AT YIELD.
- (C) 30", 20" BOLT CIRCLE, 7,800 LB LOAD AT YIELD.

FOR REFERENCE ONLY

<p>LEGEND:</p> <ul style="list-style-type: none"> <li>R RED</li> <li>Y YELLOW</li> <li>G GREEN</li> <li>←R RED ARROW</li> <li>←Y YELLOW ARROW</li> <li>←G GREEN ARROW</li> <li>W WALK/ PED. CLR</li> <li>D.W. DON'T WALK</li> <li>FL. FLASHING</li> </ul>		<ul style="list-style-type: none"> <li>○ PROPOSED WOOD SPAN POLE</li> <li>● EXISTING WOOD SPAN POLE</li> <li>□ PROPOSED STEEL SPAN POLE</li> <li>● EXISTING STEEL SPAN POLE</li> <li>○ PROPOSED UTILITY POLE</li> <li>● EXISTING UTILITY POLE</li> <li>□ PEDESTAL MOUNTING</li> <li>□ PEDESTRIAN PUSH BUTTON &amp; SIGN</li> <li>◀ DIRECTIONAL ARW. FOR PUSH BUTTON</li> </ul>		<ul style="list-style-type: none"> <li>← TRAFFIC SIGNAL FACE</li> <li>← PEDESTRIAN SIGNAL FACE</li> <li>DET. LEADS IN SAW CUT</li> <li>— PROPOSED RMC (RIGID METAL CONDUIT)</li> <li>— EXISTING RMC (RIGID METAL CONDUIT)</li> <li>— AUXILIARY TERMINATION CABINET</li> <li>— AUXILIARY EQUIPMENT CABINET</li> <li>— VIDEO DETECTION ZONE</li> </ul>		<ul style="list-style-type: none"> <li>□ PROPOSED CONTROLLER</li> <li>□ EXISTING CONTROLLER</li> <li>□ LOOP DETECTOR</li> <li>□ SD SYSTEM DETECTOR</li> <li>□ MAGNETIC DETECTOR</li> <li>□ OPTICAL DETECTOR</li> <li>□ VIDEO DETECTOR</li> <li>□ AUDIO DETECTOR</li> </ul>		<ul style="list-style-type: none"> <li>▽ SIDEWALK RAMP</li> <li>□ CABLE CLOSURE</li> <li>□ WIRELESS SENSOR</li> <li>□ WIRELESS RECEIVER</li> <li>□ WIRELESS TRANSMITTER</li> <li>□ GUY WIRE</li> <li>□ PROPOSED HANDHOLE</li> <li>□ EXISTING HANDHOLE</li> </ul>		<p>STATE OF CONNECTICUT          DEPARTMENT OF TRANSPORTATION</p>		<p>ROUTE 63 (NEW HAVEN ROAD) AT          CROSS ST AND CROSS POINT PLAZA DRIVE</p>		<p>REV #11 INTERSECTION # 087-220</p>		<p>ENERGY BY- TOWN ADDRESS #          MAINT LEVEL 5 SERVICE POLE - SNET 936          UNMETERED SERVICE</p>		<p>PROJECT NO.          174-387</p>		<p>DRAWING NO.          SHEET NO.</p>	
NO. DATE REVISION DESCRIPTION		DATE PLOTTED: 5/14/2016 \\\DOT-SDCENG07\CTDOT_Projects\0174-0387\Traffic\FINAL DESIGN\Naugatuck - 87\TR_MSH_174-387_087-220.dgn		SCALE 1" = 40'		TOWN: NAUGATUCK		DRAWING TITLE: TRAFFIC CONTROL SIGNAL PLAN													

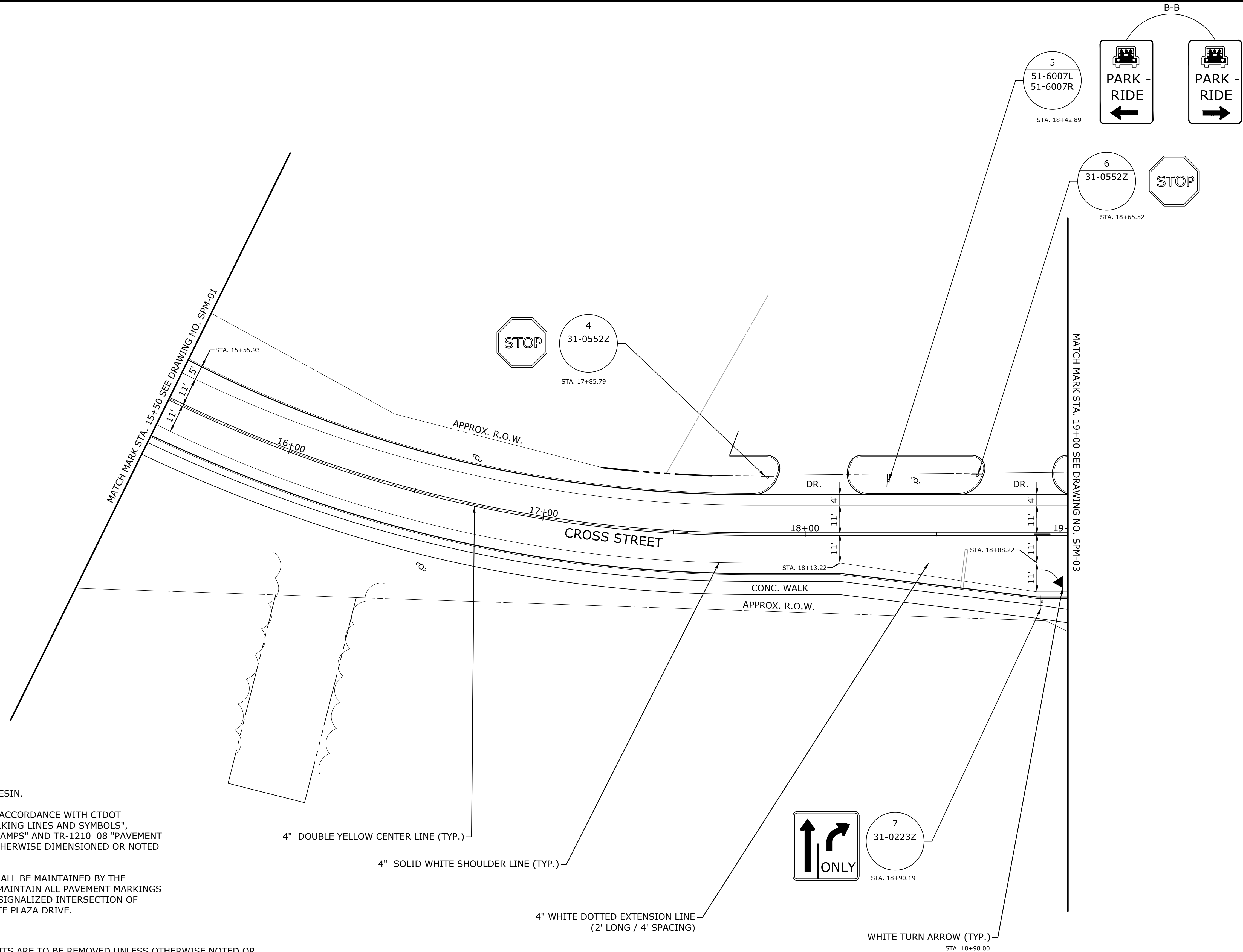






**LEGEND**

- ↖ SINGLE POST-MOUNTED SIGN
- ↖↖ DOUBLE POST-MOUNTED SIGN
- EX = EXISTING TO REMAIN

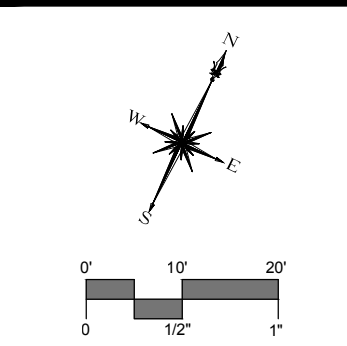


**PAVEMENT MARKING NOTES**

1. ALL NEW PAVEMENT MARKINGS TO BE EPOXY RESIN.
2. PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH CTDOT STANDARD SHEET TR-1210\_04 "PAVEMENT MARKING LINES AND SYMBOLS", TR-1210\_07 "PAVEMENT MARKINGS FOR EXIT RAMP" AND TR-1210\_08 "PAVEMENT MARKINGS FOR NON FREEWAYS" EXCEPT AS OTHERWISE DIMENSIONED OR NOTED ON THE PLANS.
3. ALL PAVEMENT MARKINGS ON TOWN ROADS SHALL BE MAINTAINED BY THE BOROUGH OF NAUGATUCK. THE STATE SHALL MAINTAIN ALL PAVEMENT MARKINGS ON STATE ROADS AND ALL STOP BARS AT THE SIGNALIZED INTERSECTION OF ROUTE 63 AT CROSS STREET AND CROSS POINTE PLAZA DRIVE.

**SIGNING NOTES**

1. ALL EXISTING SIGNS WITHIN THE PROJECT LIMITS ARE TO BE REMOVED UNLESS OTHERWISE NOTED OR DIRECTED BY ENGINEER. EXISTING SIGNS TO REMAIN SHALL BE REMOVED AND RESET OR RELOCATED, AS REQUIRED, TO ACCOMMODATE PROPOSED IMPROVEMENTS. THIS WORK SHALL BE PAID FOR UNDER ITEM NO. 1206023A- "REMOVAL AND RELOCATION OF EXISTING SIGNS."
2. INSTALL SIGNS AND METAL SIGNS POSTS IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208\_01 "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS" AND TR-1208\_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
3. SIGNS SHALL BE INSTALLED AT LEAST 10 FEET FROM ALL UTILITY POLES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER
4. THE BOROUGH SHALL MAINTAIN ALL SIGNS ON TOWN ROADS. THE STATE SHALL MAINTAIN ALL SIGNS ON STATE ROADS.
5. ALL STREET NAME SIGNS WITHIN THE PROJECT LIMITS SHALL BE RELOCATED ONTO NEW POSTS AS DIRECTED BY THE ENGINEER. THIS WORK IS PAYABLE UNDER ITEM NO. 1206023A - REMOVAL AND RELOCATION OF EXISTING SIGNS.
6. EXACT LOCATION OF SIGNS TO BE VERIFIED BY ENGINEER.



**MILONE & MACBROOM**  
 99 Realty Drive  
 Cheshire, Connecticut 06410  
 (203) 271-1773 Fax (203) 272-9733  
 www.miloneandmacbroom.com

DESCRIPTION	DATE	BY

**SIGNING AND PAVEMENT MARKING PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

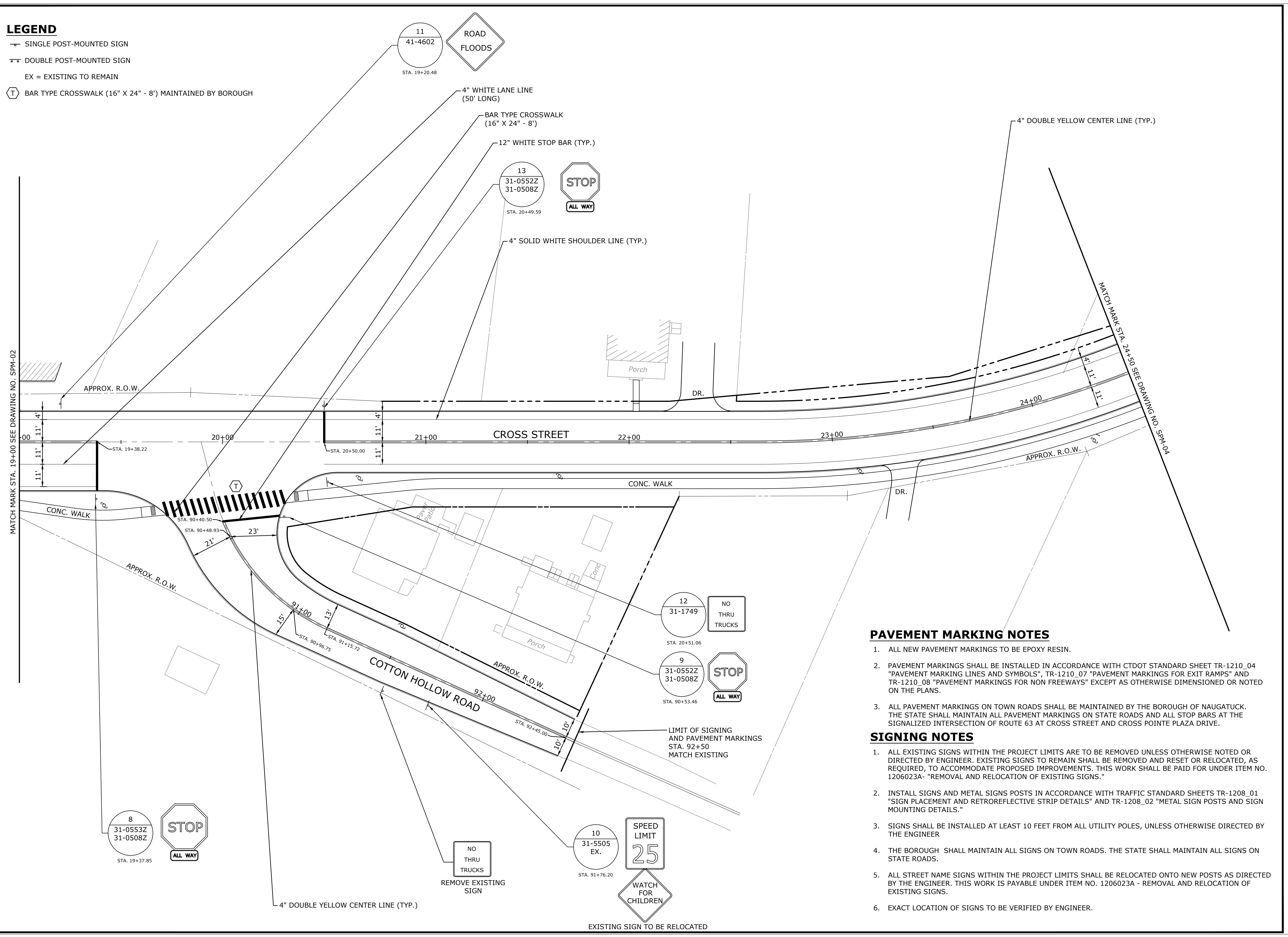
DP DESIGNED	NJY DRAWN	KB CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. SPM-02		

**47**  
 SHEET NO.



**LEGEND**

- SINGLE POST-MOUNTED SIGN
- DOUBLE POST-MOUNTED SIGN
- EX = EXISTING TO REMAIN
- ⬡ BAR TYPE CROSSWALK (16" X 24" - 8') MAINTAINED BY BOROUGH

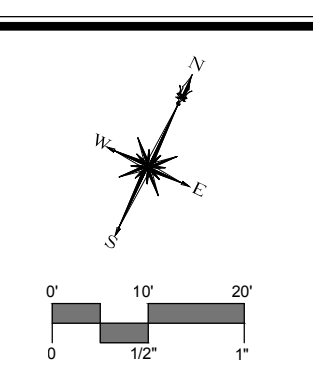


**PAVEMENT MARKING NOTES**

1. ALL NEW PAVEMENT MARKINGS TO BE EPOXY RESIN.
2. PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH CTDOT STANDARD SHEET TR-1210\_04 "PAVEMENT MARKING LINES AND SYMBOLS", TR-1210\_07 "PAVEMENT MARKINGS FOR EXIT RAMP" AND TR-1210\_08 "PAVEMENT MARKINGS FOR NON FREEWAYS" EXCEPT AS OTHERWISE DIMENSIONED OR NOTED ON THE PLANS.
3. ALL PAVEMENT MARKINGS ON TOWN ROADS SHALL BE MAINTAINED BY THE BOROUGH OF NAUGATUCK. THE STATE SHALL MAINTAIN ALL PAVEMENT MARKINGS ON STATE ROADS AND ALL STOP BARS AT THE SIGNALIZED INTERSECTION OF ROUTE 63 AT CROSS STREET AND CROSS POINTE PLAZA DRIVE.

**SIGNING NOTES**

1. ALL EXISTING SIGNS WITHIN THE PROJECT LIMITS ARE TO BE REMOVED UNLESS OTHERWISE NOTED OR DIRECTED BY ENGINEER. EXISTING SIGNS TO REMAIN SHALL BE REMOVED AND RESET OR RELOCATED, AS REQUIRED, TO ACCOMMODATE PROPOSED IMPROVEMENTS. THIS WORK SHALL BE PAID FOR UNDER ITEM NO. 1206023A- "REMOVAL AND RELOCATION OF EXISTING SIGNS."
2. INSTALL SIGNS AND METAL SIGNS POSTS IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208\_01 "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS" AND TR-1208\_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
3. SIGNS SHALL BE INSTALLED AT LEAST 10 FEET FROM ALL UTILITY POLES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER
4. THE BOROUGH SHALL MAINTAIN ALL SIGNS ON TOWN ROADS. THE STATE SHALL MAINTAIN ALL SIGNS ON STATE ROADS.
5. ALL STREET NAME SIGNS WITHIN THE PROJECT LIMITS SHALL BE RELOCATED ONTO NEW POSTS AS DIRECTED BY THE ENGINEER. THIS WORK IS PAYABLE UNDER ITEM NO. 1206023A - REMOVAL AND RELOCATION OF EXISTING SIGNS.
6. EXACT LOCATION OF SIGNS TO BE VERIFIED BY ENGINEER.



**MILONE & MACBROOM**  
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 Cheshire, Connecticut 06410  
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 www.miloneandmacbroom.com

DESCRIPTION	DATE	BY

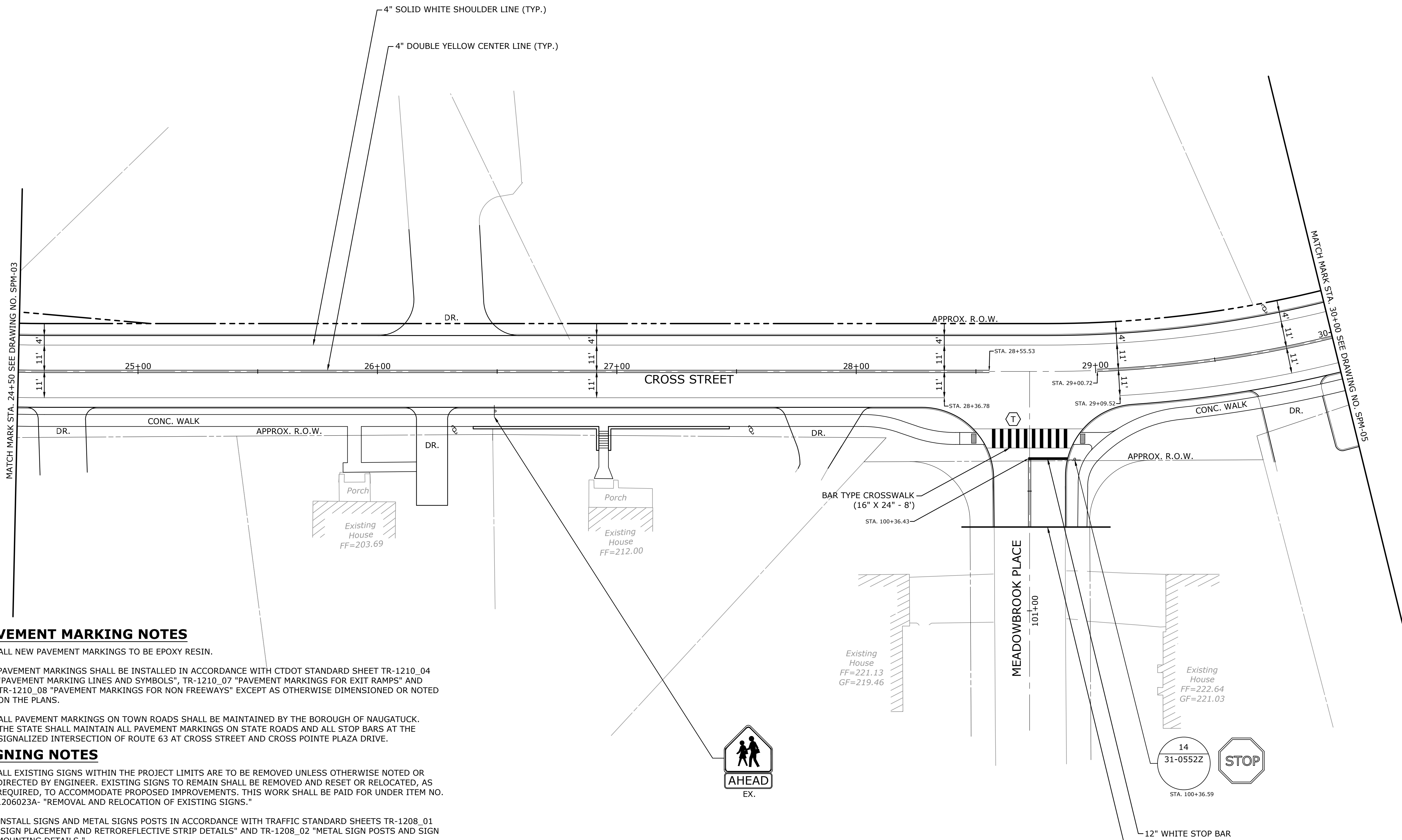
**SIGNING AND PAVEMENT MARKING PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	NJY	KB
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
DRAWING NO.: SPM-03		
SHEET NO.: 48		

EXISTING SIGN TO BE RELOCATED



MILONE & MACBROOM, INC. 2015



**PAVEMENT MARKING NOTES**

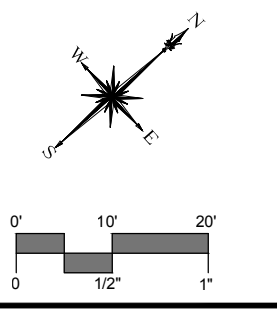
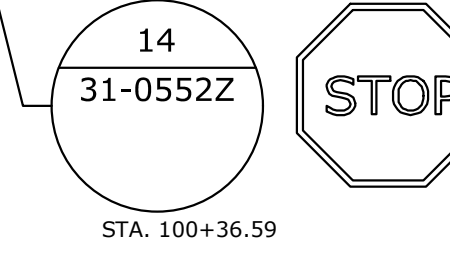
1. ALL NEW PAVEMENT MARKINGS TO BE EPOXY RESIN.
2. PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH CTDOT STANDARD SHEET TR-1210\_04 "PAVEMENT MARKING LINES AND SYMBOLS", TR-1210\_07 "PAVEMENT MARKINGS FOR EXIT RAMP" AND TR-1210\_08 "PAVEMENT MARKINGS FOR NON FREEWAYS" EXCEPT AS OTHERWISE DIMENSIONED OR NOTED ON THE PLANS.
3. ALL PAVEMENT MARKINGS ON TOWN ROADS SHALL BE MAINTAINED BY THE BOROUGH OF NAUGATUCK. THE STATE SHALL MAINTAIN ALL PAVEMENT MARKINGS ON STATE ROADS AND ALL STOP BARS AT THE SIGNALIZED INTERSECTION OF ROUTE 63 AT CROSS STREET AND CROSS POINTE PLAZA DRIVE.

**SIGNING NOTES**

1. ALL EXISTING SIGNS WITHIN THE PROJECT LIMITS ARE TO BE REMOVED UNLESS OTHERWISE NOTED OR DIRECTED BY ENGINEER. EXISTING SIGNS TO REMAIN SHALL BE REMOVED AND RESET OR RELOCATED, AS REQUIRED, TO ACCOMMODATE PROPOSED IMPROVEMENTS. THIS WORK SHALL BE PAID FOR UNDER ITEM NO. 1206023A- "REMOVAL AND RELOCATION OF EXISTING SIGNS."
2. INSTALL SIGNS AND METAL SIGNS POSTS IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208\_01 "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS" AND TR-1208\_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
3. SIGNS SHALL BE INSTALLED AT LEAST 10 FEET FROM ALL UTILITY POLES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER
4. THE BOROUGH SHALL MAINTAIN ALL SIGNS ON TOWN ROADS. THE STATE SHALL MAINTAIN ALL SIGNS ON STATE ROADS.
5. ALL STREET NAME SIGNS WITHIN THE PROJECT LIMITS SHALL BE RELOCATED ONTO NEW POSTS AS DIRECTED BY THE ENGINEER. THIS WORK IS PAYABLE UNDER ITEM NO. 1206023A - REMOVAL AND RELOCATION OF EXISTING SIGNS.
6. EXACT LOCATION OF SIGNS TO BE VERIFIED BY ENGINEER.

**LEGEND**

- ↖ SINGLE POST-MOUNTED SIGN
- ↖↖ DOUBLE POST-MOUNTED SIGN
- EX = EXISTING TO REMAIN
- (T) BAR TYPE CROSSWALK (16" X 24" - 8') MAINTAINED BY BOROUGH



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DESCRIPTION	DATE	BY

**SIGNING AND PAVEMENT MARKING PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	NJY	KB
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. SPM-04		

**49**

SHEET NO.

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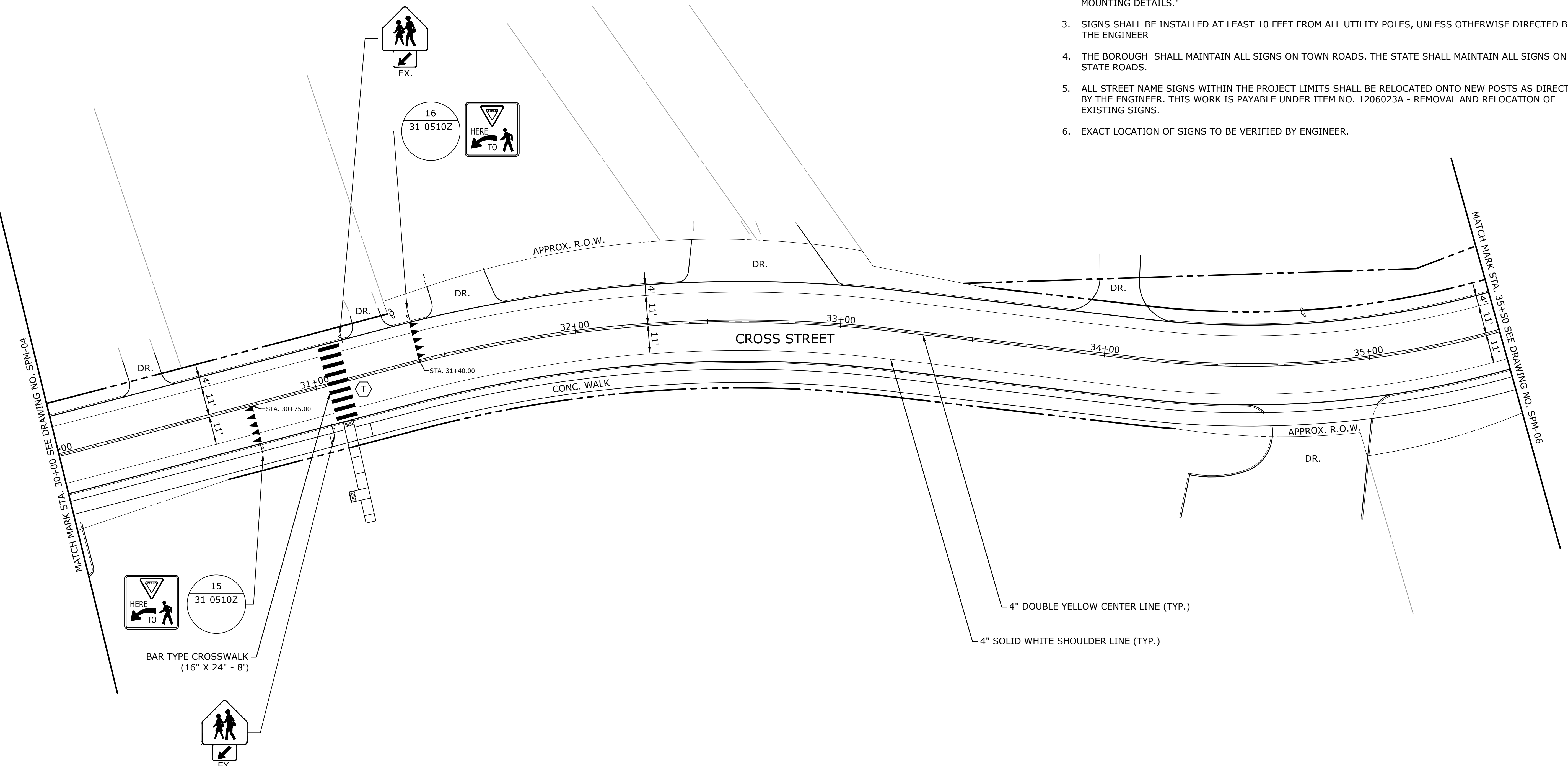
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### PAVEMENT MARKING NOTES

1. ALL NEW PAVEMENT MARKINGS TO BE EPOXY RESIN.
2. PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH CTDOT STANDARD SHEET TR-1210\_04 "PAVEMENT MARKING LINES AND SYMBOLS", TR-1210\_07 "PAVEMENT MARKINGS FOR EXIT RAMP" AND TR-1210\_08 "PAVEMENT MARKINGS FOR NON FREEWAYS" EXCEPT AS OTHERWISE DIMENSIONED OR NOTED ON THE PLANS.
3. ALL PAVEMENT MARKINGS ON TOWN ROADS SHALL BE MAINTAINED BY THE BOROUGH OF NAUGATUCK. THE STATE SHALL MAINTAIN ALL PAVEMENT MARKINGS ON STATE ROADS AND ALL STOP BARS AT THE SIGNALIZED INTERSECTION OF ROUTE 63 AT CROSS STREET AND CROSS POINTE PLAZA DRIVE.

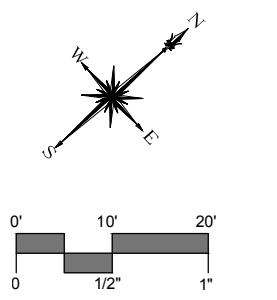
### SIGNING NOTES

1. ALL EXISTING SIGNS WITHIN THE PROJECT LIMITS ARE TO BE REMOVED UNLESS OTHERWISE NOTED OR DIRECTED BY ENGINEER. EXISTING SIGNS TO REMAIN SHALL BE REMOVED AND RESET OR RELOCATED, AS REQUIRED, TO ACCOMMODATE PROPOSED IMPROVEMENTS. THIS WORK SHALL BE PAID FOR UNDER ITEM NO. 1206023A- "REMOVAL AND RELOCATION OF EXISTING SIGNS."
2. INSTALL SIGNS AND METAL SIGNS POSTS IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208\_01 "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS" AND TR-1208\_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
3. SIGNS SHALL BE INSTALLED AT LEAST 10 FEET FROM ALL UTILITY POLES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
4. THE BOROUGH SHALL MAINTAIN ALL SIGNS ON TOWN ROADS. THE STATE SHALL MAINTAIN ALL SIGNS ON STATE ROADS.
5. ALL STREET NAME SIGNS WITHIN THE PROJECT LIMITS SHALL BE RELOCATED ONTO NEW POSTS AS DIRECTED BY THE ENGINEER. THIS WORK IS PAYABLE UNDER ITEM NO. 1206023A - REMOVAL AND RELOCATION OF EXISTING SIGNS.
6. EXACT LOCATION OF SIGNS TO BE VERIFIED BY ENGINEER.



### LEGEND

- SINGLE POST-MOUNTED SIGN
- DOUBLE POST-MOUNTED SIGN
- EX = EXISTING TO REMAIN
- (T) BAR TYPE CROSSWALK (16" X 24" - 8') MAINTAINED BY BOROUGH



**MILONE & MACBROOM**  
 99 Realty Drive  
 Cheshire, Connecticut 06410  
 (203) 271-1773 Fax (203) 272-9733  
 www.miloneandmacbroom.com

DESCRIPTION	DATE	BY

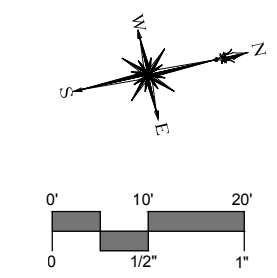
**SIGNING AND PAVEMENT MARKING PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	NJY DRAWN	KB CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. SPM-05		

**50**  
 SHEET NO.



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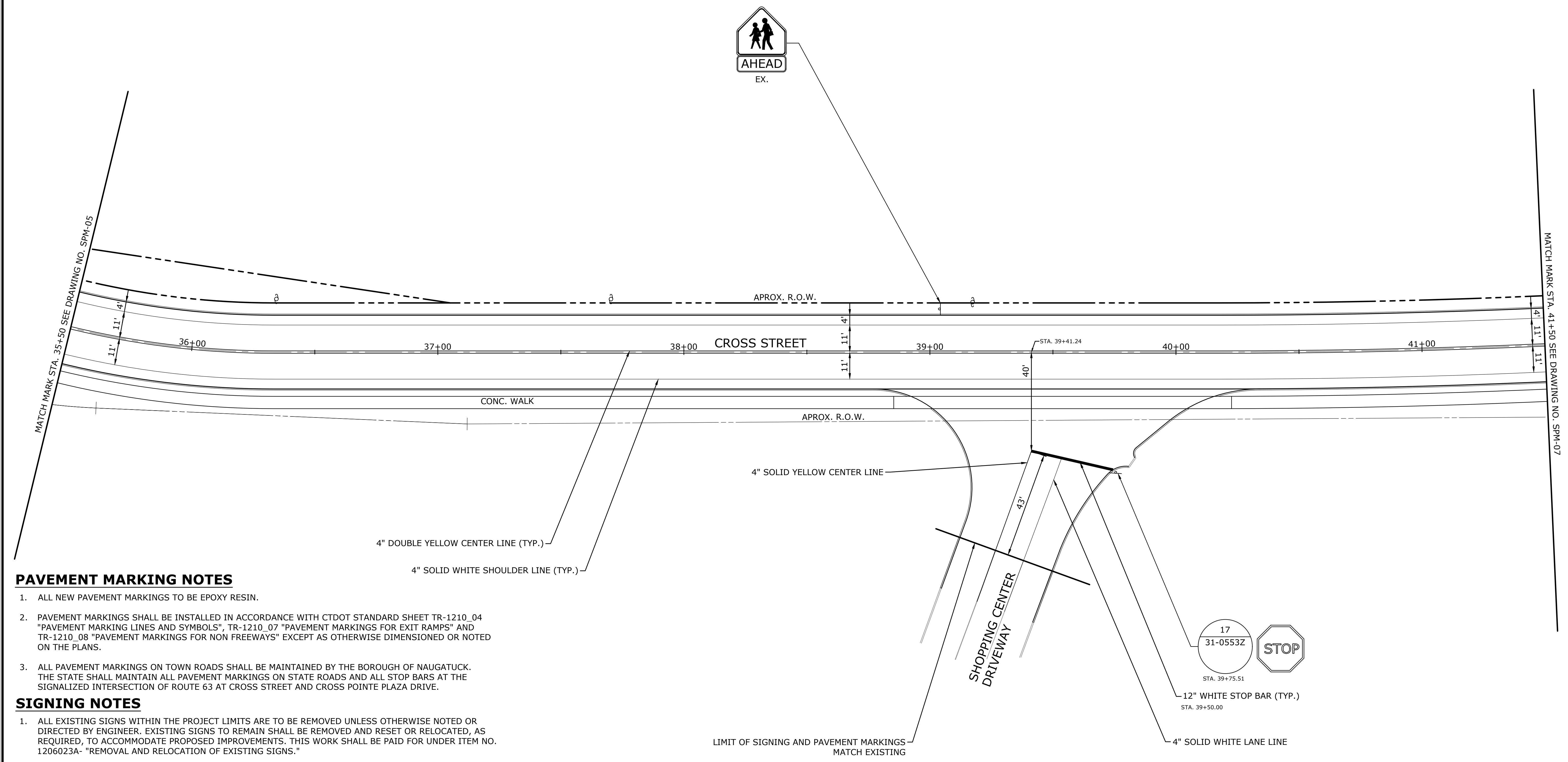
DESCRIPTION	DATE	BY

**SIGNING AND PAVEMENT MARKING PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	NJY DRAWN	KB CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. SPM-06		

51

SHEET NO.



**PAVEMENT MARKING NOTES**

1. ALL NEW PAVEMENT MARKINGS TO BE EPOXY RESIN.
2. PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH CTDOT STANDARD SHEET TR-1210\_04 "PAVEMENT MARKING LINES AND SYMBOLS", TR-1210\_07 "PAVEMENT MARKINGS FOR EXIT RAMP" AND TR-1210\_08 "PAVEMENT MARKINGS FOR NON FREEWAYS" EXCEPT AS OTHERWISE DIMENSIONED OR NOTED ON THE PLANS.
3. ALL PAVEMENT MARKINGS ON TOWN ROADS SHALL BE MAINTAINED BY THE BOROUGH OF NAUGATUCK. THE STATE SHALL MAINTAIN ALL PAVEMENT MARKINGS ON STATE ROADS AND ALL STOP BARS AT THE SIGNALIZED INTERSECTION OF ROUTE 63 AT CROSS STREET AND CROSS POINTE PLAZA DRIVE.

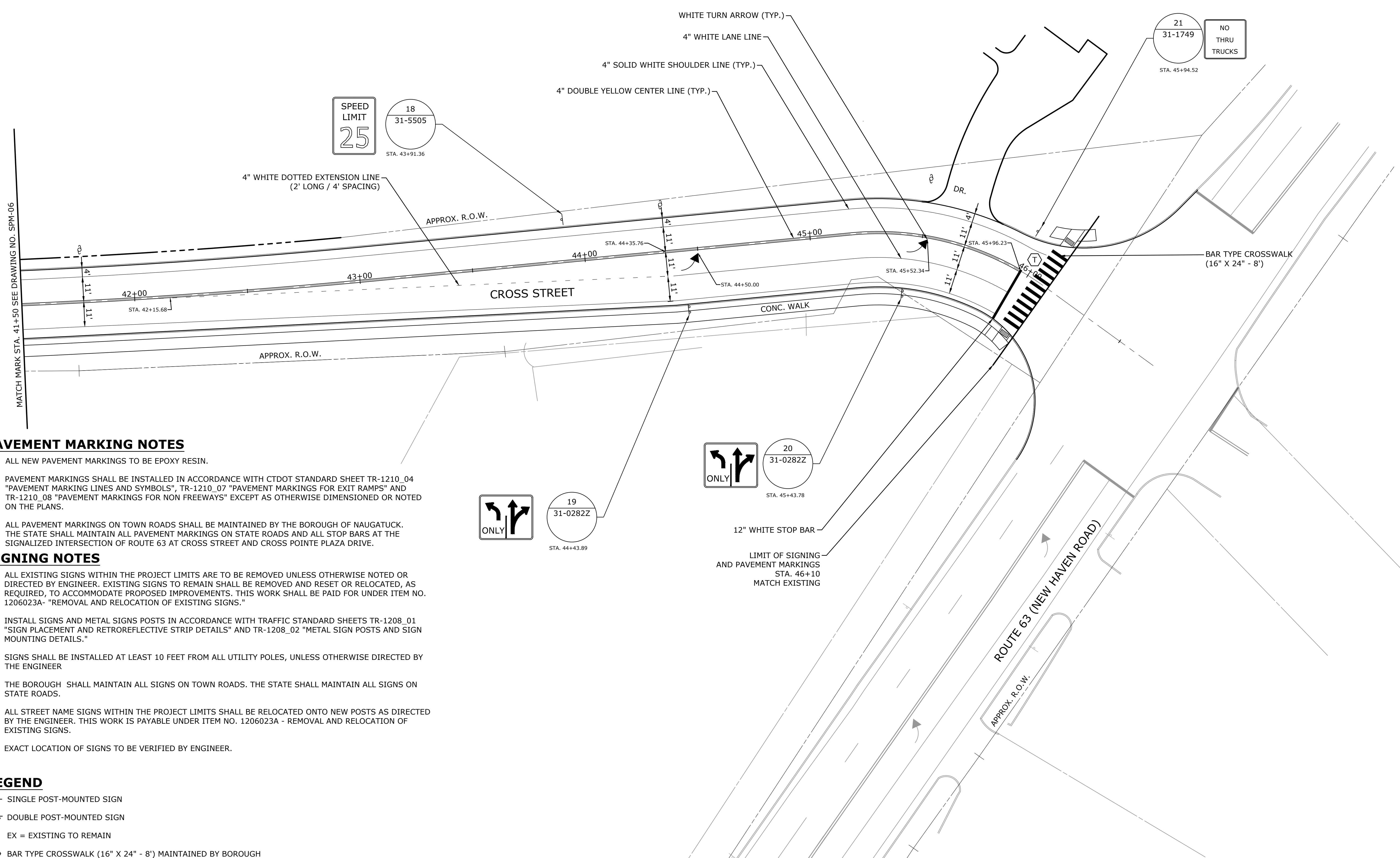
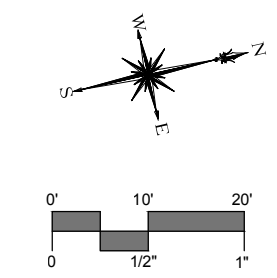
**SIGNING NOTES**

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2. INSTALL SIGNS AND METAL SIGNS POSTS IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208\_01 "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS" AND TR-1208\_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
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6. EXACT LOCATION OF SIGNS TO BE VERIFIED BY ENGINEER.

**LEGEND**

	SINGLE POST-MOUNTED SIGN
	DOUBLE POST-MOUNTED SIGN
EX	EXISTING TO REMAIN

SHEET 52 - 3. PRELIMINARY PLAN. THIS SHEET IS NOT TO BE USED FOR CONSTRUCTION. IT IS THE RESPONSIBILITY OF THE USER TO VERIFY THE ACCURACY OF THE INFORMATION PROVIDED HEREON.



**PAVEMENT MARKING NOTES**

1. ALL NEW PAVEMENT MARKINGS TO BE EPOXY RESIN.
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DESCRIPTION	DATE	BY

**SIGNING AND PAVEMENT MARKING PLAN**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	NJY DRAWN	KB CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. SPM-07		
<b>52</b>		



R1 - SERIES										R2 - SERIES										R3 - SERIES										R4 - SERIES										R5 - SERIES																																																																																																																																																																																																																																
<b>R1-1</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>1.85</td><td>18</td><td>31-0532</td><td>1</td><td>.080</td></tr> <tr><td>5.19</td><td>30</td><td>31-0552</td><td>1</td><td>.080</td></tr> <tr><td>7.98</td><td>36</td><td>31-0553</td><td>1</td><td>.080</td></tr> <tr><td>13.3</td><td>48</td><td>31-0557</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	1.85	18	31-0532	1	.080	5.19	30	31-0552	1	.080	7.98	36	31-0553	1	.080	13.3	48	31-0557	2	.100	<b>R2-1</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>5.00</td><td>24X30</td><td>31-5505</td><td>1</td><td>.080</td></tr> <tr><td>7.50</td><td>30X36</td><td>31-5504</td><td>1</td><td>.080</td></tr> <tr><td>12.00</td><td>36X48</td><td>31-5506</td><td>2</td><td>.100</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-5507</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	5.00	24X30	31-5505	1	.080	7.50	30X36	31-5504	1	.080	12.00	36X48	31-5506	2	.100	20.00	48X60	31-5507	2	.100	<b>R3-1</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>4.00</td><td>24X24</td><td>31-1604</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36X36</td><td>31-1627</td><td>2</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	4.00	24X24	31-1604	1	.080	9.00	36X36	31-1627	2	.080	<b>R3-5</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>7.50</td><td>30X36</td><td>31-0183</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	7.50	30X36	31-0183	1	.080	<b>R3-6</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>7.50</td><td>30X36</td><td>31-0175</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	7.50	30X36	31-0175	1	.080	<b>R3-8</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>7.50</td><td>36X30</td><td>31-0282</td><td>2</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	7.50	36X30	31-0282	2	.080	<b>R3-8b</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>10.00</td><td>48X30</td><td>31-0302</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	10.00	48X30	31-0302	2	.100	<b>R4-1</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>5.00</td><td>24X30</td><td>31-1502</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	5.00	24X30	31-1502	1	.080	<b>R4-16</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>20.00</td><td>48X60</td><td>31-1574</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	20.00	48X60	31-1574	2	.100	<b>R5-1</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30X30</td><td>31-1119</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36X36</td><td>31-1120</td><td>2</td><td>.080</td></tr> <tr><td>16.00</td><td>48X48</td><td>31-1121</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30X30	31-1119	1	.080	9.00	36X36	31-1120	2	.080	16.00	48X48	31-1121	2	.100	<b>R5-10a(CT)</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>9.00</td><td>36X36</td><td>31-1775</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36X36	31-1775	2	.100
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<b>R1-2</b>  LEGEND - RED BACKGROUND - WHITE <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>0.97</td><td>18</td><td>31-0520</td><td>1</td><td>.080</td></tr> <tr><td>3.90</td><td>36</td><td>31-0523</td><td>1</td><td>.080</td></tr> <tr><td>6.77</td><td>48</td><td>31-0522</td><td>2</td><td>.100</td></tr> <tr><td>10.83</td><td>60</td><td>31-0528</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	0.97	18	31-0520	1	.080	3.90	36	31-0523	1	.080	6.77	48	31-0522	2	.100	10.83	60	31-0528	2	.100	<b>R2-4a</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>32</td><td>48X96</td><td>31-5510</td><td>2</td><td>.125</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	32	48X96	31-5510	2	.125	<b>R3-2</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>4.00</td><td>24X24</td><td>31-1603</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36X36</td><td>31-1629</td><td>2</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	4.00	24X24	31-1603	1	.080	9.00	36X36	31-1629	2	.080	<b>R3-6</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>7.50</td><td>30X36</td><td>31-0157</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	7.50	30X36	31-0157	1	.080	<b>R3-8</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>7.50</td><td>36X30</td><td>31-0290</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	7.50	36X30	31-0290	1	.080	<b>R3-8</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>7.50</td><td>36X30</td><td>31-0295</td><td>2</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	7.50	36X30	31-0295	2	.080	<b>R3-8b</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>10.00</td><td>48X30</td><td>31-0307</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	10.00	48X30	31-0307	2	.100	<b>R4-3</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>5.00</td><td>24X30</td><td>31-1562</td><td>1</td><td>.080</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-1564</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	5.00	24X30	31-1562	1	.080	20.00	48X60	31-1564	2	.100	<b>R5-1a</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.00</td><td>36X24</td><td>31-1122</td><td>2</td><td>.080</td></tr> <tr><td>8.75</td><td>42X30</td><td>31-1123</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.00	36X24	31-1122	2	.080	8.75	42X30	31-1123	2	.100	<b>R5-11(CT)</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>5.00</td><td>24X30</td><td>31-1790</td><td>1</td><td>.080</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-1792</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	5.00	24X30	31-1790	1	.080	20.00	48X60	31-1792	2	.100																														
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<b>R6 - SERIES</b> <b>R6-1</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>3.00</td><td>36X12</td><td>31-1188</td><td>1</td><td>.080</td></tr> <tr><td>6.75</td><td>54X18</td><td>31-1189</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	3.00	36X12	31-1188	1	.080	6.75	54X18	31-1189	2	.100	<b>R7 - SERIES</b> <b>R7-1</b>  LEGEND - RED BACKGROUND - WHITE <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>1.50</td><td>12X18</td><td>31-0630</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	1.50	12X18	31-0630	1	.080	<b>R8 - SERIES</b> <b>R8-7</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>12.00</td><td>48X36</td><td>31-0670</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	12.00	48X36	31-0670	2	.100	<b>R9 - SERIES</b> <b>R9-3</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>2.25</td><td>18X18</td><td>31-1705</td><td>1</td><td>.080</td></tr> <tr><td>4.00</td><td>24X24</td><td>31-1769</td><td>1</td><td>.080</td></tr> <tr><td>6.25</td><td>30X30</td><td>31-1770</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	2.25	18X18	31-1705	1	.080	4.00	24X24	31-1769	1	.080	6.25	30X30	31-1770	1	.080	<b>R10 - SERIES</b> <b>R10-6</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>31-0801</td><td>6.00</td><td>24X36</td><td>1</td><td>.080</td></tr> <tr><td>31-0802</td><td>9.00</td><td>36X36</td><td>2</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	31-0801	6.00	24X36	1	.080	31-0802	9.00	36X36	2	.080	<b>R10-11b</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>9.00</td><td>36X36</td><td>31-0816</td><td>2</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36X36	31-0816	2	.080	<b>R10-11c(CT)</b>  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>7.50</td><td>30X36</td><td>31-0847</td><td>1</td><td>.080</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-1918</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	7.50	30X36	31-0847	1	.080	20.00	48X60	31-1918	2	.100	<b>R11 - SERIES</b> <b>R11-1</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>5.00</td><td>24X30</td><td>31-1915</td><td>1</td><td>.080</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-1918</td><td>2</td><td>.100</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	5.00	24X30	31-1915	1	.080	20.00	48X60	31-1918	2	.100	<b>RETROREFLECTIVE STRIP</b> LEGEND - N/A <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>1.00</td><td>4X36</td><td>31-5003</td><td>1</td><td>.080</td></tr> <tr><td>2.00</td><td>4X72</td><td>31-5004</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	1.00	4X36	31-5003	1	.080	2.00	4X72	31-5004	1	.080	<b>**</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>5.00</td><td>24X30</td><td>31-1749</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	5.00	24X30	31-1749	1	.080	<b>**</b>  LEGEND - WHITE BACKGROUND - RED <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>2.25</td><td>18X18</td><td>31-05102</td><td>1</td><td>.080</td></tr> </table>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	2.25	18X18	31-05102	1	.080										
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**NOTES:**

1. THE LEGEND "O.S.T.A." SHALL APPEAR ON ALL R- SERIES SIGNS EXCEPT WHEN SUFFIXED WITH THE LETTER "Z".
2. FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERCTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
3. POSTS - SEE STANDARD SHEET TR-1208.02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
4. POSTS SHALL BE 4 LBS./FT.
5. SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM. SPLICING OF SHEET ALUMINUM WILL NOT BE ACCEPTED.
6. FOR OVERHEAD MOUNTED SIGNS, SEE STANDARD SHEET TR-1114.01 - "BONDING AND UTILITY POLE ATTACHMENT DETAILS, SIGN HANGER, "Y" CLAMP DETAIL."

**COLORS:**

- BACKGROUND - WHITE - EXCEPT AS NOTED.
- LEGEND - BLACK - EXCEPT AS NOTED.
- ALL SIGNS TO USE TYPE IX RETROREFLECTIVE SHEETING.
- \* SIGNS TO USE TYPE IX RETROREFLECTIVE SHEETING
- \*\* THIS SIGN ADDED FOR PROJECT ONLY

MILONE & MACBROOM  
 99 Reedy Drive  
 Cheshire, Connecticut 06410  
 (203) 271-1773 Fax (203) 272-9733  
 www.miloneandmacbroom.com

SIGNING AND PAVEMENT MARKING PLAN-SIGN FACE SHEET ALUMINUM  
 R-SERIES SIGNS TYP. DETAILS  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT

DESIGNER	CTDOT
SCALE	N.T.S.
DATE	SEPTEMBER 7, 2018
PROJECT NO.	87-145
DRAWING NO.	SPM-08
SHEET NO.	53

BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DESCRIPTION: \_\_\_\_\_







**NOTES**

- SEE ROADWAY PLANS, TYPICAL SECTIONS AND CROSS SECTIONS FOR GRADES AND TREATMENT.
- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIFICATIONS FOR SECTION 1.08 - PROSECUTION AND PROGRESS AND ITEM NO. 0971001A - MAINTENANCE AND PROTECTION OF TRAFFIC.
- UNIFORMED POLICE OFFICERS ARE TO BE USED WHENEVER TRAFFIC CONTROL DEVICES ARE INSTALLED, RELOCATED OR REMOVED.
- DIMENSIONS SHOWN ARE APPROXIMATE AND SHALL BE USED AS A GUIDE ONLY. THE ACTUAL DIMENSIONS SHALL BE MODIFIED AS DIRECTED BY THE ENGINEER AS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN TRAVEL DIRECTION.
- EACH STAGE SHALL BE FUNCTIONALLY COMPLETE, AS DETERMINED BY THE ENGINEER, BEFORE ANY SUBSEQUENT STAGES MAY BE IMPLEMENTED, UNLESS OTHERWISE ALLOWED BY THE ENGINEER.
- DRIVEWAY CONSTRUCTION SHALL BE COORDINATED WITH THE PROPERTY OWNERS. ALL EXISTING ACCESS TO ADJACENT PROPERTIES MUST BE MAINTAINED AT ALL TIMES UNLESS THE CONTRACTOR HAS FIRST NEGOTIATED ALTERNATE ARRANGEMENTS WITH THE PROPERTY OWNERS OR AS OTHERWISE NOTED ON THESE PLANS. AT A MINIMUM, TEMPORARY GRADED SURFACES SHALL CONSIST OF PROCESSED AGGREGATE BASE, GRANULAR FILL, OR OTHER SUITABLE MATERIAL(S) APPROVED BY THE ENGINEER. IF TEMPORARY ACCESS IS TO BE PROVIDED LONGER THAN FIVE DAYS, THEN A TEMPORARY BITUMINOUS CONCRETE DRIVEWAY WALL SHALL BE INSTALLED IN ACCORDANCE WITH THE SPECIFICATIONS.
- EXISTING SIGNS ARE TO BE RELOCATED AS NEEDED AND AS DIRECTED BY THE ENGINEER DURING CONSTRUCTION SO THAT THEY ARE IN THE APPROPRIATE LOCATION AND VISIBLE TO MOTORISTS. SOME SIGNS MAY HAVE TO BE TEMPORARILY LOCATED WITHIN THE WORK AREA. THIS WORK WILL BE PAID FOR UNDER ITEM#0971001A MAINTENANCE AND PROTECTION OF TRAFFIC.
- ALL CONSTRUCTION SIGNS SHOWN FOR STAGE CONSTRUCTION SHALL BE INSTALLED IN ACCORDANCE WITH TRAFFIC STANDARD SHEET TR-1208\_1, TR-1208\_2, TR-1220\_01, OR TR-1220\_2.
- EXISTING CONFLICTING PAVEMENT MARKINGS SHALL BE REMOVED WITHIN THE PROJECT LIMITS. BLACK LINE MASK PAVEMENT MARKING TAPE SHALL BE USED TO COVER EXISTING CONFLICTING PAVEMENT MARKINGS OUTSIDE OF THE LIMITS. CONFLICTING MARKINGS TO BE COVERED OR REMOVED INCLUDES THOSE OUTSIDE THE TRAVELWAY.
- ALL TEMPORARY PAVEMENT MARKINGS SHALL BE HOT-APPLIED PAINTED PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS. ANY MARKINGS OUTSIDE OF THE LIMITS SHALL BE TEMPORARY PLASTIC PAVEMENT MARKING TAPE. ANY PAVEMENT MARKINGS TO EXTEND THROUGH THE WINTER SHALL BE EPOXY RESIN.
- SEDIMENTATION AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION.
- WHEN CONSTRUCTING FINAL PAVEMENT, THE FINAL LIFT OF BITUMINOUS CONCRETE AND FINAL PAVEMENT MARKINGS SHALL NOT BE PLACED UNTIL ALL STAGE CONSTRUCTION HAS BEEN COMPLETED.
- REFER TO THE APPLICABLE CONSTRUCTION TRAFFIC CONTROL PLANS CONTAINED IN THE SPECIAL PROVISION FOR MAINTENANCE AND PROTECTION OF TRAFFIC FOR ADDITIONAL NOTES, SIGNING AND TRAFFIC DRUM/TRAFFIC CONE PLACEMENT.
- EXISTING SIGNS THAT CONFLICT WITH TEMPORARY STAGE CONSTRUCTION SIGNS SHALL BE COVERED, REMOVED OR REVISED TO MEET FIELD CONDITIONS AND/OR AS DIRECTED BY THE ENGINEER.
- BARRICADE WARNING LIGHTS - HIGH INTENSITY SHALL BE INSTALLED ON ALL POST-MOUNTED DIAMOND SHAPE ADVANCE WARNING CONSTRUCTION SIGNS.
- THE LOCATIONS OF TRAFFIC DRUMS AND TYPE III CONSTRUCTION BARRICADES SHOWN ON THE PLANS ARE APPROXIMATE AND SHALL BE ADJUSTED BY THE CONTRACTOR TO MEET FIELD CONDITIONS AND TO CLEARLY DEFINE ACCESS TO AND EGRESS FROM ALL ROADWAYS AND DRIVEWAYS.
- ANY INCOMPLETE DRAINAGE RUNS THAT OCCUR AS A RESULT OF STAGE CONSTRUCTION SHALL BE TEMPORARILY CAPPED AND PROTECTED FROM DAMAGE UNTIL THE DRAINAGE RUN IS COMPLETED IN FUTURE STAGES, THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK.
- THE CONTRACTOR SHALL PROPERLY REMOVE AND DISPOSE OF ALL TEMPORARY PAVEMENT WHEN NO LONGER REQUIRED. THIS WORK SHALL BE PAID FOR UNDER "EARTH EXCAVATION".
- THE CONTRACTOR SHALL INSTALL "ONE-WAY" SIGNAGE (CTDOT ID: 31-1177R AND 31-1188L) WHERE APPROPRIATE TO PROVIDE DIRECTION FOR THE DETOUR DURING CONSTRUCTION. "ONE WAY" SIGNS SHALL BE INSTALLED OPPOSITE EACH DRIVEWAY INTERSECTING CROSS STREET BETWEEN COTTON HOLLOW ROAD AND ROUTE 63 DURING STAGE 1 AND 2. CONTRACTOR SHALL PROVIDE ACCESS TO THE NORTHBOUND SIDE OF CROSS STREET TO ALL RESIDENTS WHO'S DRIVEWAYS OR STREET INTERSECTS WITHIN THE WORK LIMITS.
- THE LOCATIONS OF TEMPORARY SIGNS SHOWN ON THE PLANS ARE APPROXIMATE AND SHALL BE ADJUSTED BY THE CONTRACTOR TO MEET FIELD CONDITIONS.
- ANY EXISTING SIGNING DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT NO COST TO THE STATE OR BOROUGH.

**LEGEND**

- ① 4" YELLOW PAINTED PAVEMENT MARKINGS
- ② TRAFFIC DRUMS/TRAFFIC CONES @ 20' SPACING OR AS SHOWN IN THE PLANS OR AS DIRECTED BY ENGINEER IN THE FIELD
- ③ BITUMINOUS CONCRETE LIP CURBING
- ④ 3" BITUMINOUS CONCRETE HMA S1.0
- ⑤ 8" PROCESSED AGGREGATE BASE
- ⑥ CUT BITUMINOUS CONCRETE PAVEMENT
- ⑦ FORMATION OF SUBGRADE
- ⑧ PORTION OF THE ROADWAY CONSTRUCTED DURING STAGE 1
- ⑨ TURF ESTABLISHMENT
- ⑩ 4" WHITE PAINTED PAVEMENT MARKINGS
- ↑ DIRECTION OF TRAFFIC (ONE WAY NORTHBOUND)
- \* MAINTAIN EXISTING CROSS SLOPE
- \*\* PROVIDE 14' WIDTH WHENEVER IS POSSIBLE IN THE FIELD OR AS DIRECTED BY ENGINEER
- \*\*\* SIGN TO HAVE A BLACK BORDER & LEGEND, ORANGE BACKGROUND

**SEQUENCE OF CONSTRUCTION**

**STAGE 1 (CROSS STREET)**

- INSTALL TEMPORARY PAVEMENT ALONG THE RIGHT SIDE OF CROSS STREET BETWEEN COTTON HOLLOW ROAD AND STA. 21+62.5± BY UTILIZING ONE LANE ALTERNATING TRAFFIC DURING ALLOWABLE PERIODS. SEE STAGE-1 (CROSS STREET) PLAN FOR ADDITIONAL INFORMATION. FOR ADDITIONAL INFORMATION REGARDING SIGNING PATTERNS, SEE TRAFFIC CONTROL PLANS GIVEN IN THE SPECIAL PROVISION FOR THE ITEM "MAINTENANCE AND PROTECTION OF TRAFFIC."
- DETOUR CROSS STREET SOUTHBOUND TRAFFIC ALONG ROUTE 63 NORTHBOUND TO ROUTE 8, EXIT 26. FOR ADDITIONAL INFORMATION REGARDING DETOUR, SEE CROSS STREET SOUTHBOUND DETOUR PLAN.
- SHIFT THE CROSS STREET NORTHBOUND TRAFFIC AS SHOWN IN THE TYPICAL SECTIONS PROVIDED ON THIS SHEET.
- COMPLETE THE WORK ALONG SOUTHBOUND SIDE OF CROSS STREET. DO NOT INSTALL FINAL LIFT OF HMA S0.5.
- PROCEED TO STAGE 2.

**STAGE 2 (CROSS STREET)**

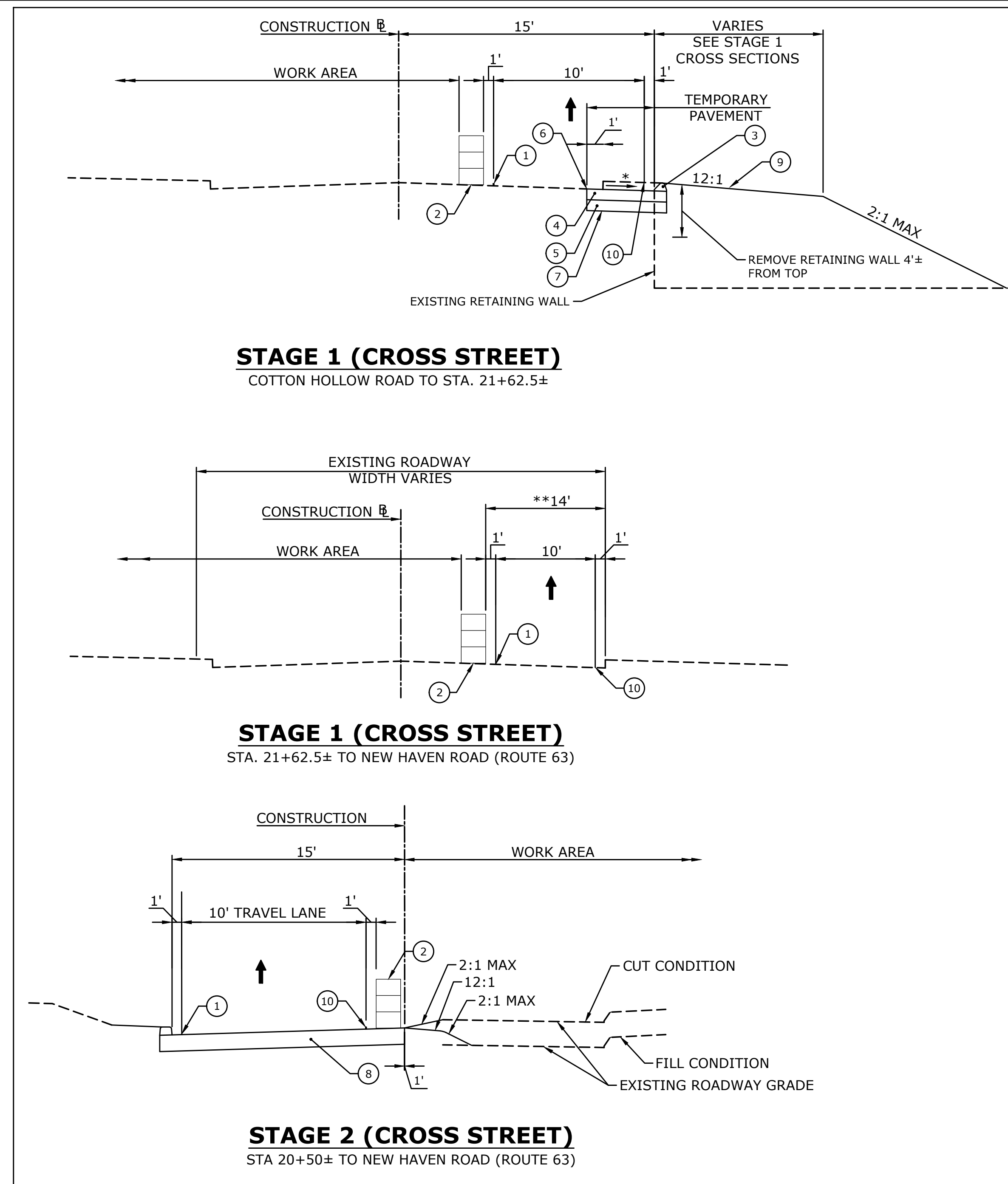
- SHIFT THE CROSS STREET NORTHBOUND TRAFFIC TO NEWLY CONSTRUCTED PAVEMENT DURING STAGE 1 AS SHOWN IN TYPICAL CROSS SECTION FOR STAGE 2.
- CROSS STREET SOUTHBOUND TRAFFIC SHALL REMAIN ALONG DETOUR ROUTE.
- INSTALL PORTION OF SIDEWALK AT NORTH CORNER OF MEADOWBROOK PLACE AND CROSS STREET, AS SHOWN ON DWG NO. MPT-04, AND COMPLETE SIDEWALK CONSTRUCTION TO THE NORTHERN DRIVEWAY ENTRANCE OF CROSS STREET SCHOOL (STA. 28+90 TO 34+60). NOTE: THE CONTRACTOR SHALL INSTALL CONSTRUCTION BARRICADES AND SIDEWALK CLOSURE SIGNAGE AT THE EXISTING SIDEWALK ENTRANCE AT THE NORTHERN DRIVEWAY OF CROSS STREET SCHOOL, FOR PEDESTRIAN TRAFFIC HEADING SOUTH. THE CONTRACTOR SHALL DIRECT PEDESTRIANS THROUGH THE WORK AREA VIA THE DESIGNATED PEDESTRIAN PATH, AS SHOWN. TEMPORARY BITUMINOUS SIDEWALK & SIDEWALK RAMPS SHALL BE USED AT THE MIDBLOCK CROSSING ON CROSS STREET, JUST NORTH OF MEADOWBROOK PLACE, TO MAINTAIN ACCESS TO THE SCHOOL AT ALL TIMES. TEMPORARY SIDEWALK SHALL BE PAID FOR UNDER "MAINTENANCE AND PROTECTION OF TRAFFIC".
- COMPLETE WORK ALONG THE NORTHBOUND SIDE OF CROSS STREET. DO NOT INSTALL THE FINAL LIFT OF HMA S0.5.
- PROCEED TO STAGE 3.

**STAGE 3 (COTTON HOLLOW ROAD)**

- OPEN CROSS STREET TRAFFIC IN BOTH DIRECTIONS.
- CLOSE COTTON HOLLOW ROAD BETWEEN CROSS STREET AND STA. 92+45±.
- SEE DETOUR PLAN FOR "COTTON HOLLOW ROAD" DETOUR FOR ADDITIONAL INFORMATION.
- COMPLETE THE WORK FOR COTTON HOLLOW ROAD. DO NOT INSTALL THE FINAL LIFT OF HMA S0.5.
- PROCEED TO STAGE 4.

**STAGE 4 (FROM BEGINNING OF PROJECT TO INTERSECTION OF COTTON HOLLOW ROAD)**

- WORK IN THIS AREA COULD BE COMPLETED BY ONE LANE ALTERNATING TRAFFIC DURING ALLOWABLE PERIOD AS PER SECTION 1.08 - PROSECUTION AND PROGRESS AND ITEM NO. 0971001A "MAINTENANCE AND PROTECTION OF TRAFFIC". NO SEPARATE STAGE CONSTRUCTION PLANS FOR THIS STAGE ARE PROVIDED.
- INSTALL FINAL LIFT OF HMA S0.5 THROUGHOUT THE PROJECT.
- RESTORE THE NORMAL TRAFFIC CONDITION ALONG CROSS STREET AND COTTON HOLLOW ROAD.



<b>DETOUR SIGN LEGEND (SEE M&amp;PT DRAWINGS FOR ALL OTHER SIGNS)</b>			
PLAN DESIGNATION	MESSAGE	SIZE	CONNDOT SIGN DESIGNATION
(A)	END DETOUR	24"x18"	80-9708
(B)	CROSS STREET CLOSED AHEAD FOLLOW DETOUR	60"x30"	80-9928
(C)	CROSS STREET DETOUR	48"x12" 24"x12" 21"x15"	51-2010 *** 80-9707 51-2625 ***
(D)	CROSS STREET	48"x12" 48"x18"	51-2010 *** 80-9702L
(E)	CROSS STREET	48"x12" 48"x18"	51-2010 *** 80-9701R
(F)	COTTON HOLLOW RD CLOSED AHEAD FOLLOW DETOUR	60"x30"	80-9928
(G)	COTTON HOLLOW RD DETOUR	48"x12" 24"x12" 21"x15"	51-2010 *** 80-9707 51-2625 ***
(H)	COTTON HOLLOW RD	48"x12" 48"x18"	51-2010 *** 80-9701R
(I)	COTTON HOLLOW RD	48"x12" 48"x18"	51-2010 *** 80-9702L
(J)	COTTON HOLLOW RD CLOSED AT CROSS ST FOLLOW DETOUR	60"x30"	80-9928
(K)	ONE-WAY	36"x12"	31-1188L
(L)	DO NOT ENTER	36"x36"	31-1120
(M)	ONE-WAY	36"x12"	31-1177R
(N)	WRONG WAY	36"x24"	31-1122
(O)	ROAD CLOSED	48"x30"	80-9080
(P)	STOP	30"x30"	31-0552
(Q)	CROSS STREET DETOUR	48"x12" 24"x12" 21"x15"	51-2010 *** 80-9707 51-2629 ***
(R)	CROSS STREET DETOUR USE EXIT 2S	60"x30"	80-9928
(S)	ROAD CLOSED 1 MILE AHEAD LOCAL TRAFFIC ONLY	60"x30"	80-9077
(T)	SIDEWALK CLOSED	30"x18"	80-9076 MOUNT ON CONSTRUCTION BARRICADE TYPE III

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DESCRIPTION	DATE	BY

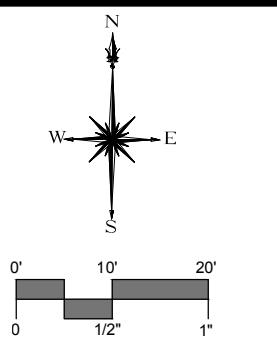
MAINTENANCE AND PROTECTION OF TRAFFIC  
(TYPICAL SECTIONS AND GENERAL NOTES)  
RECONSTRUCTION OF CROSS STREET  
NAUGATUCK, CONNECTICUT

DP	SMB	MJJ
DESIGNED	DRAWN	CHECKED
SCALE		
NOT TO SCALE		
DATE		
SEPTEMBER 7, 2018		
PROJECT NO.		
87-145		
DRAWING NO.		
MPT-01		
SHEET NO.		
56		



**NOTES**

- SEE DRAWING NO. MPT-01 FOR NOTES, LEGENDS AND DETAILS.



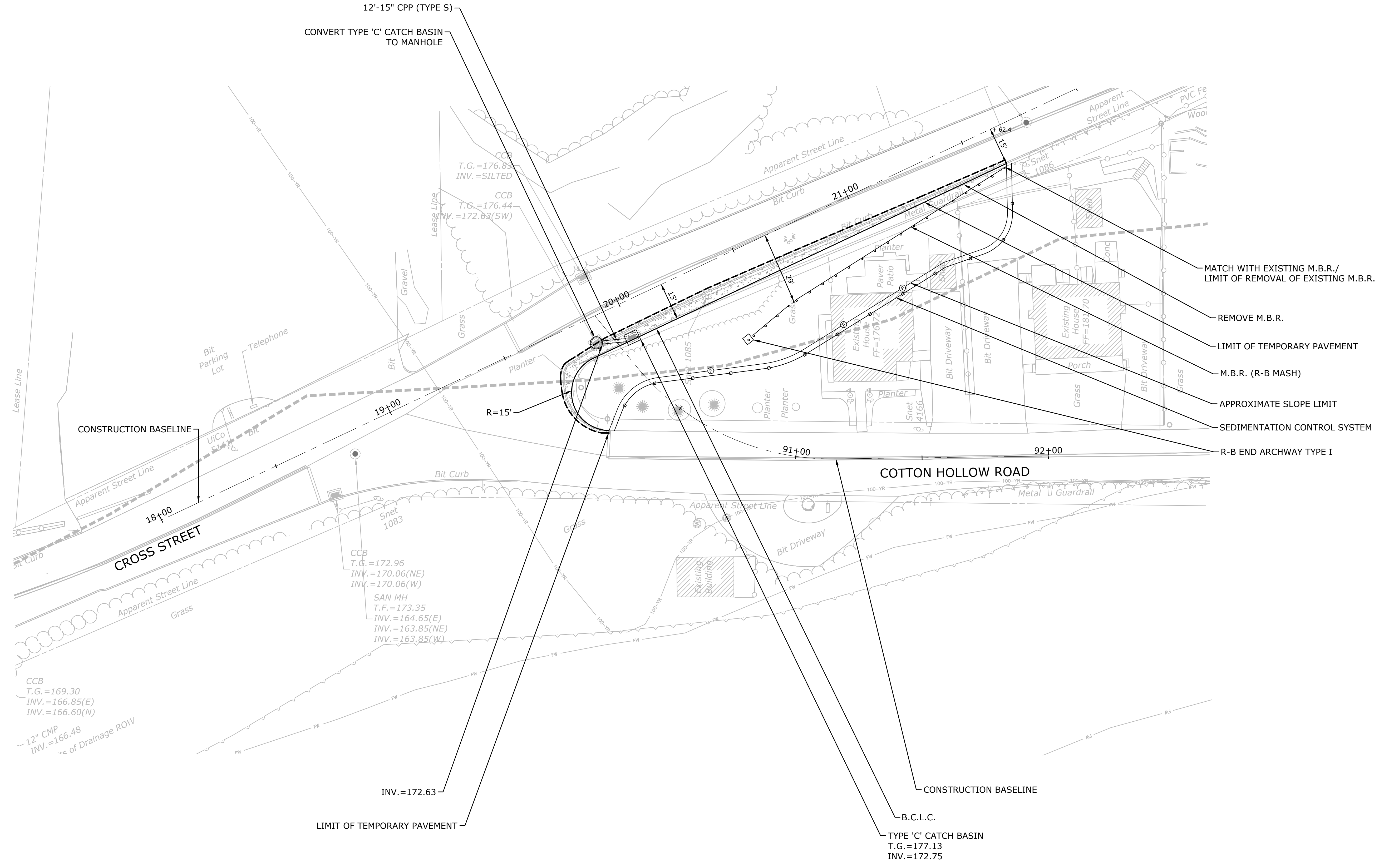
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DESCRIPTION	DATE	BY

**MAINTENANCE AND PROTECTION OF TRAFFIC  
 (STAGE-1-TEMPORARY PAVEMENT INSTALLATION)  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT**

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. MPT-02		

**57**

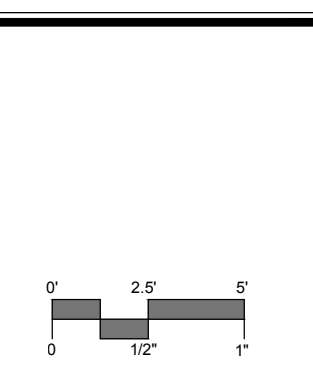
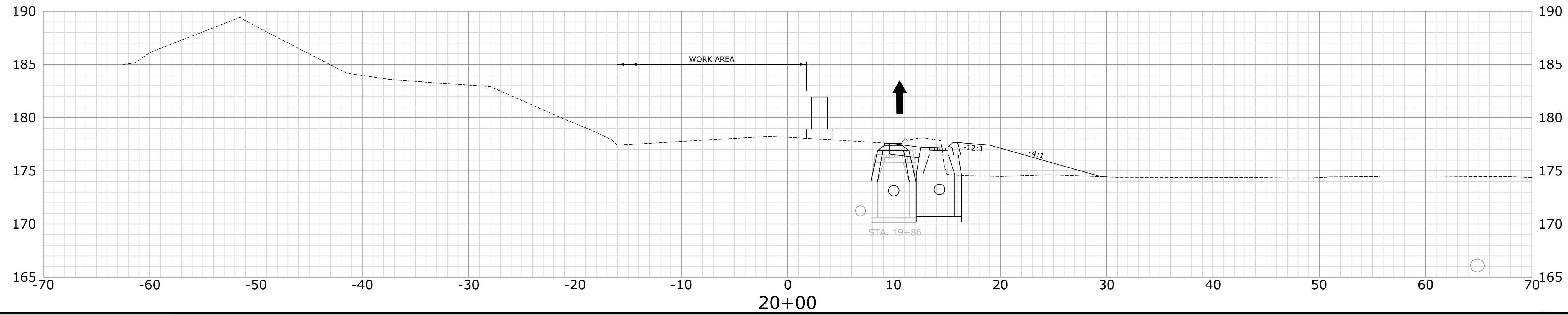
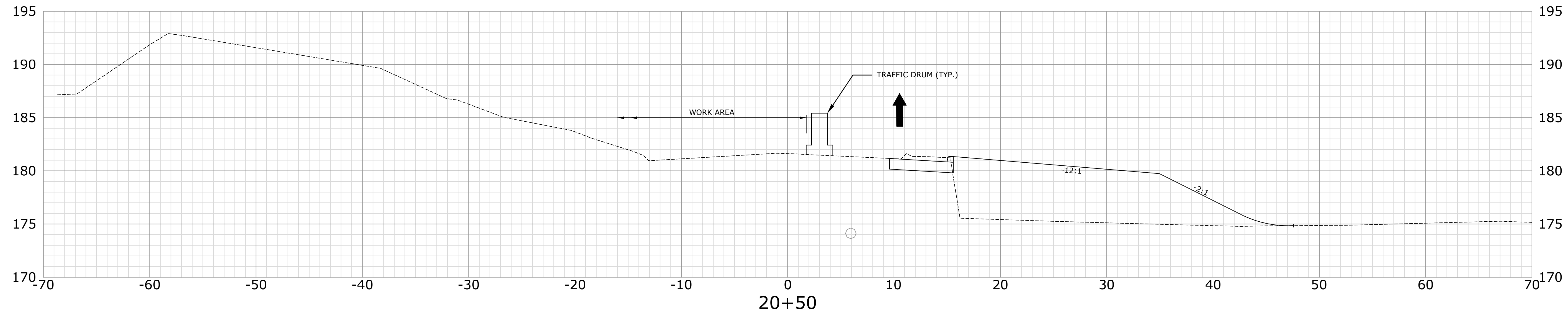
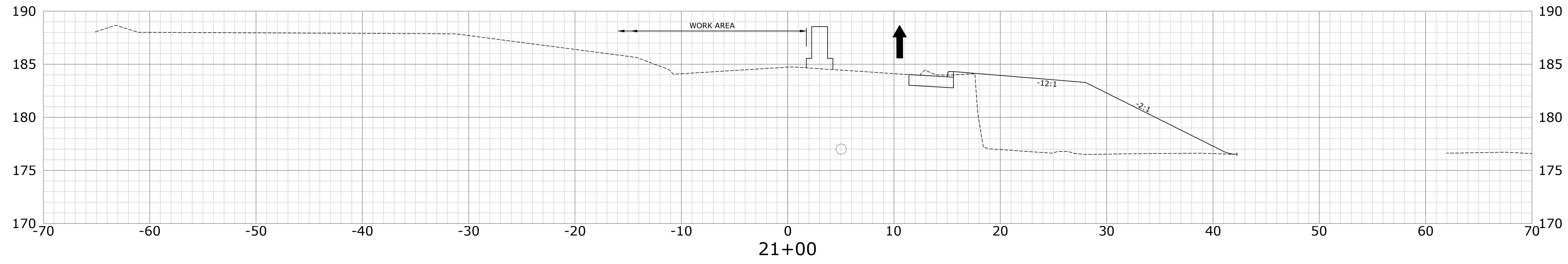
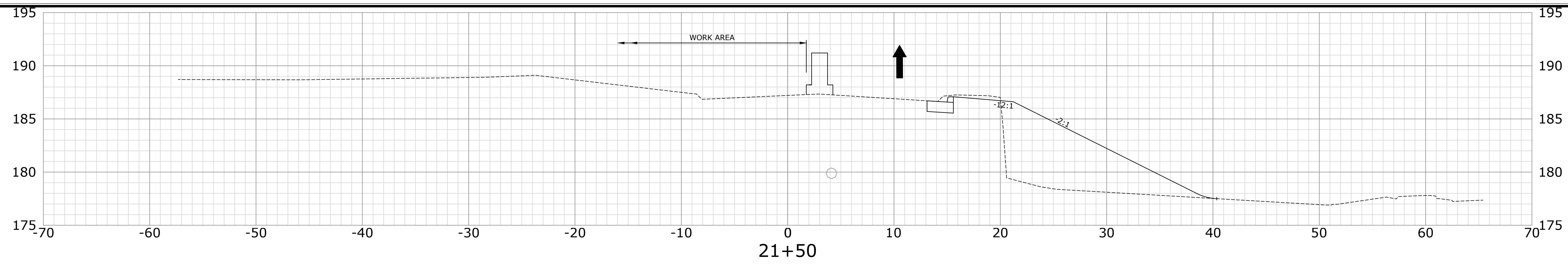


**LEGEND**

- TEMPORARY PAVEMENT
- CUT BITUMINOUS CONCRETE PAVEMENT

MPT-02 - 02 PRELIMINARY PLAN, 100% DESIGN, 100% PERMITS, 100% CONSTRUCTION, 100% AS-BUILT, 100% FINAL

SHEET 58 - PROTECTIVE AND PROTECTION OF TRAFFIC  
 (CROSS SECTIONS FOR STAGE-1 TEMPORARY PAVEMENT)  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT



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**MAINTENANCE AND PROTECTION OF TRAFFIC  
 (CROSS SECTIONS FOR STAGE-1 TEMPORARY PAVEMENT)**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
DRAWING NO. MPT-03		
<b>58</b>		



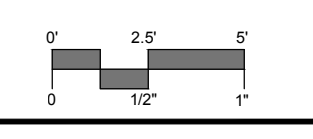
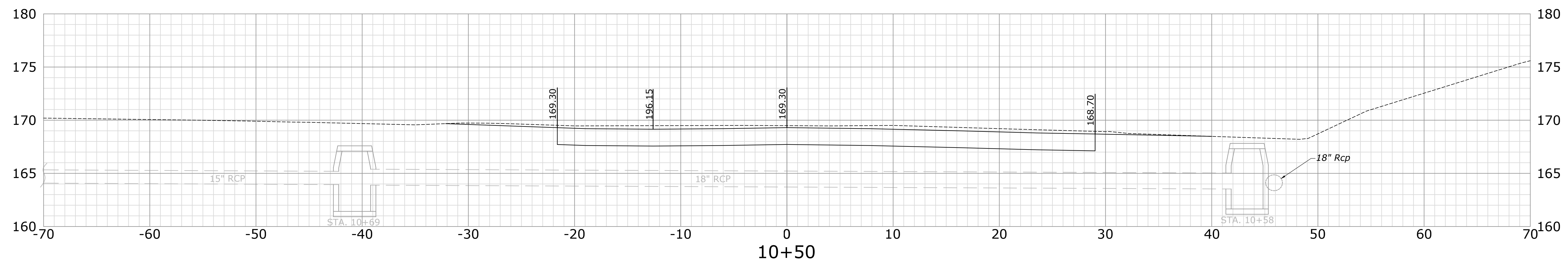
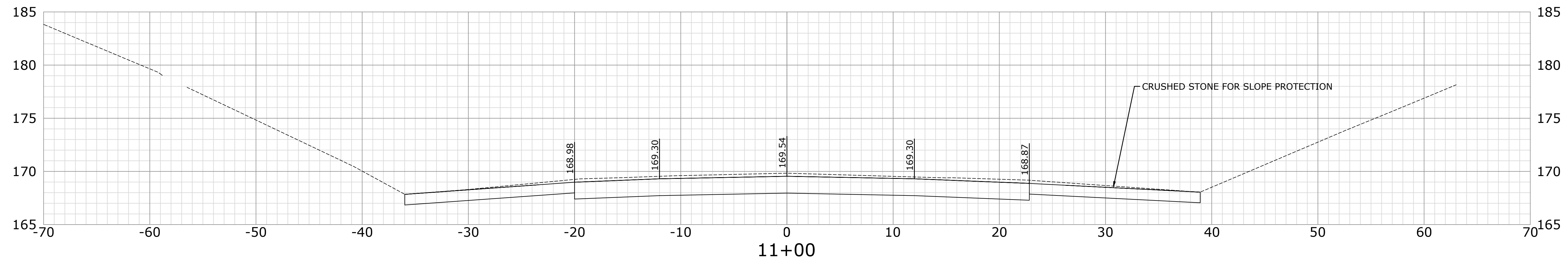
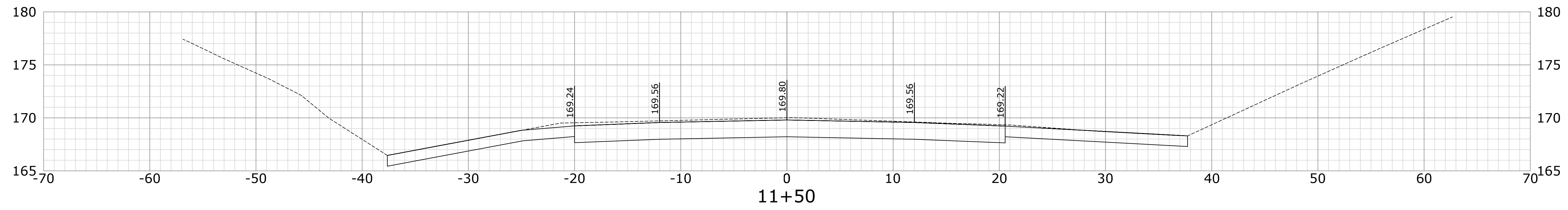








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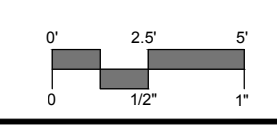
DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=5'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
SHEET NAME: XSC-01		

**61**

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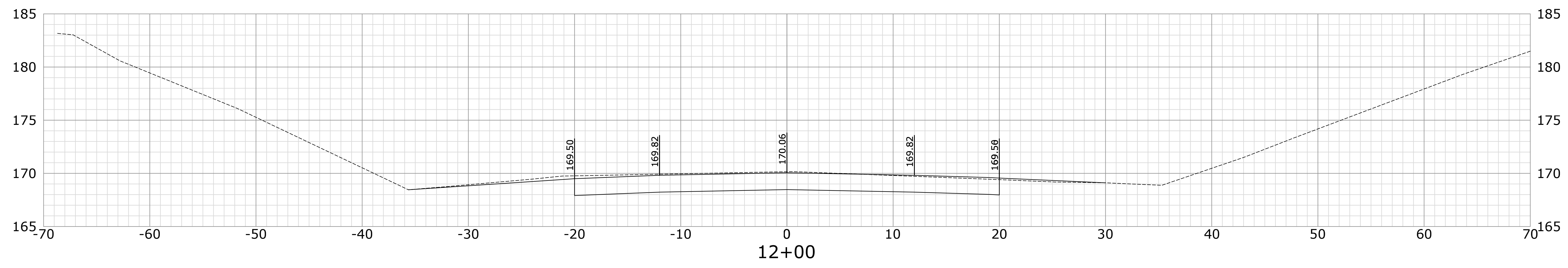
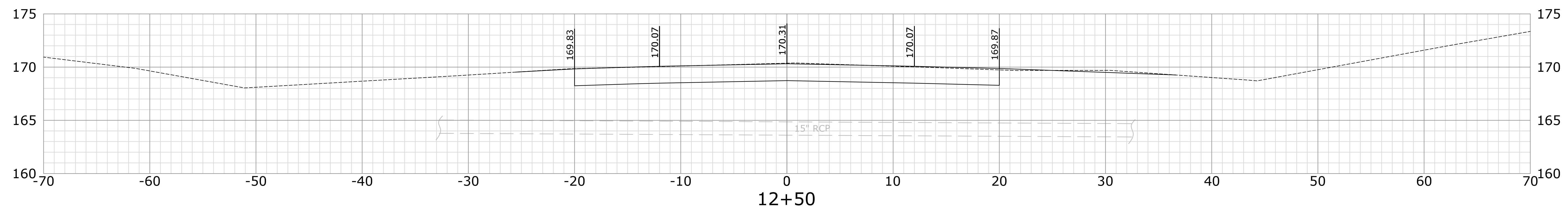
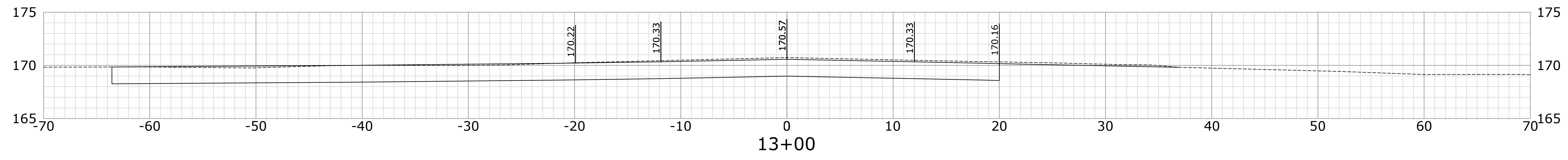
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**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

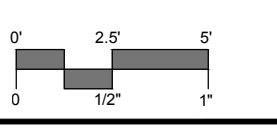
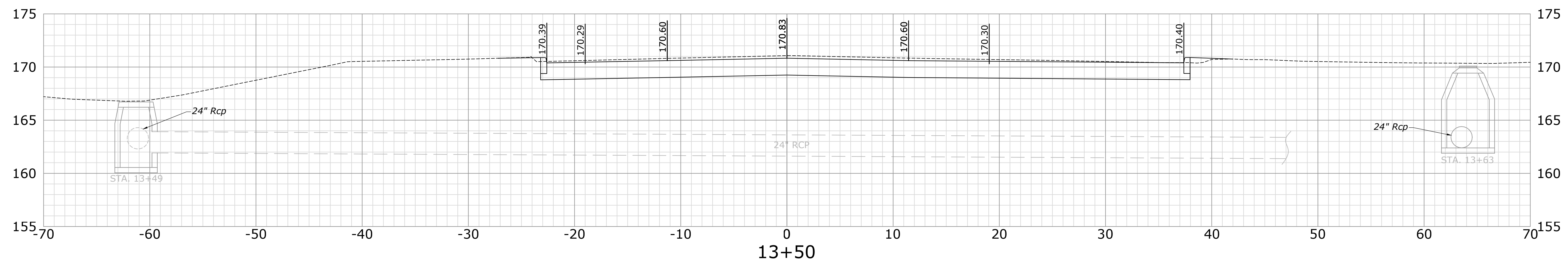
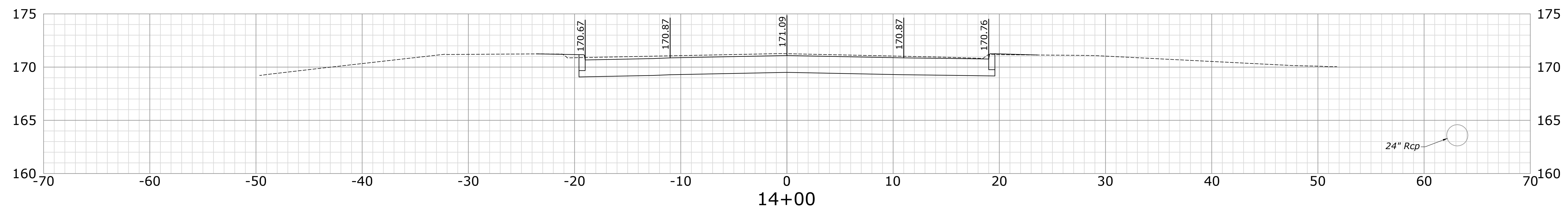
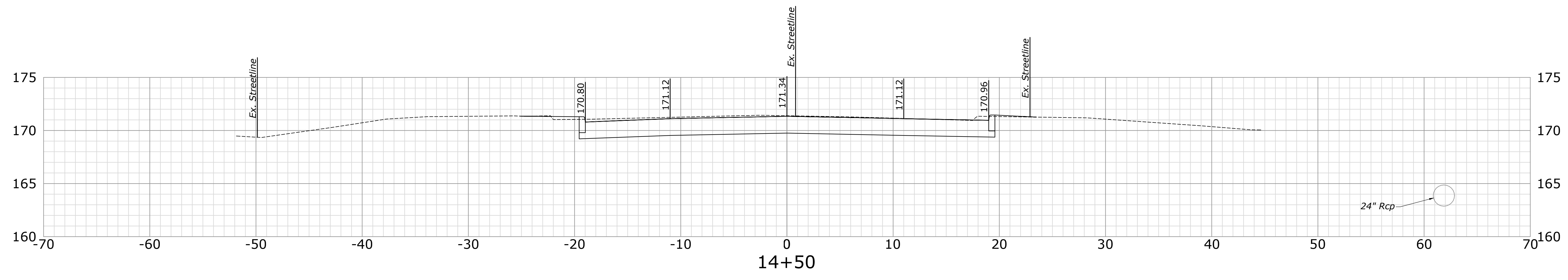
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SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-02		

SHEET NO. **62**





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**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-03		

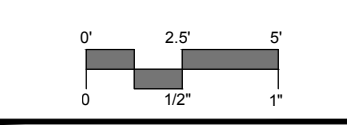
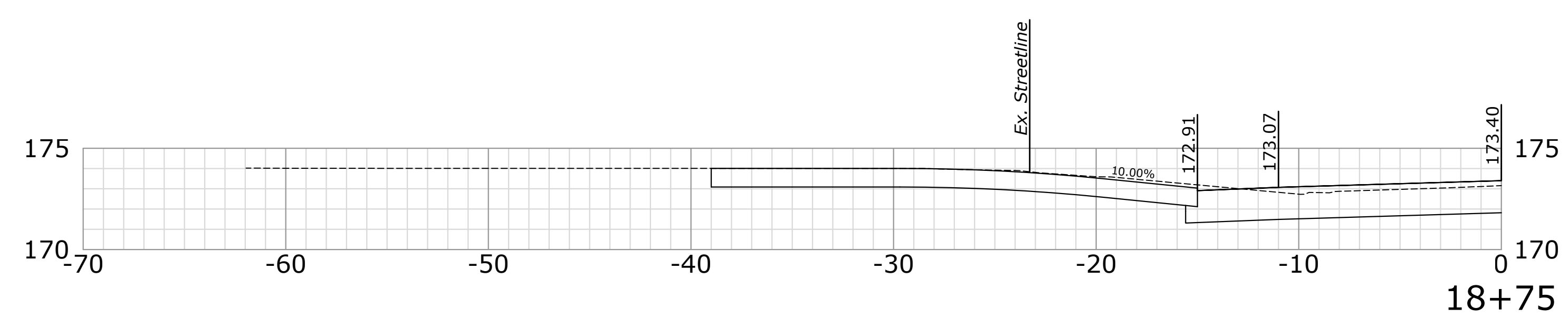
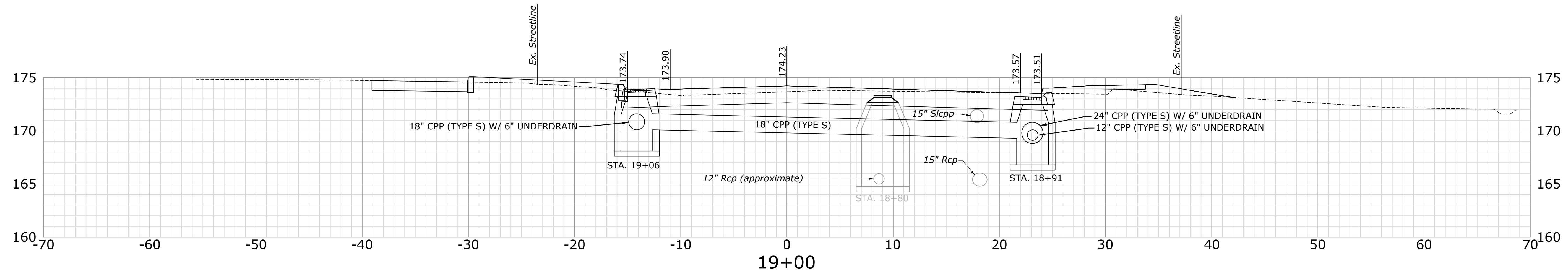
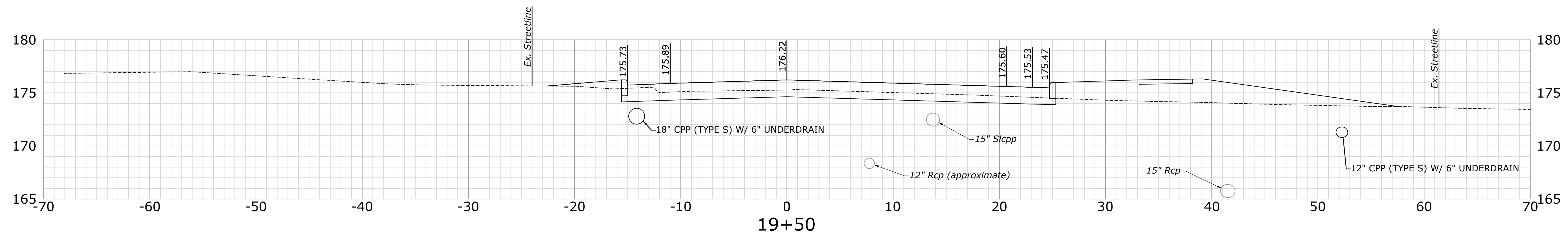
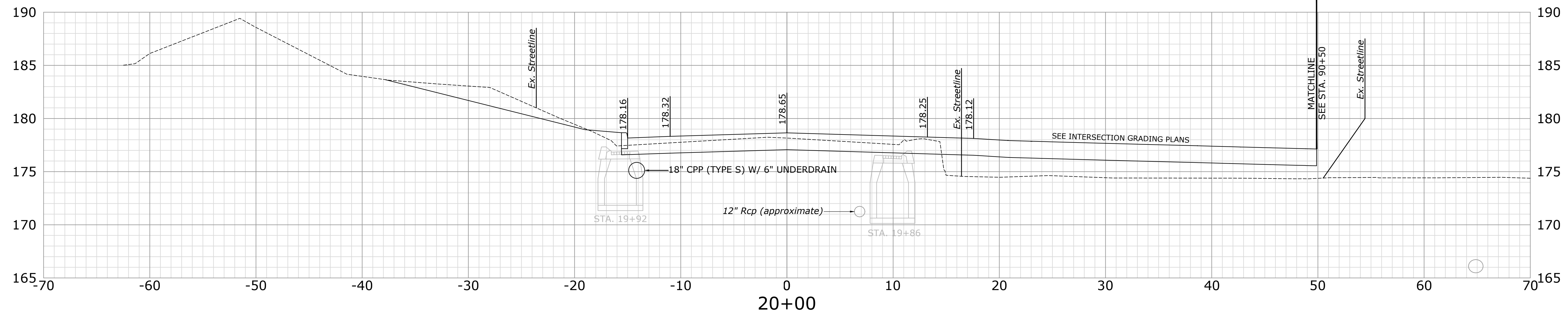
SHEET NO. **63**







10/20/16 - 3. REVISED BASE TO SHOW 10' - 10" OF WIDTH  
 FOR NEWLY CONSTRUCTED SIDEWALKS AND BIKEWAY.



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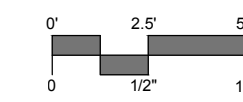
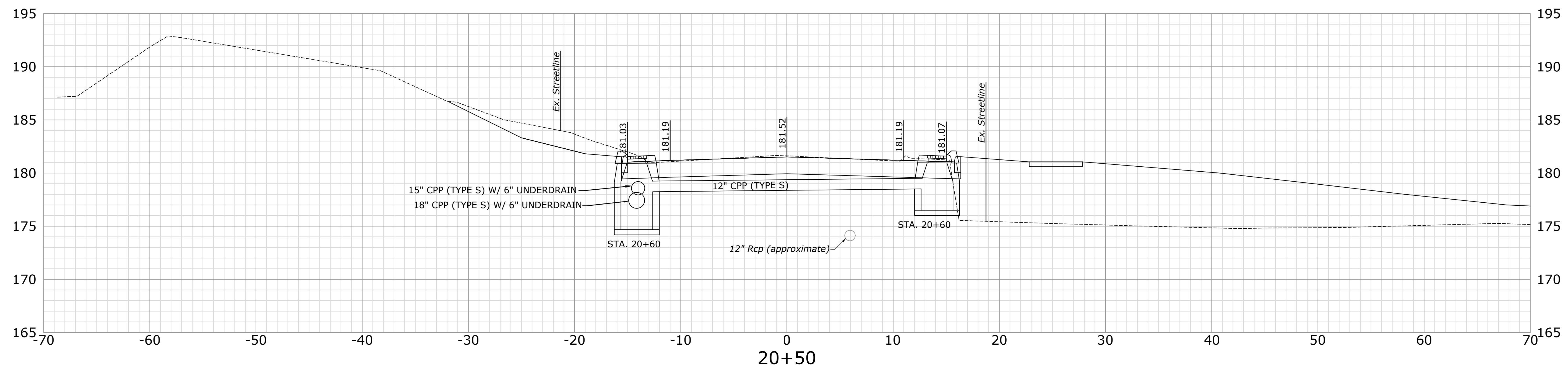
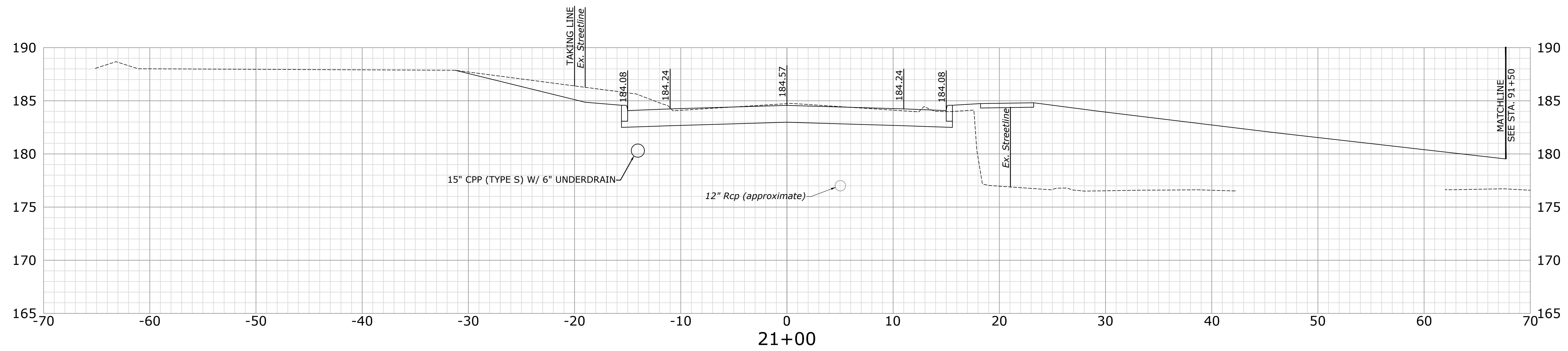
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-06		

SHEET NO. **66**



10/20/18 - 2. REVISIONS: SEE THE WORK SHEET FOR THE DATE OF THE REVISIONS.



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**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-07		

SHEET NO. **67**



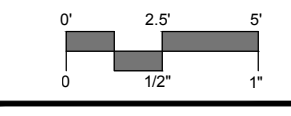
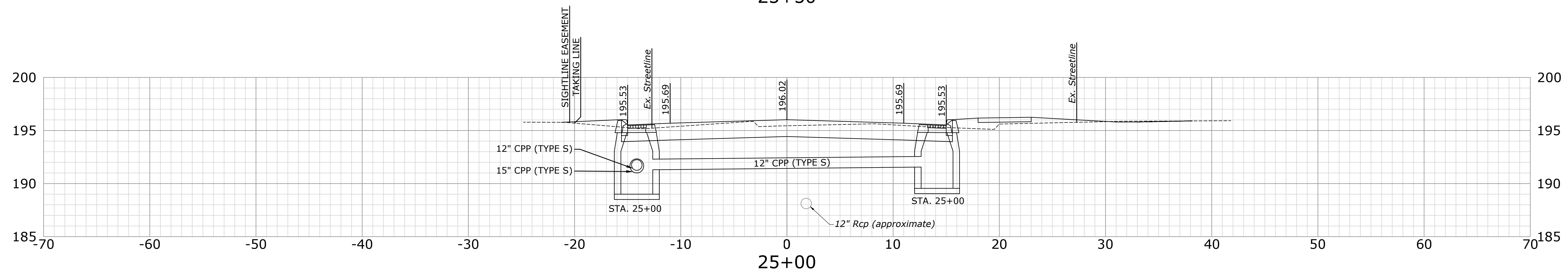
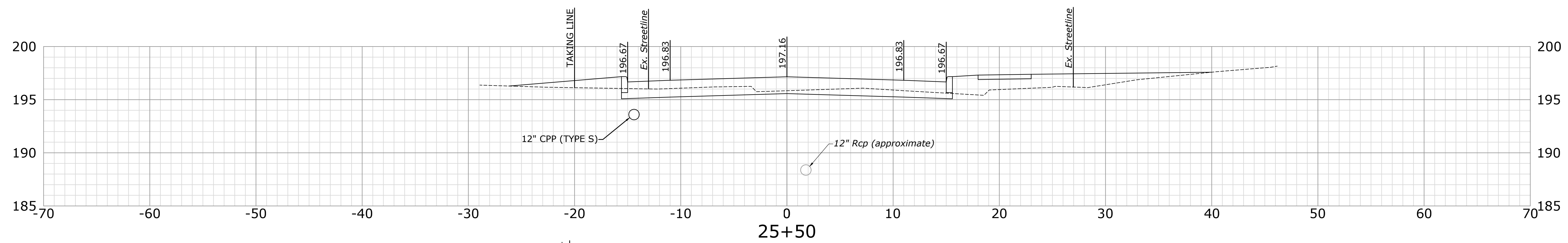
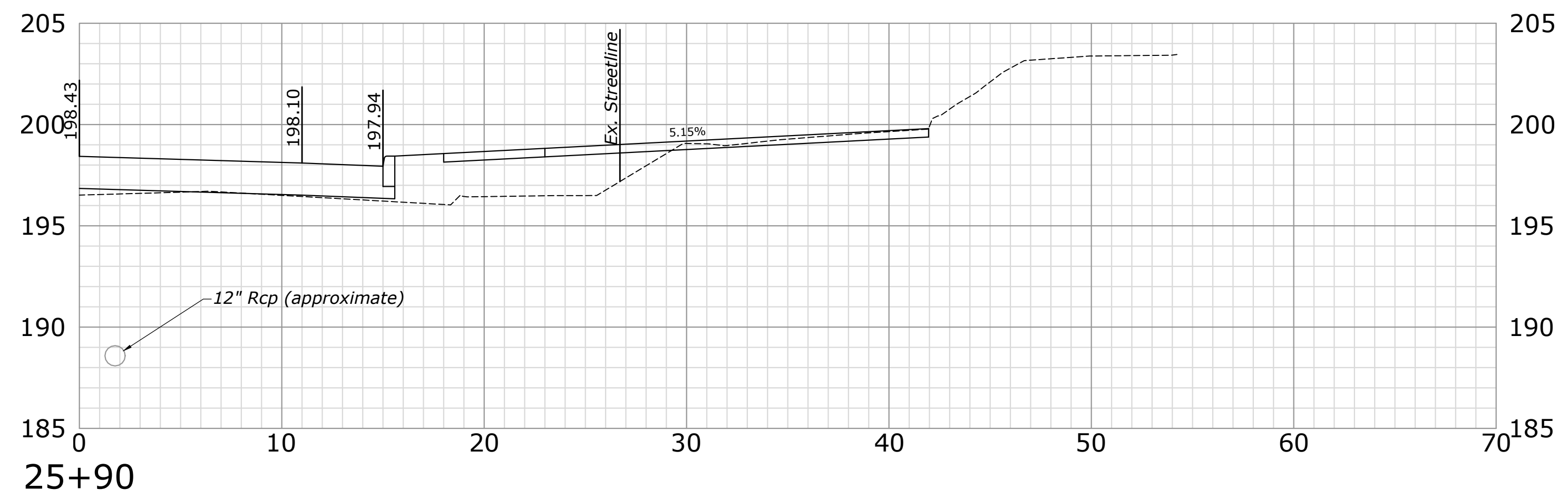
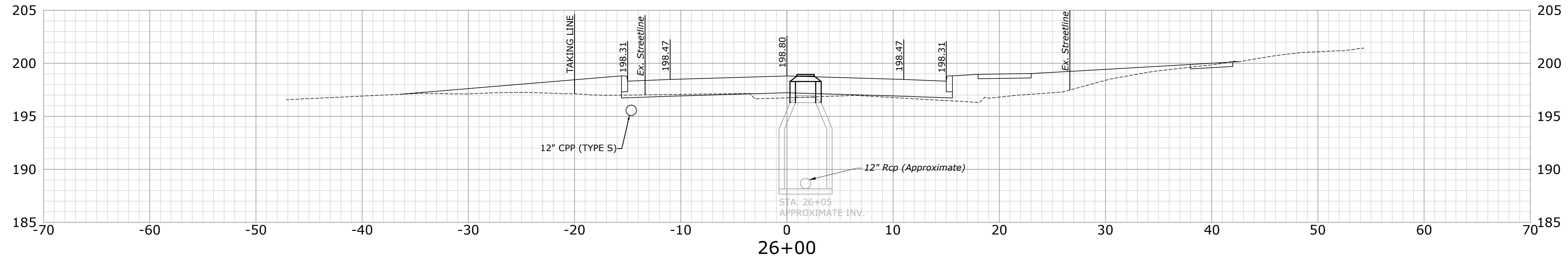








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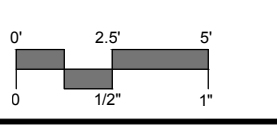
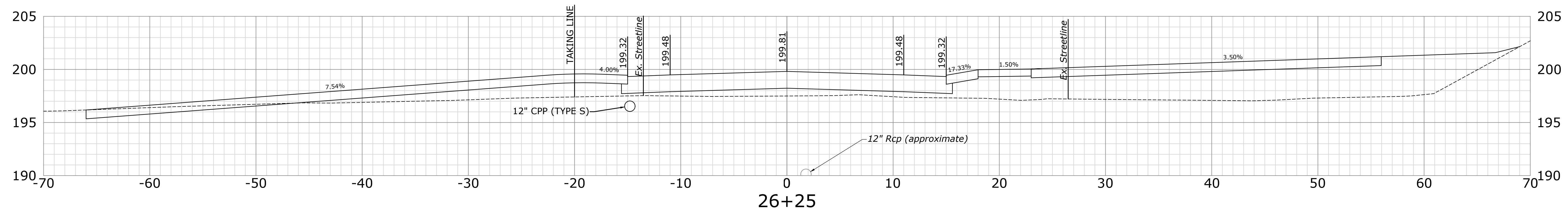
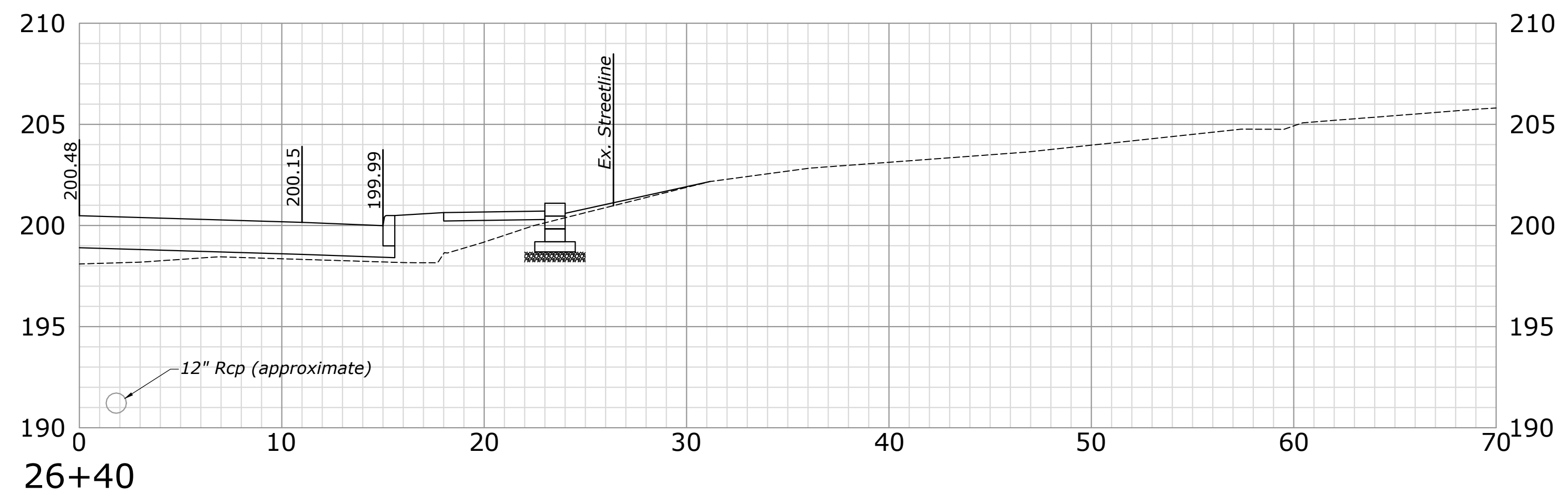
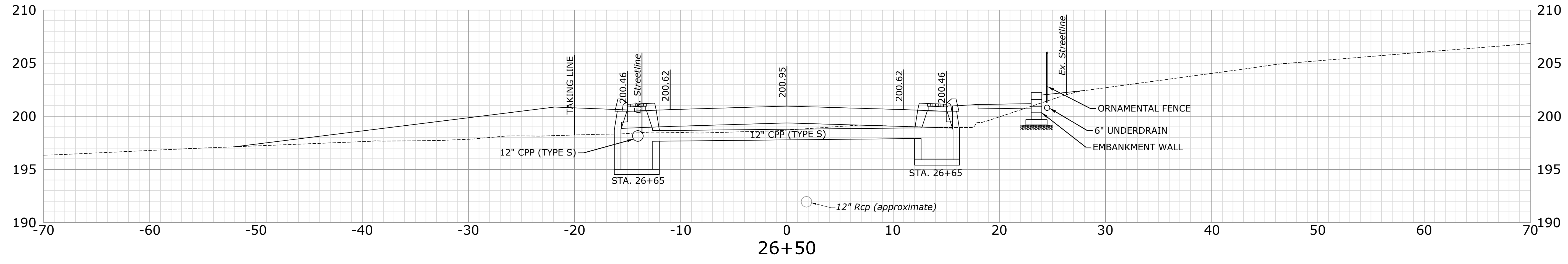
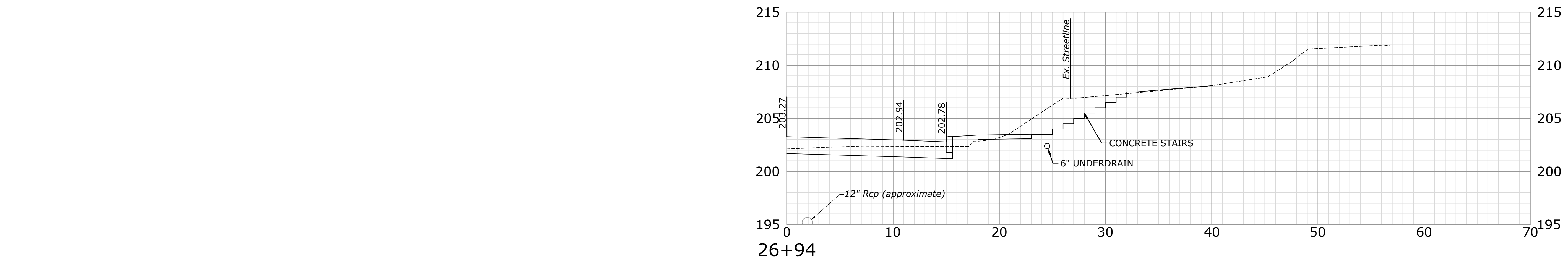


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DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
 RECONSTRUCTION OF CROSS STREET  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
1"=5'		
SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-11		
71		



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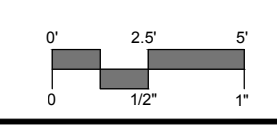
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-12		

**72**



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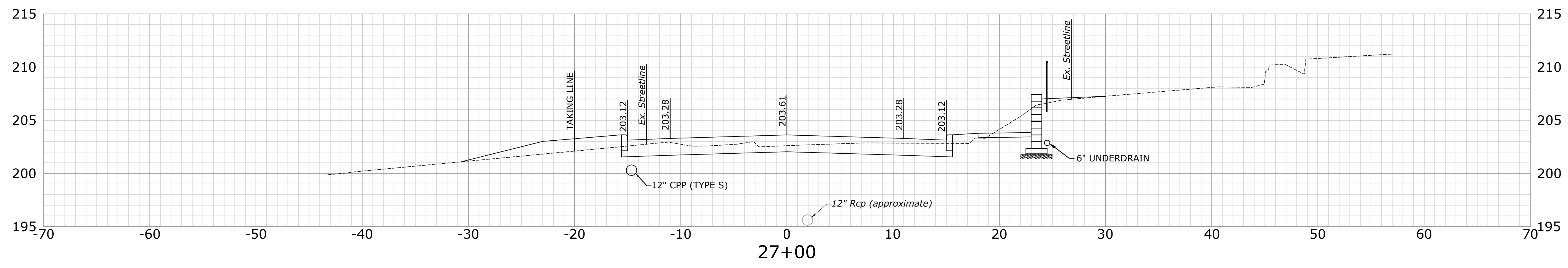
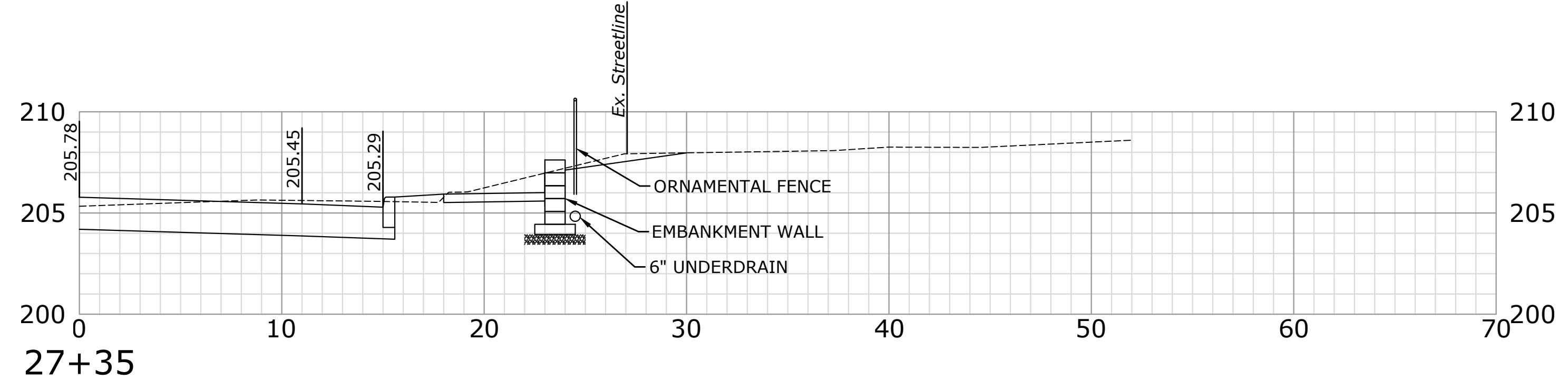
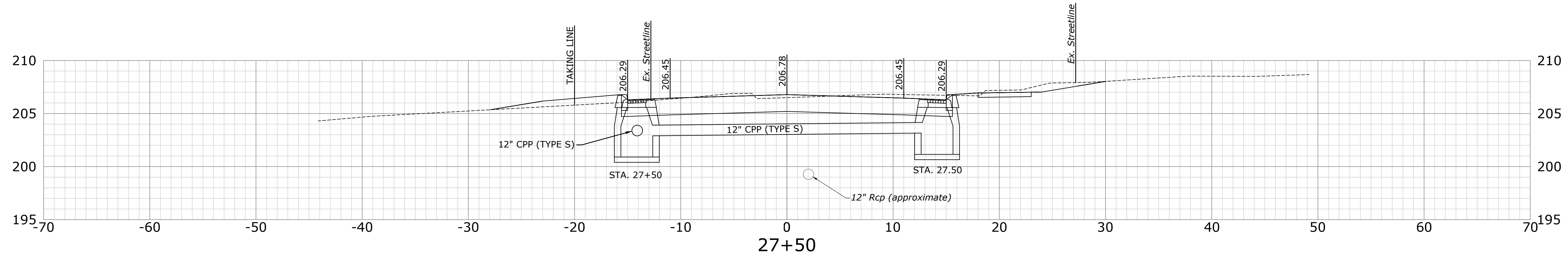
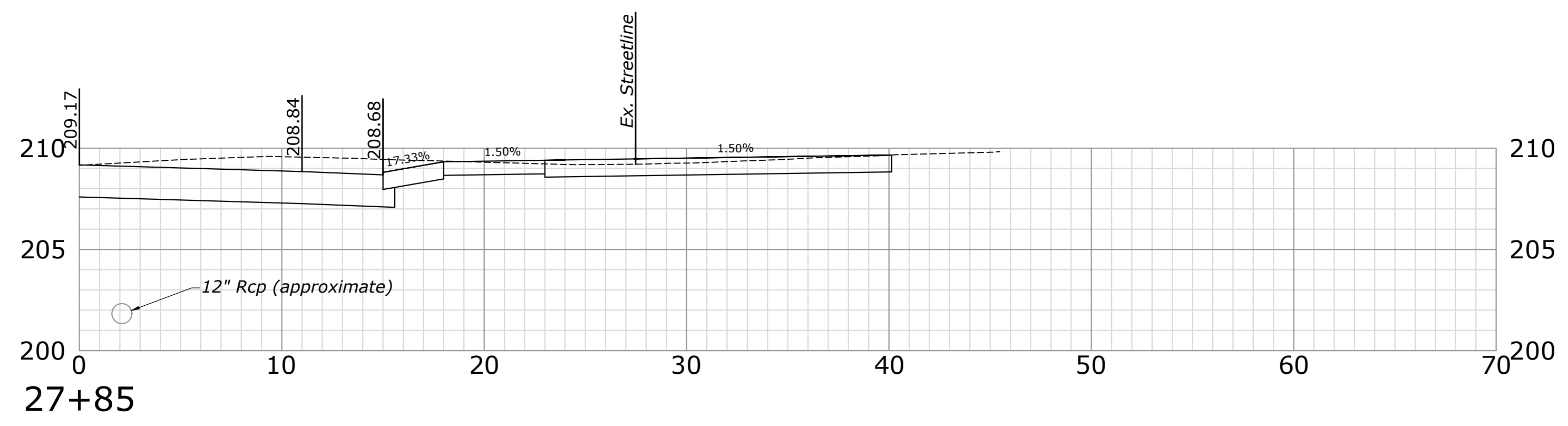
**MILONE & MACBROOM**  
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 Cheshire, Connecticut 06410  
 (203) 271-1773 Fax (203) 272-9733  
 www.miloneandmacbroom.com

DESCRIPTION	DATE	BY

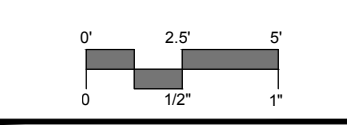
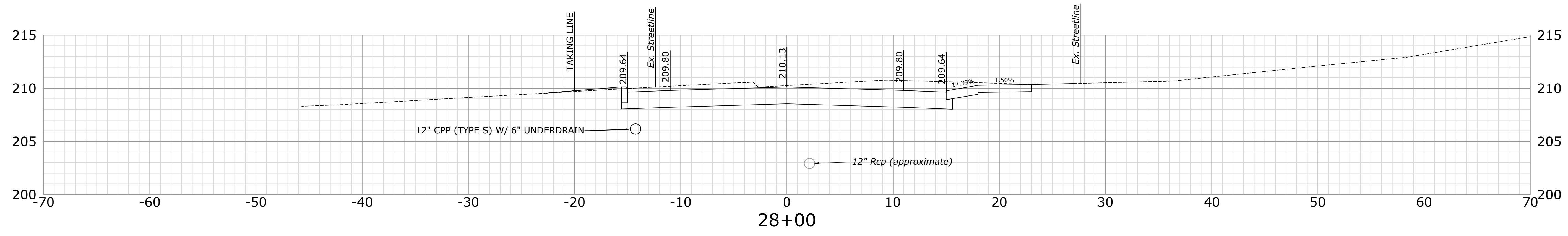
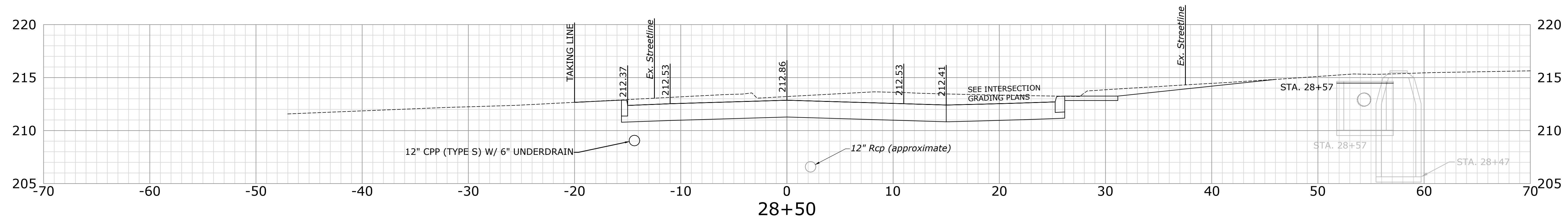
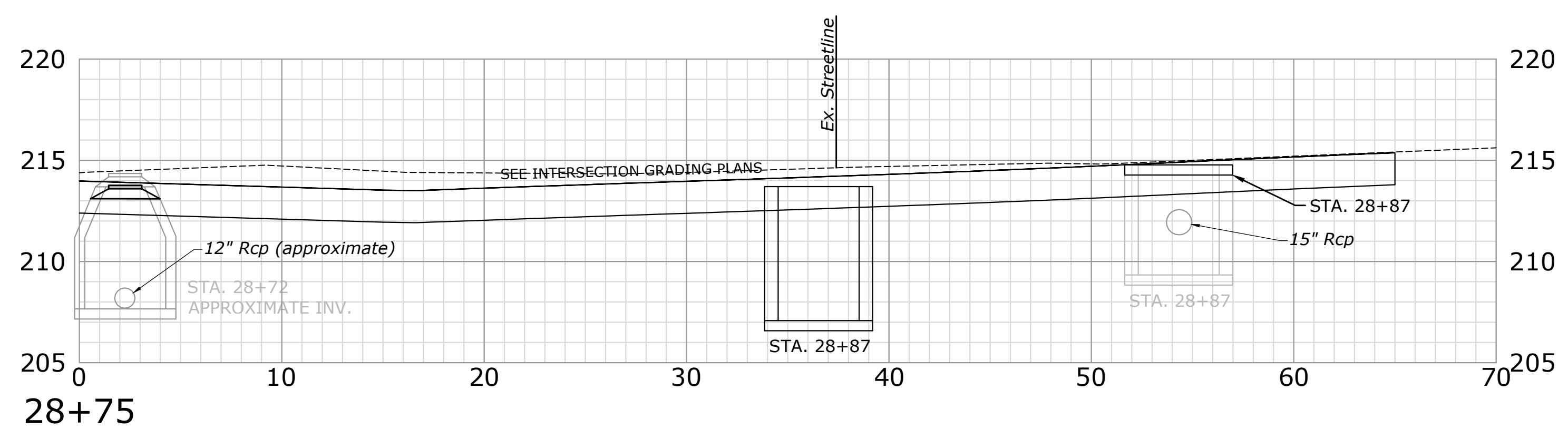
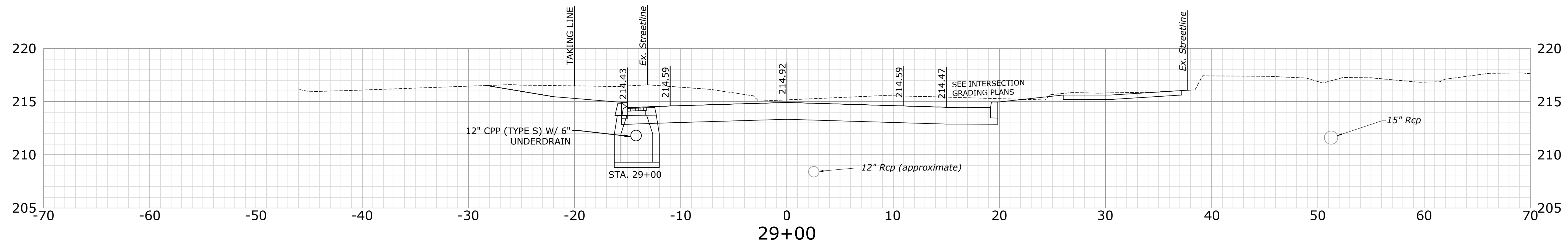
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-13		

**73**  
 SHEET NO.



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DESCRIPTION	DATE	BY

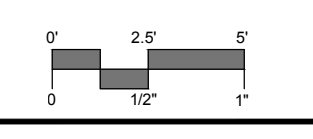
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**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-14		

**74**



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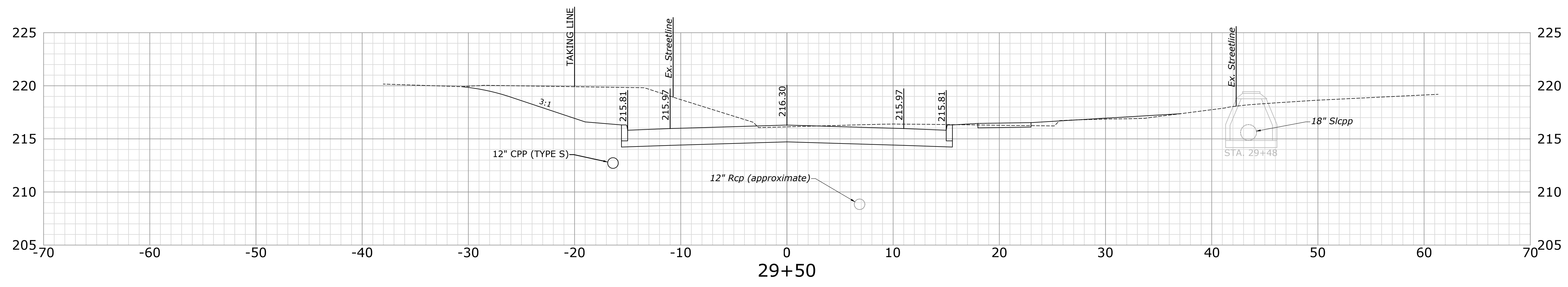
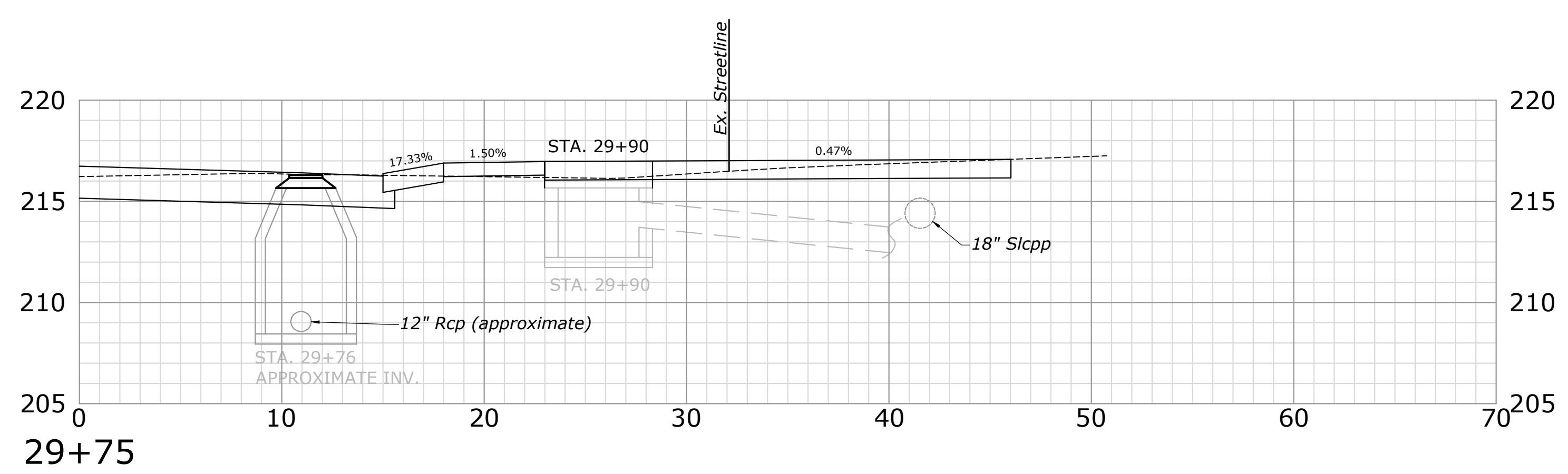
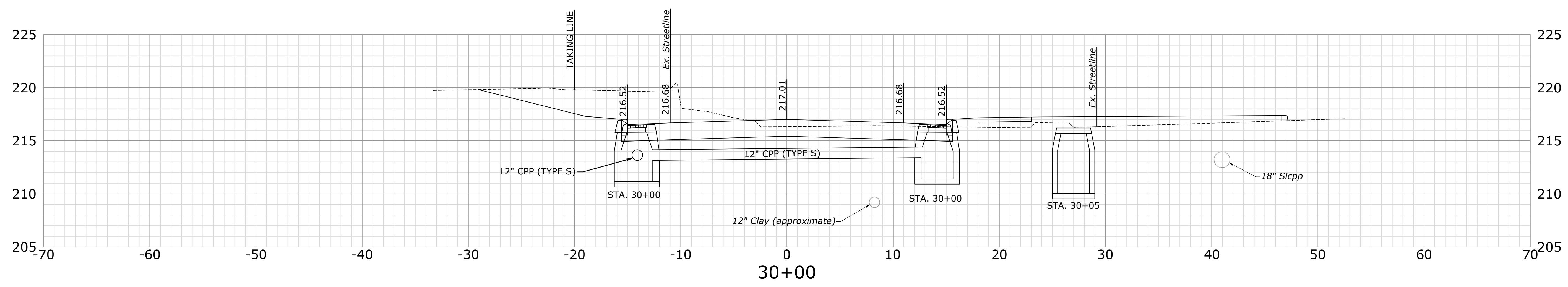
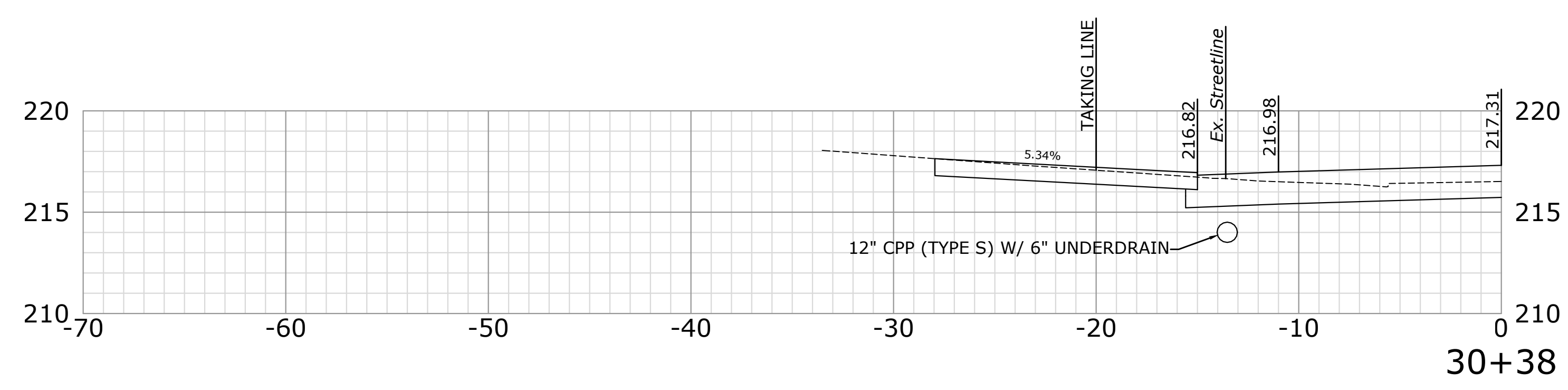
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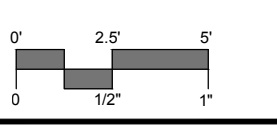
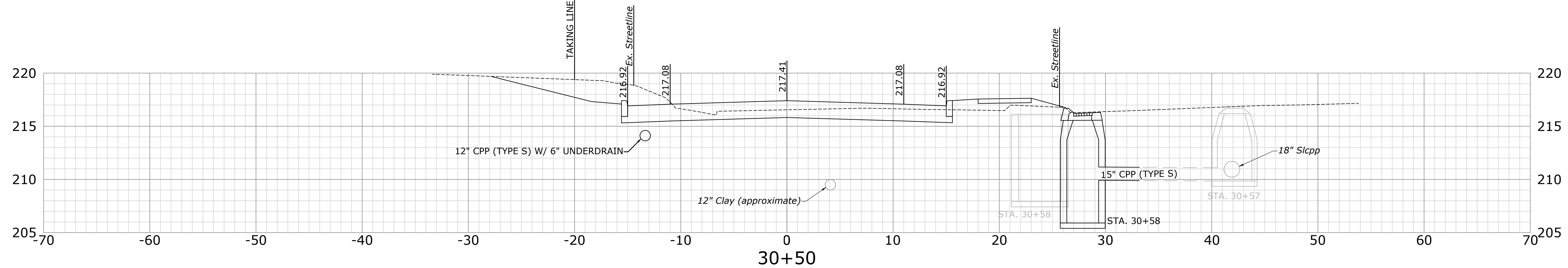
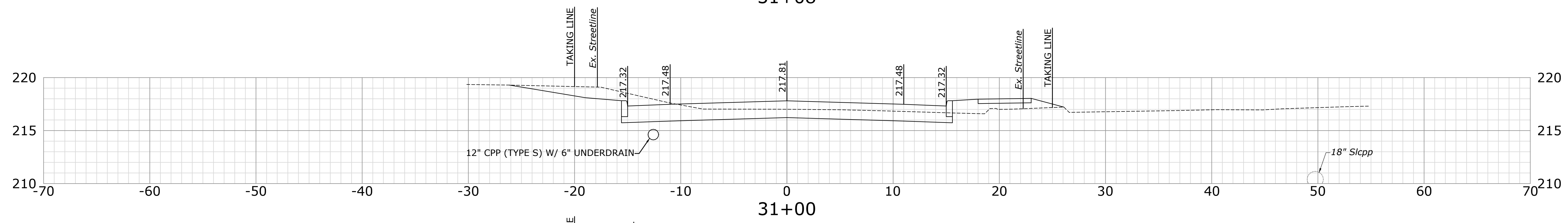
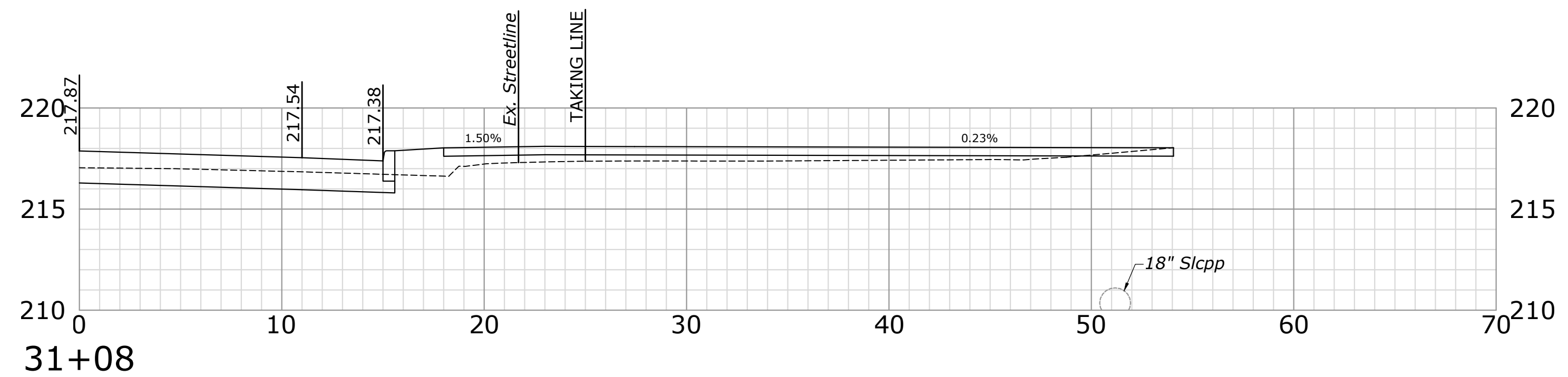
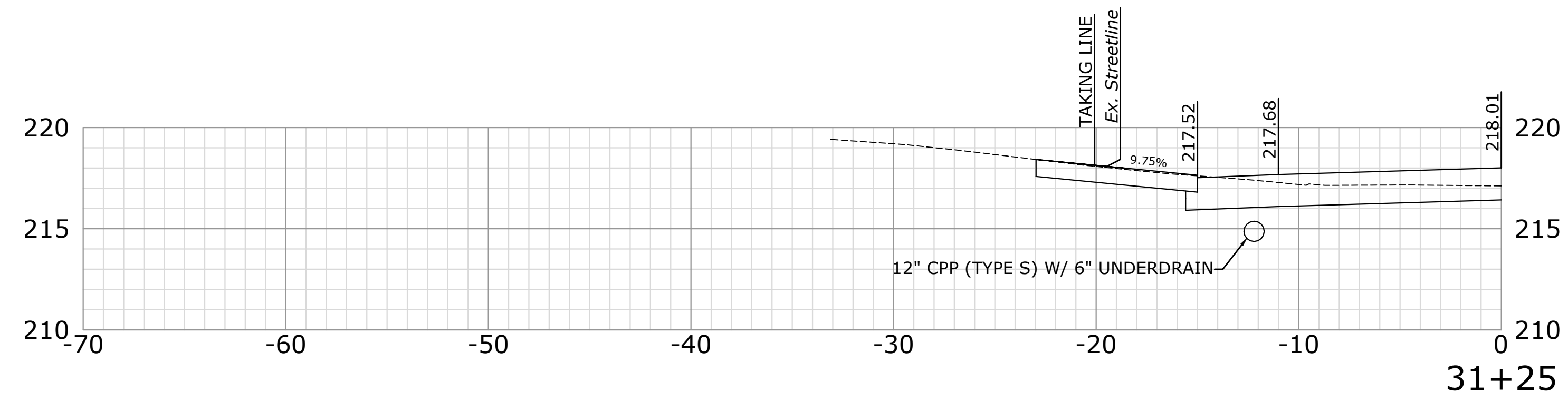
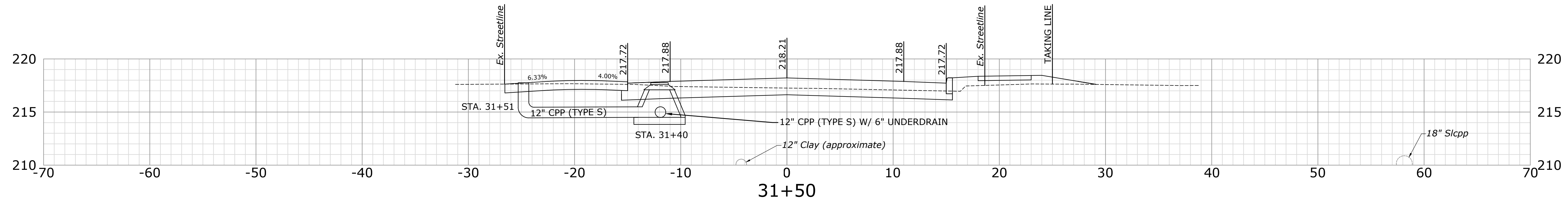
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-15		

**75**



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DESCRIPTION	DATE	BY

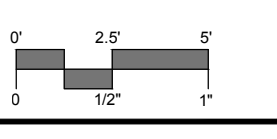
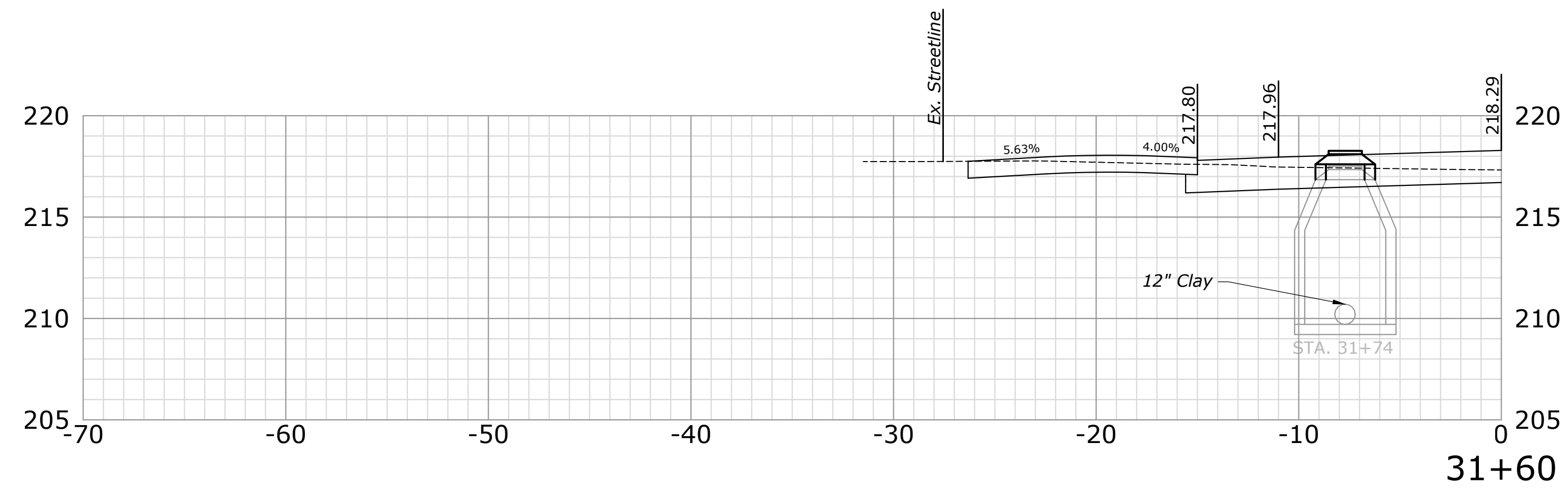
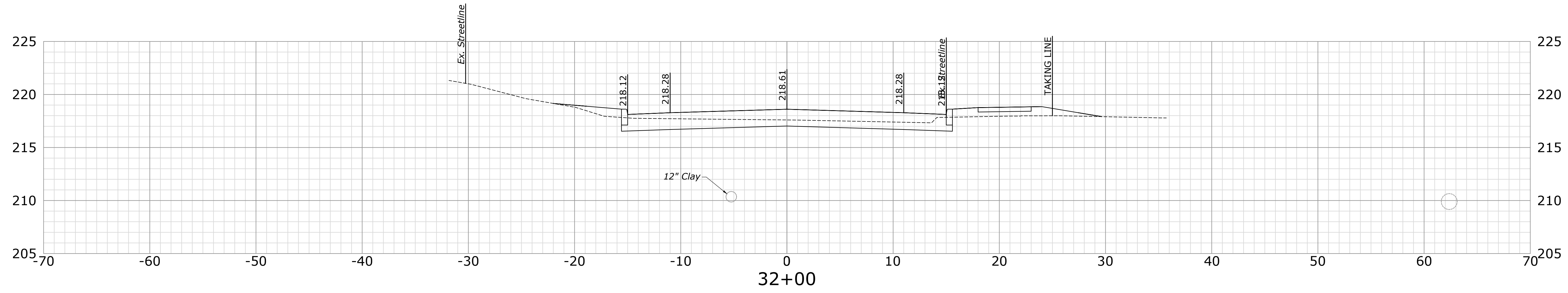
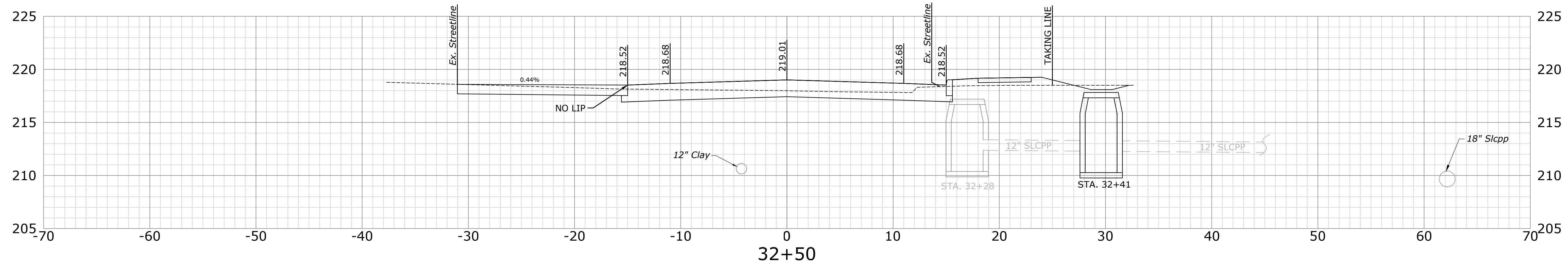
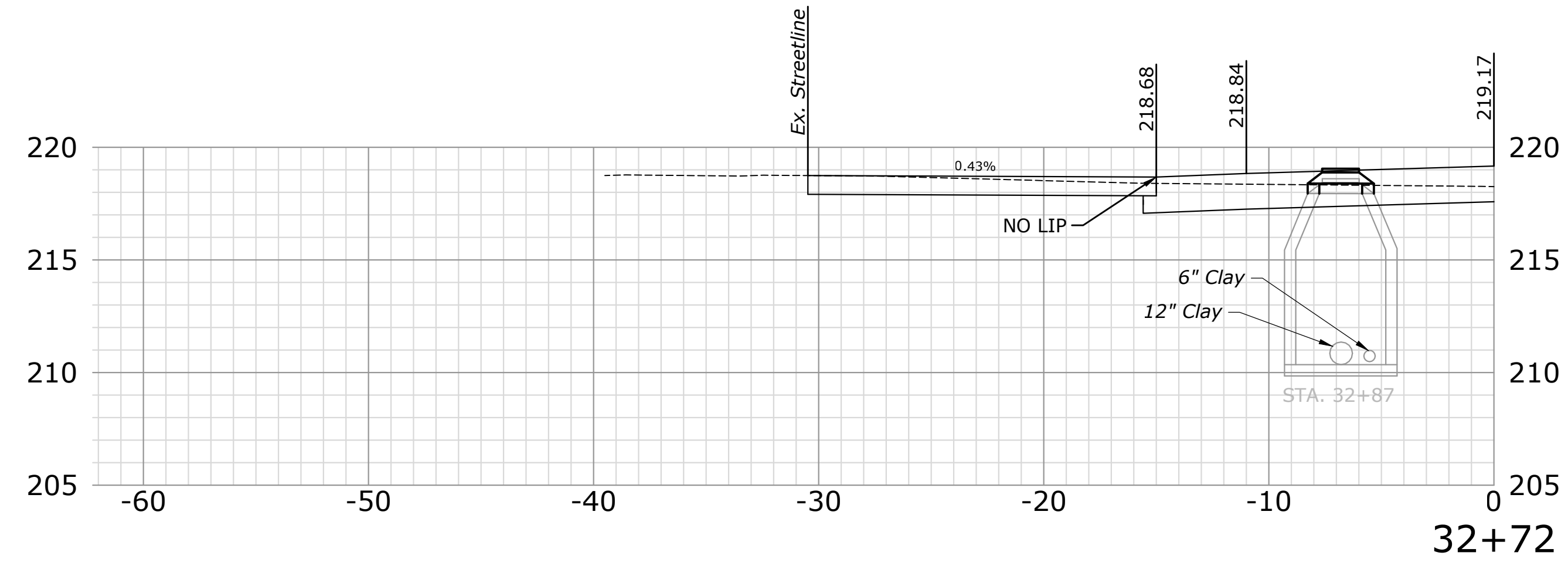
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-16		

**76**



SHEET 77 - 3' RECONSTRUCTION OF CROSS STREET  
 11/18/18 10:00 AM  
 11/18/18 10:00 AM



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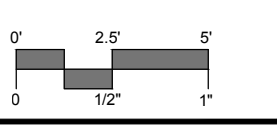
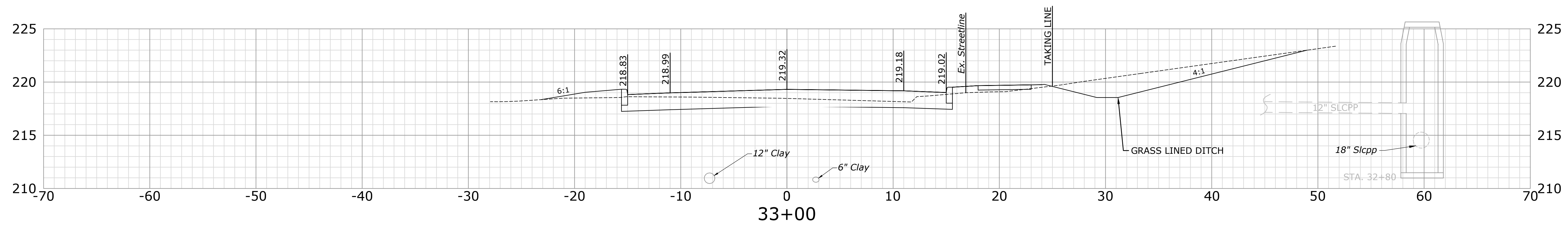
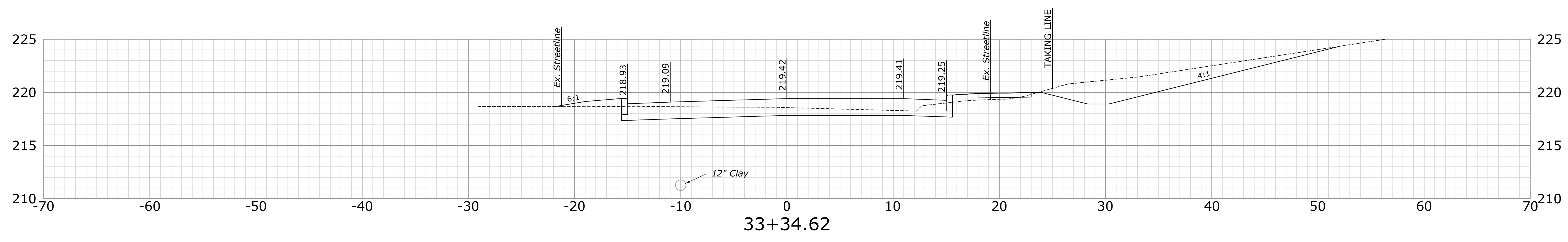
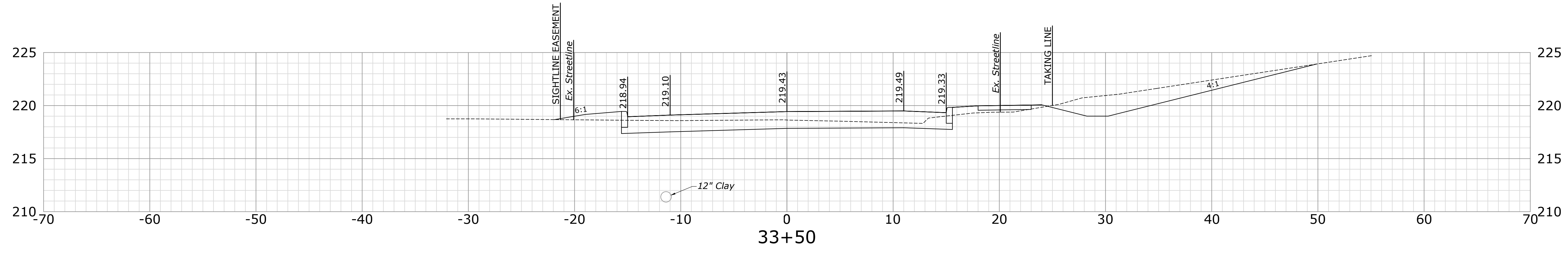
DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-17		

**77**

SHEET 78 - RECONSTRUCTION OF CROSS STREET, NAUGATUCK, CONNECTICUT  
 PROJECT NO. 87-145  
 DATE: SEPTEMBER 7, 2018  
 DRAWN BY: TDS  
 CHECKED BY: MJJ  
 DESIGNED BY: DP



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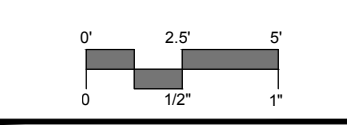
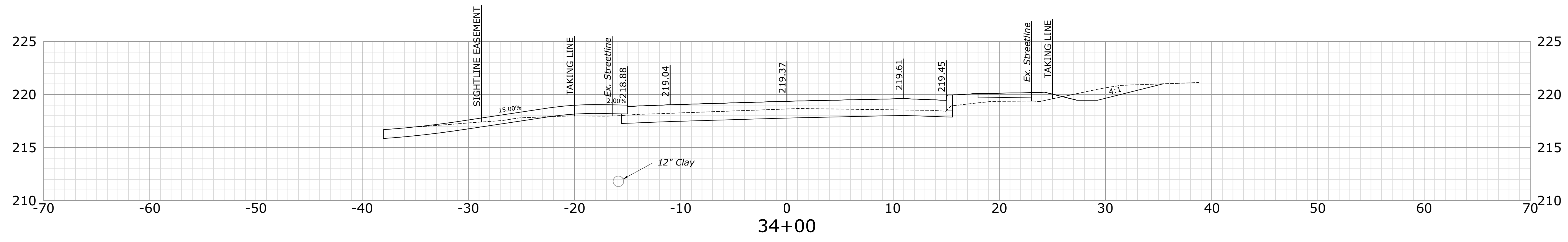
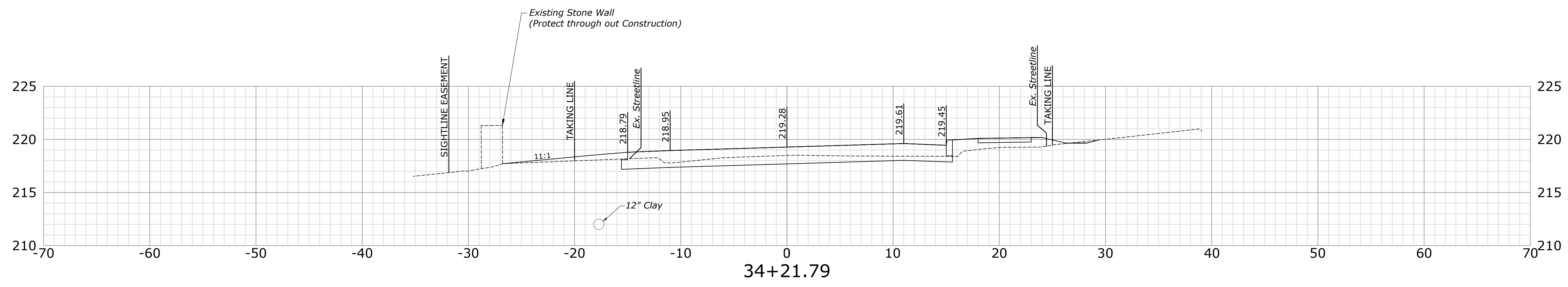
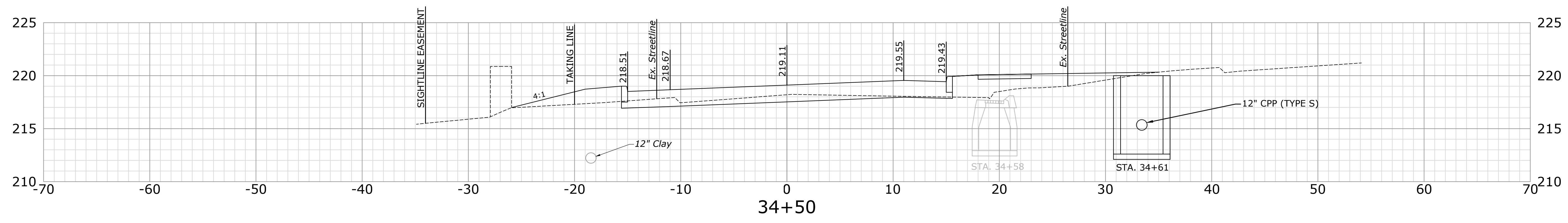
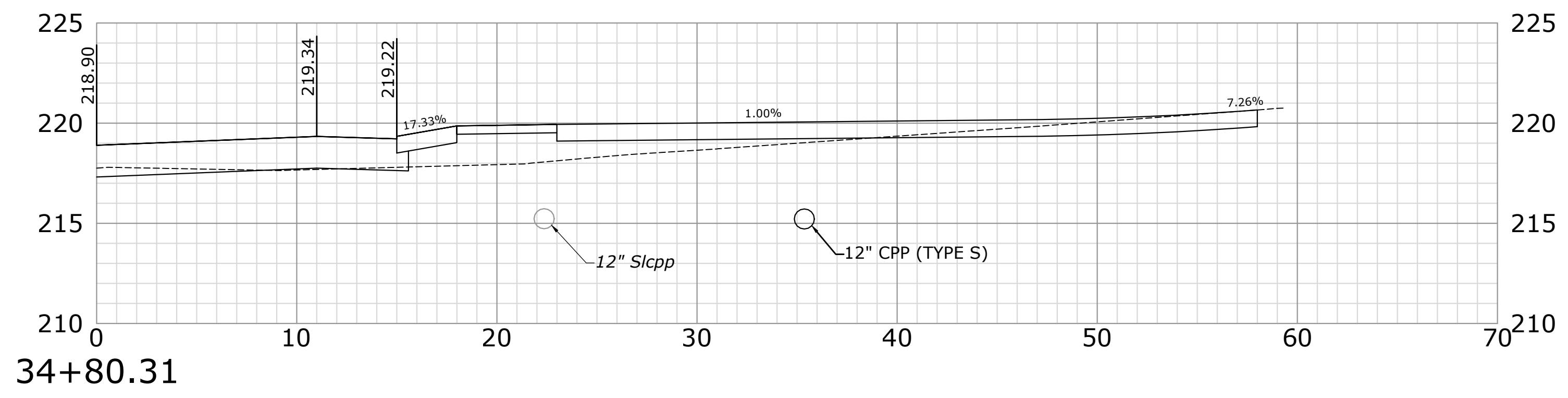
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-18		

SHEET NO. **78**



10/22/18 - 2. REVISIONS: SEE THE 2018 AND 2019 DATED ON SHEET 78 FOR THE REVISIONS TO THE CROSS SECTION.



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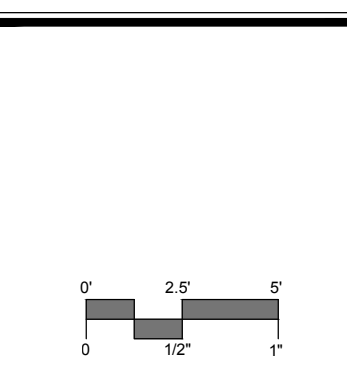
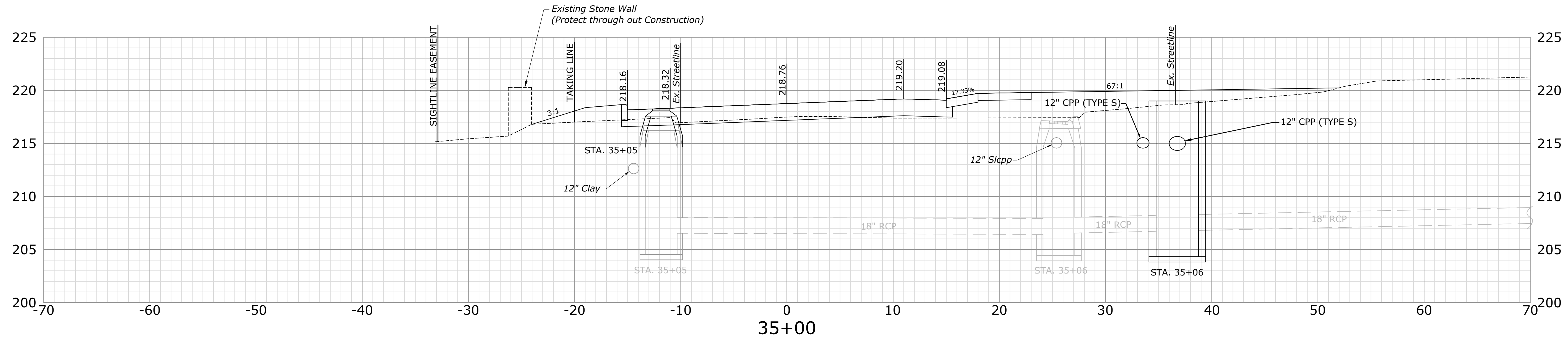
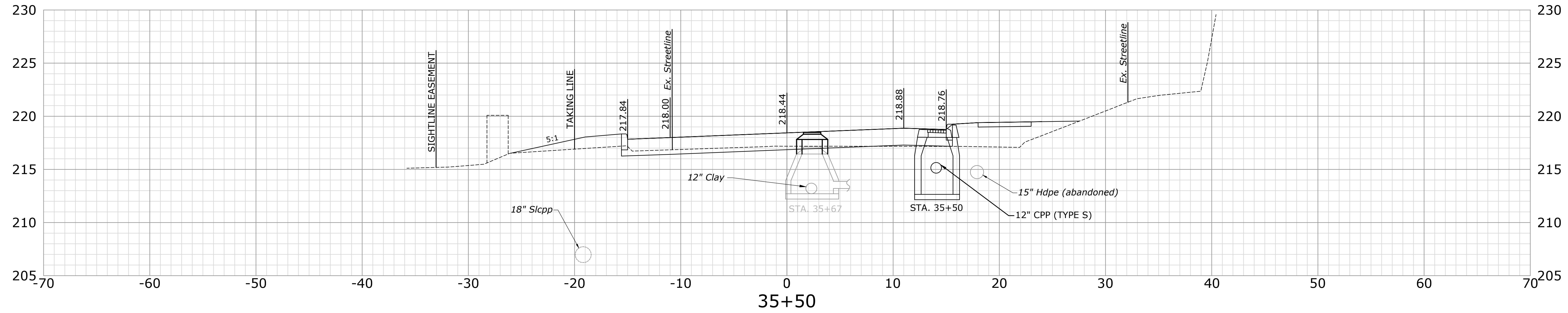
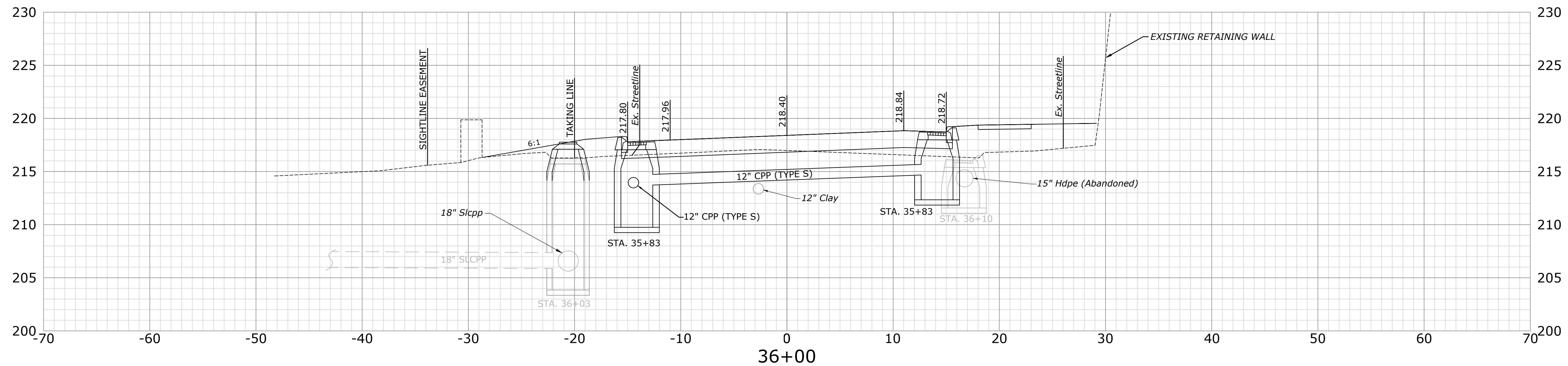
DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-19		

SHEET NO.  
**79**

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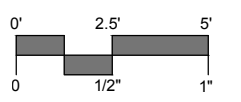
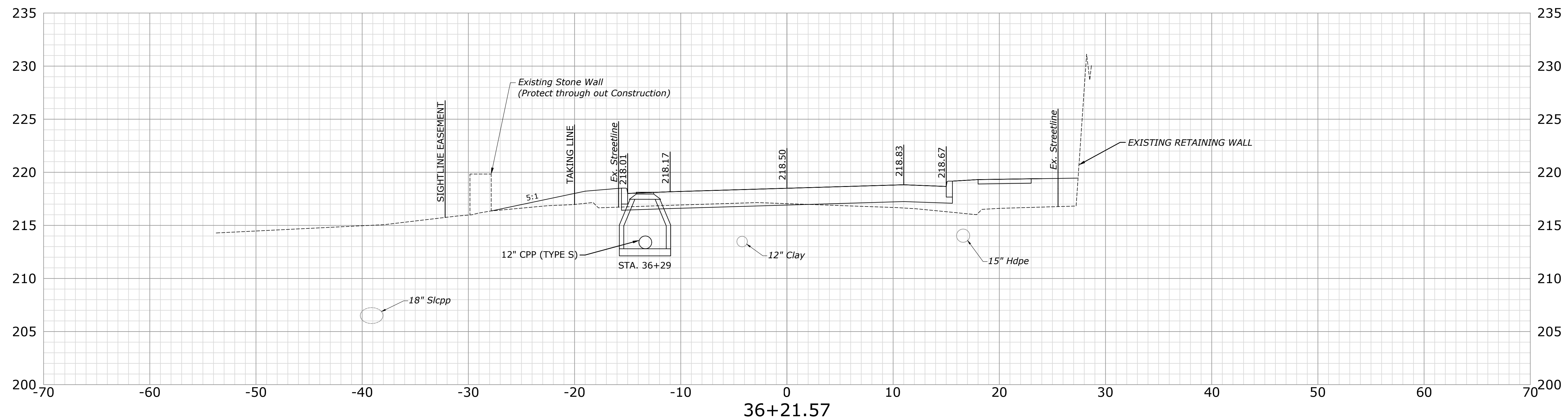
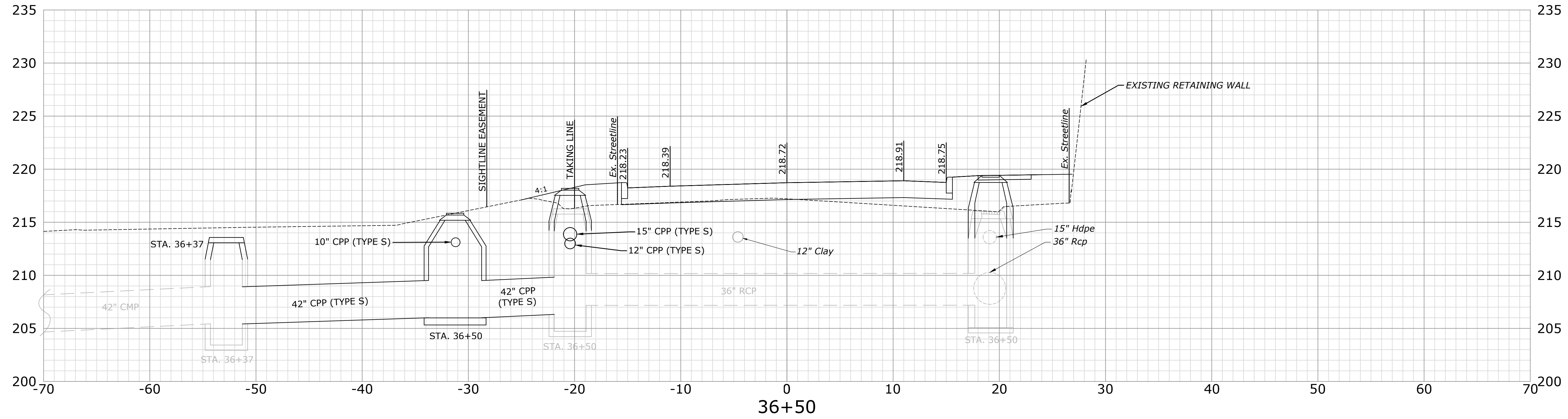
DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-20		
<b>80</b>		



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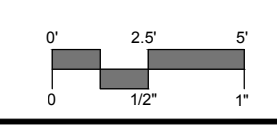
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DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

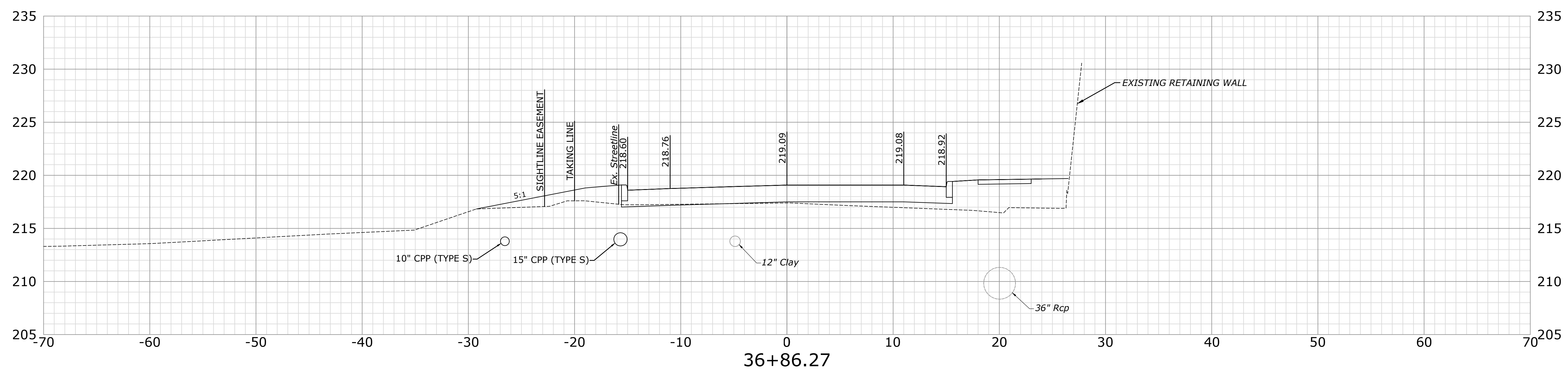
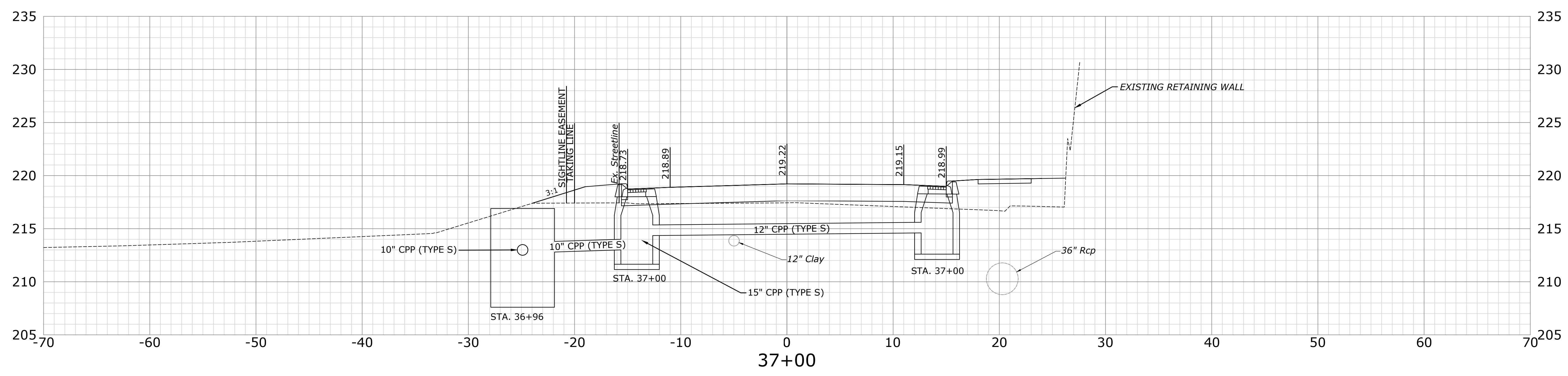
DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-21		
<b>81</b>		

SHEET 2 OF 2  
 PROJECT NO. 87-145  
 DATE: SEPTEMBER 7, 2018  
 DRAWN BY: MJJ  
 CHECKED BY: TDS  
 DESIGNED BY: DP



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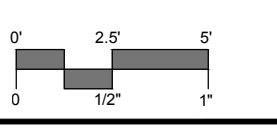
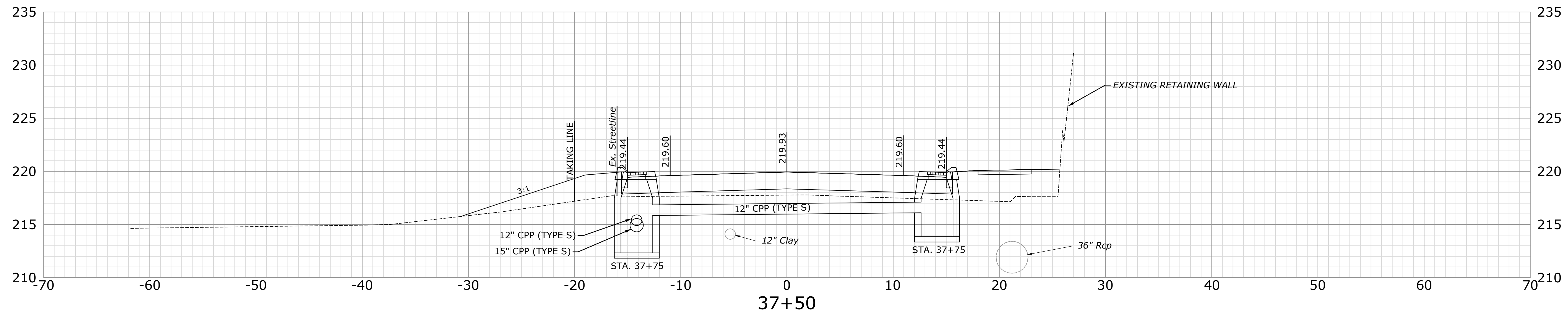
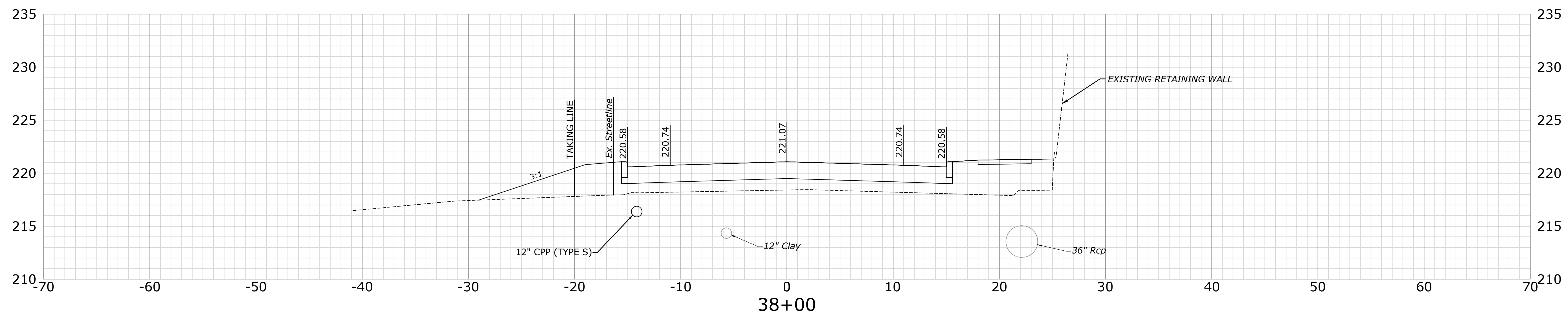
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=5'		
DATE: SEPTEMBER 7, 2018		
PROJECT NO.: 87-145		
SHEET NAME: XSC-22		

SHEET NO. **82**



SHEET 3 - 3. RECONSTRUCTION OF CROSS STREET, NAUGATUCK, CONNECTICUT



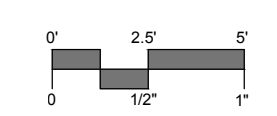
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DESCRIPTION	DATE	BY

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**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-23		
<b>83</b>		

SHEET 24 OF 24  
 PROJECT NO. 87-145  
 SHEET NAME XSC-24  
 DATE SEPTEMBER 7, 2018  
 DRAWN BY TDS  
 CHECKED BY MJJ  
 DESIGNED BY DP



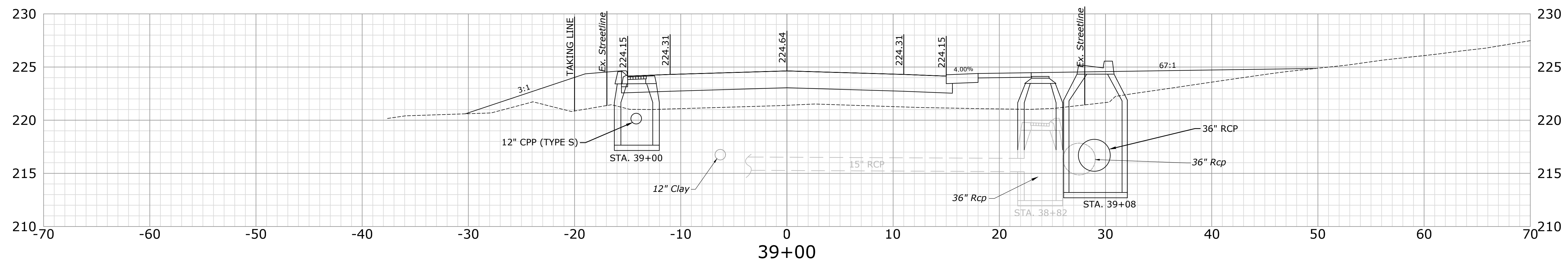
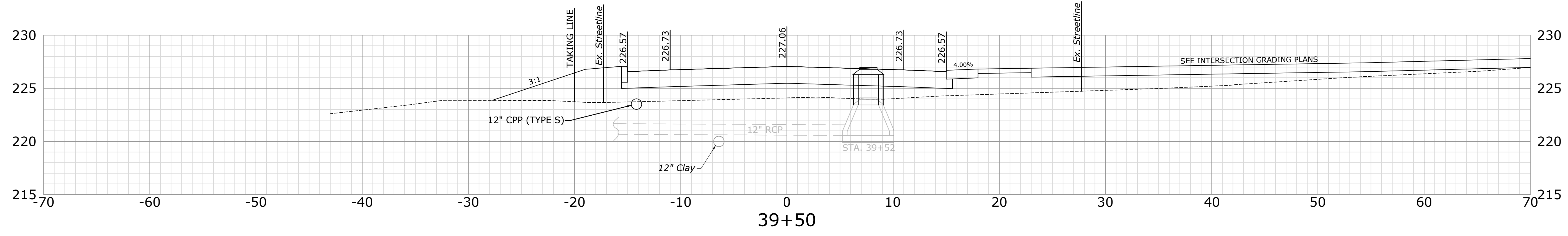
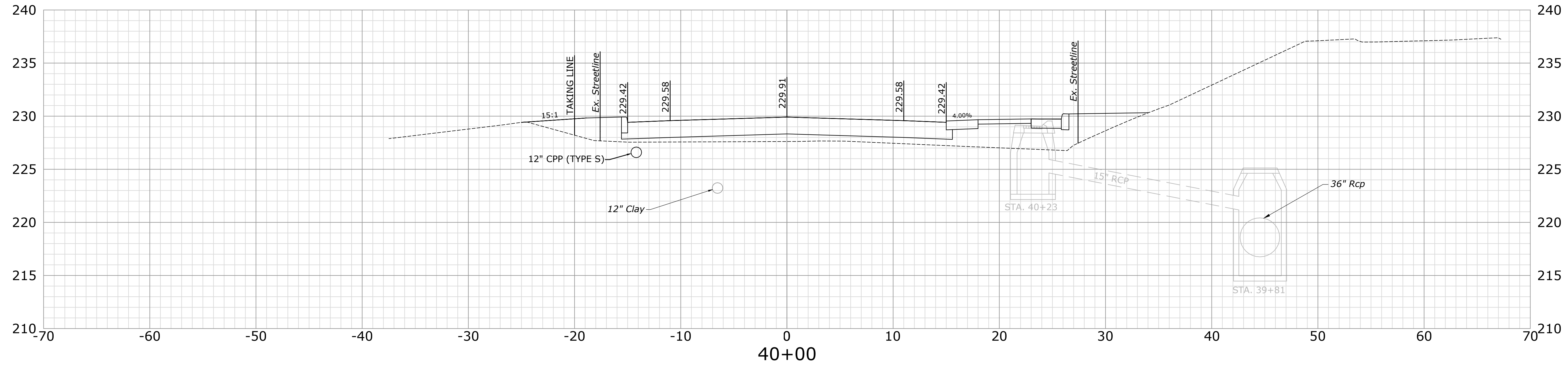
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DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

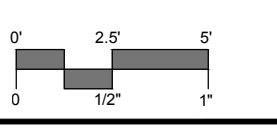
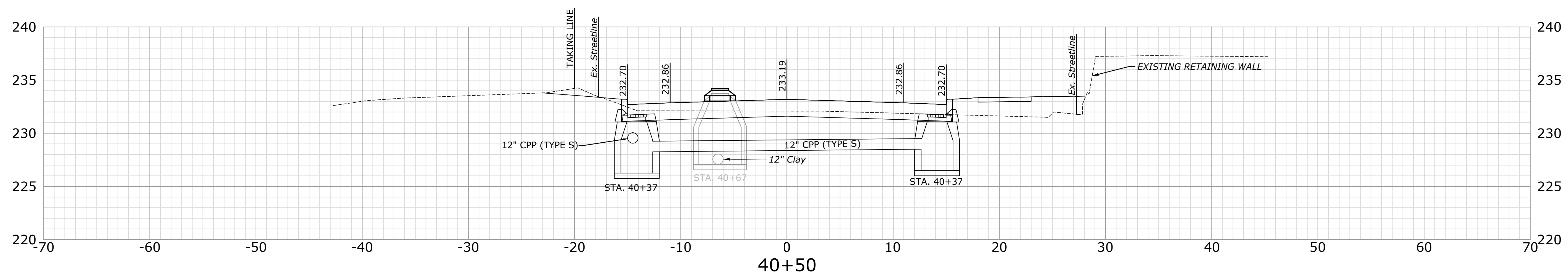
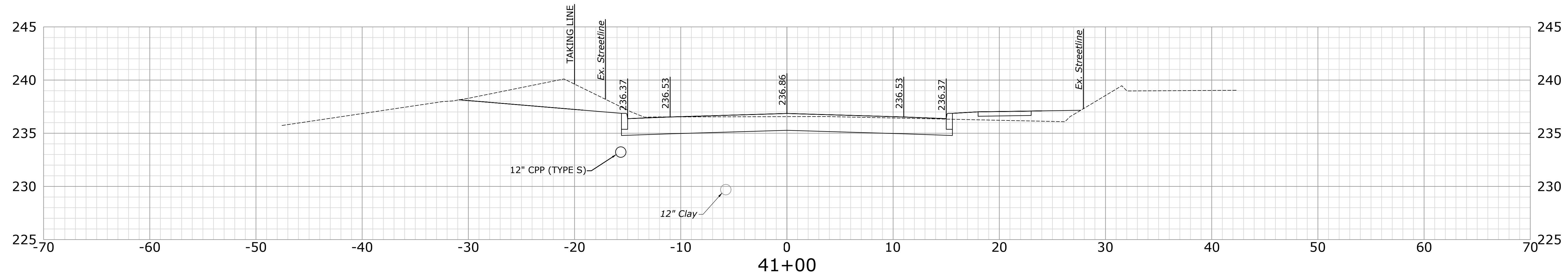
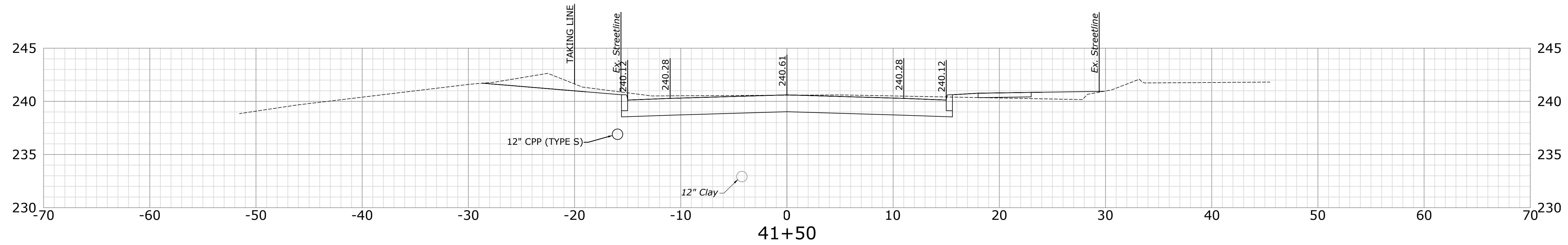
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SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-24		

84





SHEET 25 OF 25  
 PROJECT NO. 87-145  
 DATE: SEPTEMBER 7, 2018  
 DRAWN BY: MJD  
 CHECKED BY: MJD  
 DESIGNED BY: MJD



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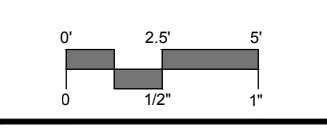
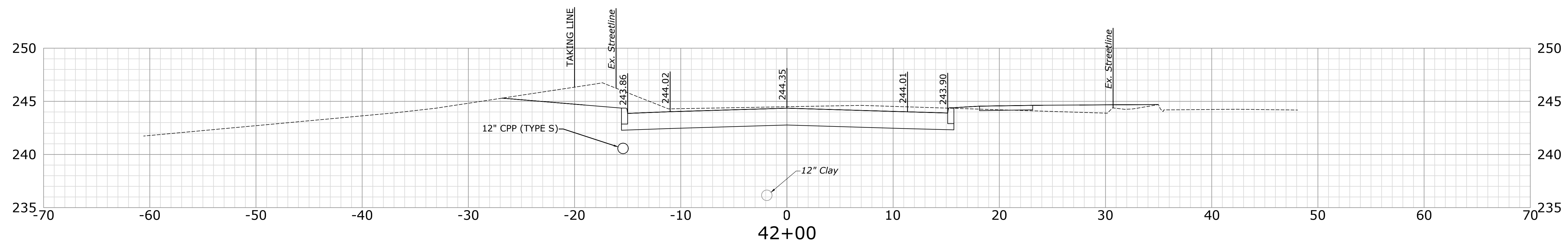
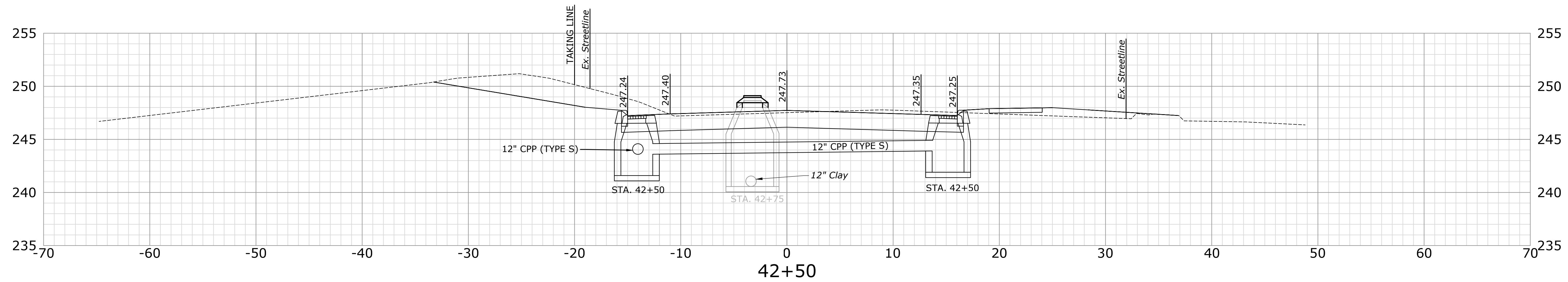
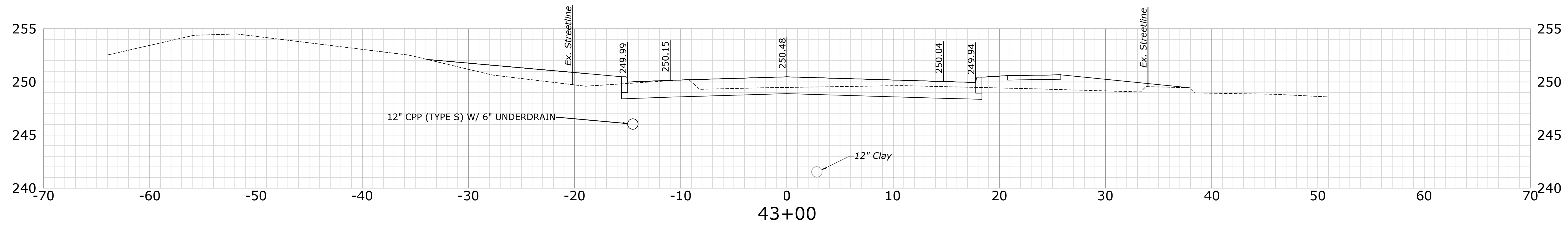
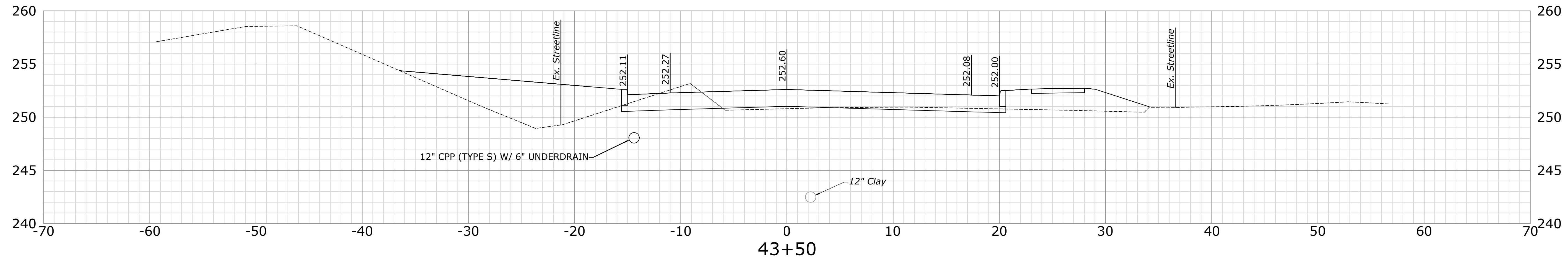
DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-25		

**85**

SHEET 26 - 26 - RECONSTRUCTION OF CROSS STREET, NAUGATUCK, CONNECTICUT  
 PROJECT NO. 87-145, SHEET NO. 86 OF 86  
 DATE: SEPTEMBER 7, 2018  
 DRAWN BY: M.J.J.



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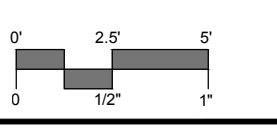
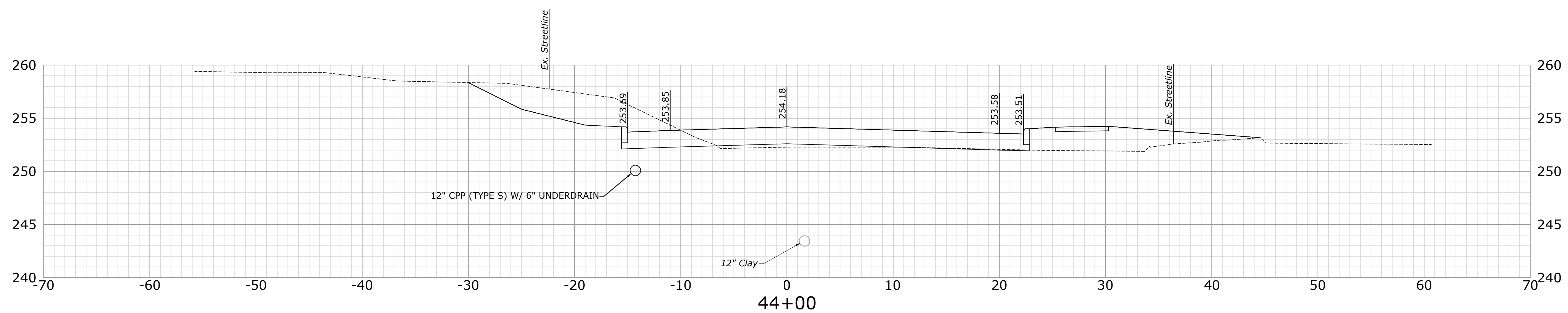
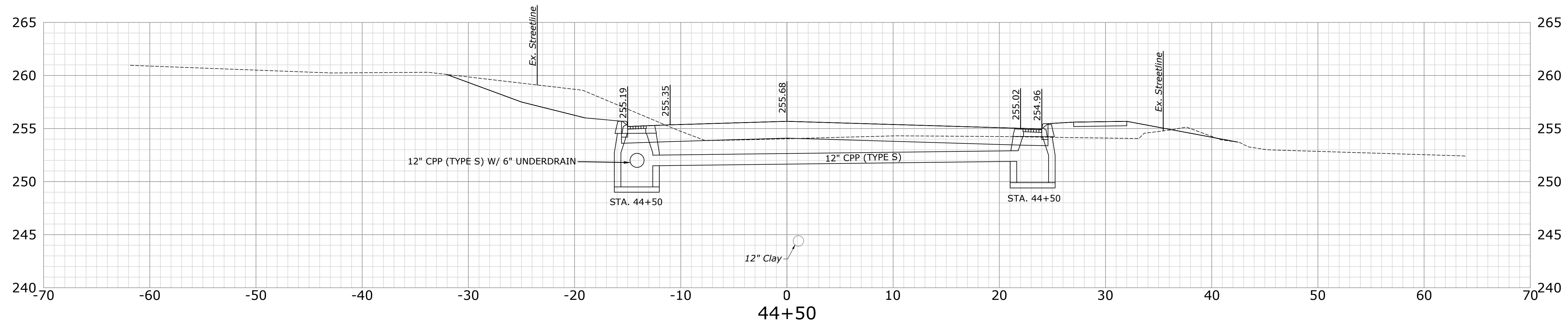
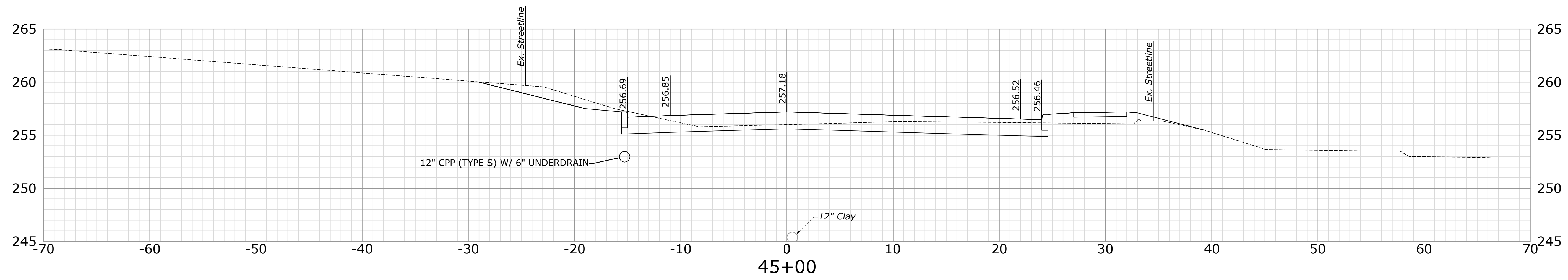
**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-26		

SHEET NO. **86**



SHEET 27 OF 27  
 PROJECT NO. 87-145  
 DATE: SEPTEMBER 7, 2018  
 DRAWN BY: M.J.J.  
 CHECKED BY: M.J.J.



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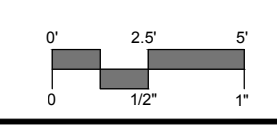
DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-27		

**87**

SHEET 28 OF 28  
 PROJECT NO. 87-145  
 SHEET NAME XSC-28  
 DATE SEPTEMBER 7, 2018  
 DRAWN BY TDS  
 CHECKED BY MJJ  
 DESIGNED BY DP



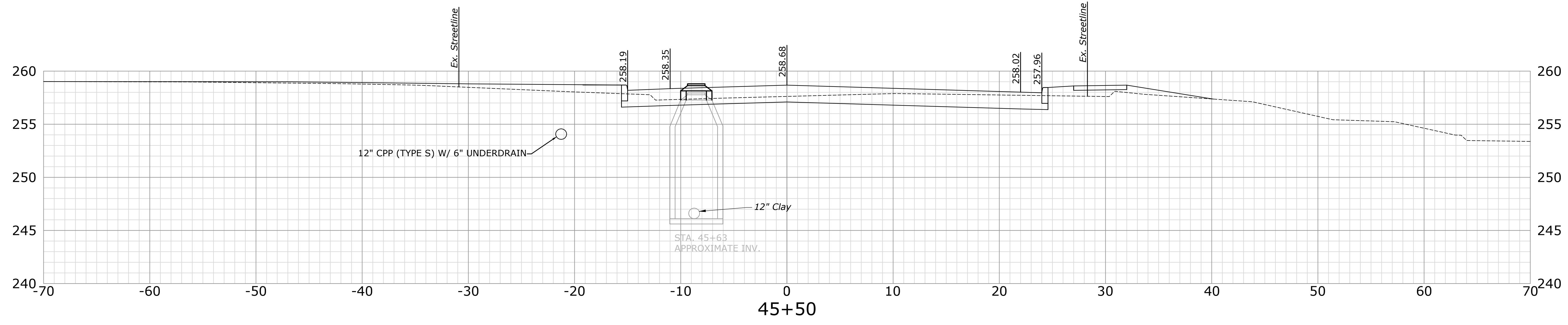
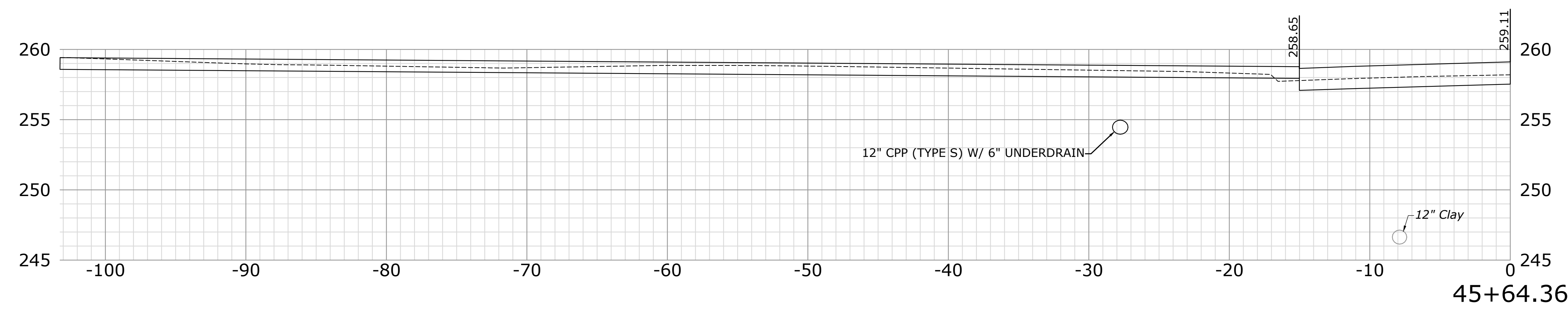
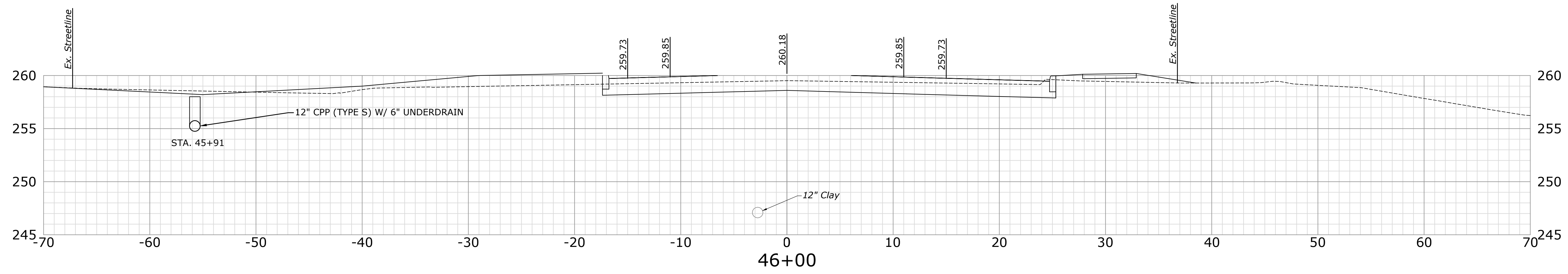
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DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
**NAUGATUCK, CONNECTICUT**

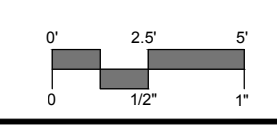
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DESIGNED	DRAWN	CHECKED
SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-28		

**88**





SHEET 29 OF 30  
 PROJECT NO. 87-145  
 SHEET NAME XSC-29  
 DATE SEPTEMBER 7, 2018  
 DRAWN BY MJD  
 CHECKED BY MJD  
 DESIGNED BY MJD



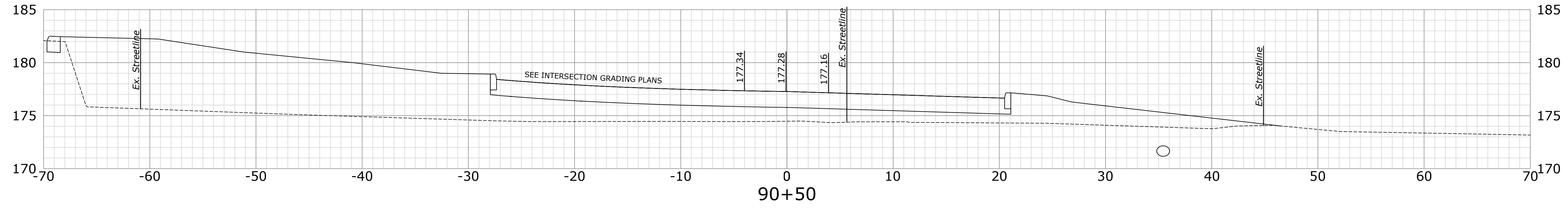
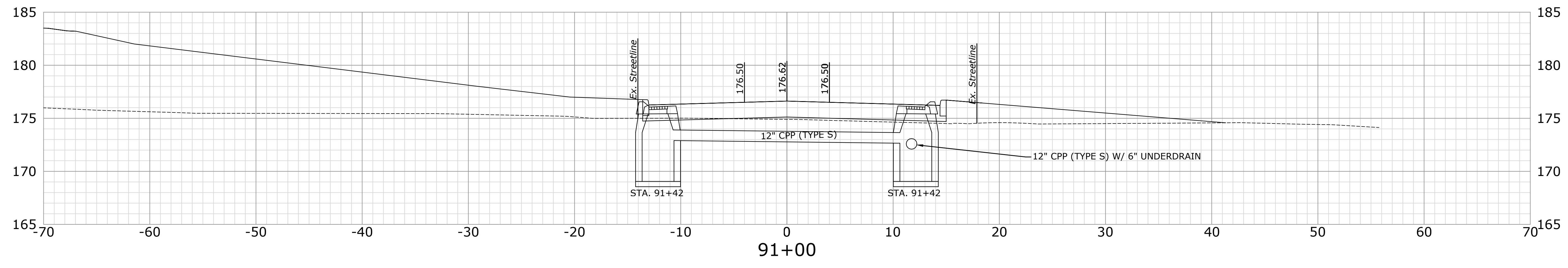
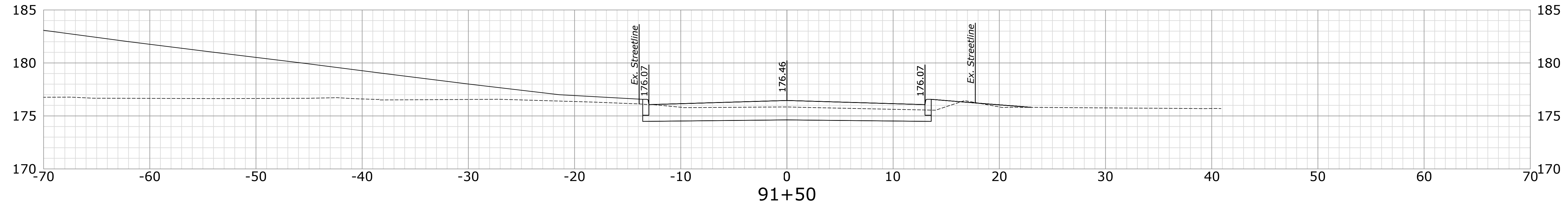
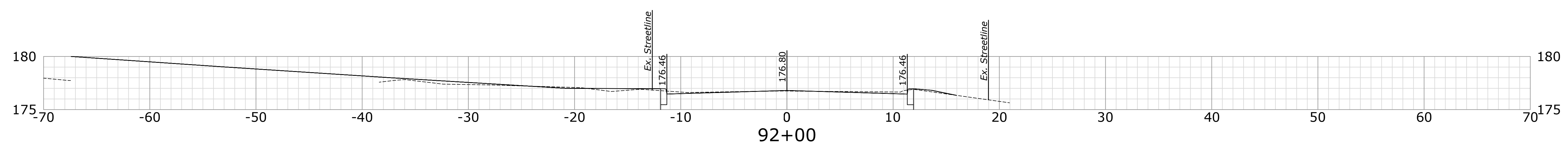
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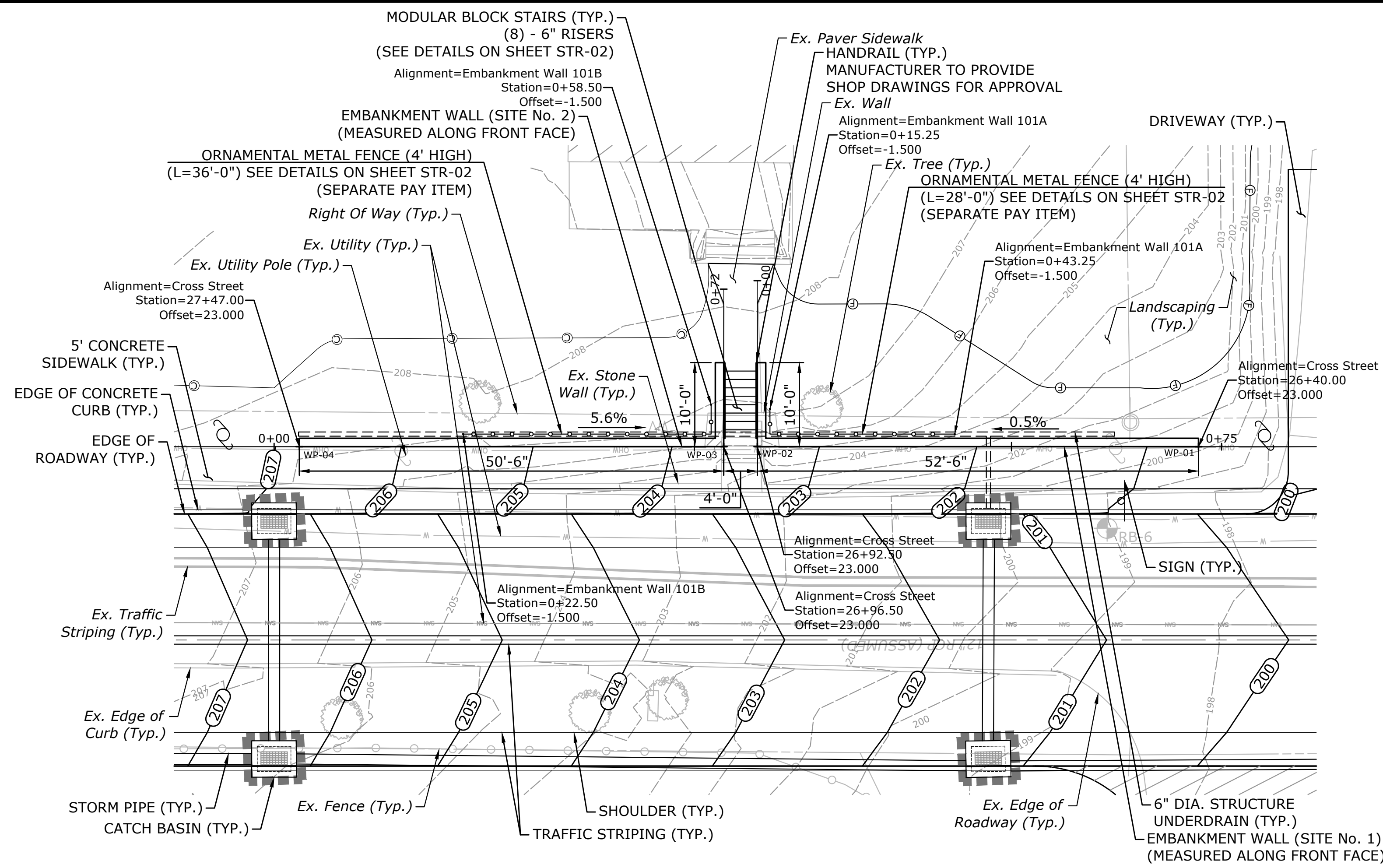
DESCRIPTION	DATE	BY

**CROSS SECTIONS**  
**RECONSTRUCTION OF CROSS STREET**  
 NAUGATUCK, CONNECTICUT

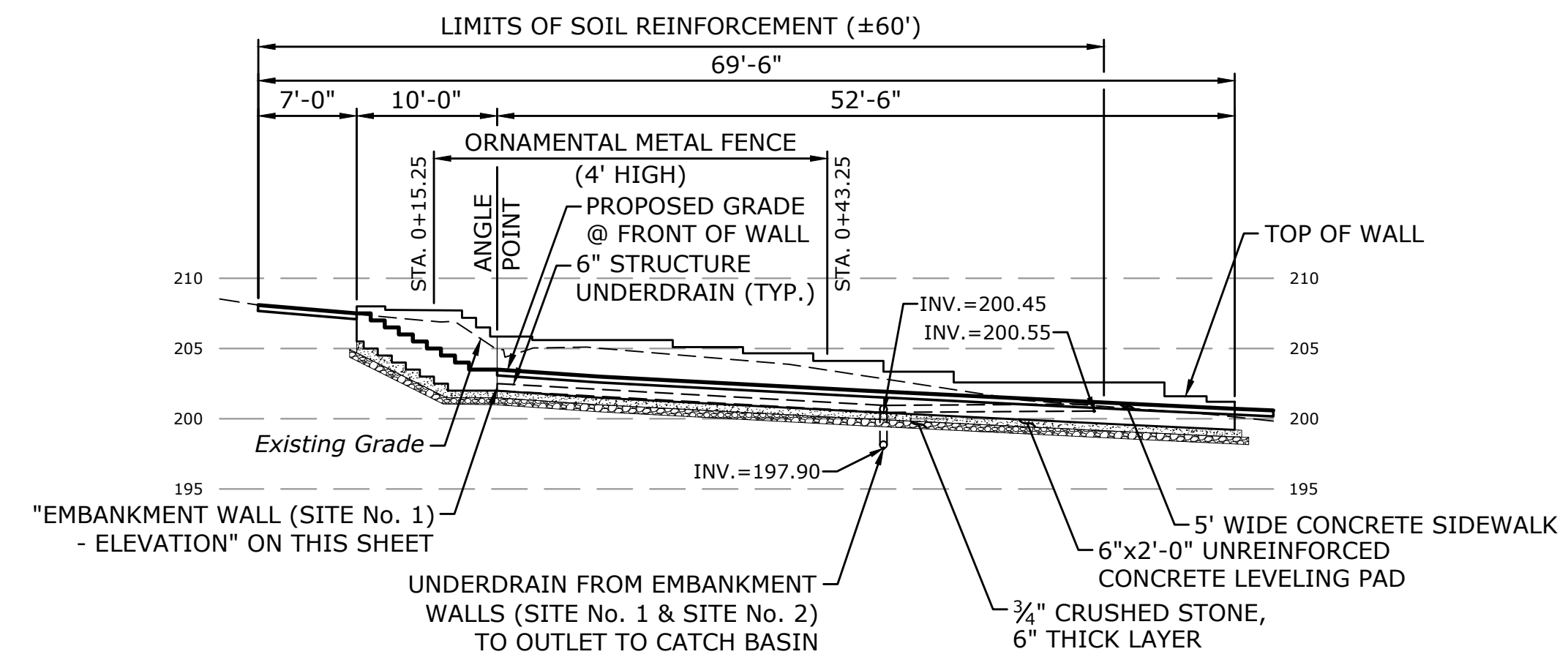
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SCALE 1"=5'		
DATE SEPTEMBER 7, 2018		
PROJECT NO. 87-145		
SHEET NAME XSC-29		

**89**  
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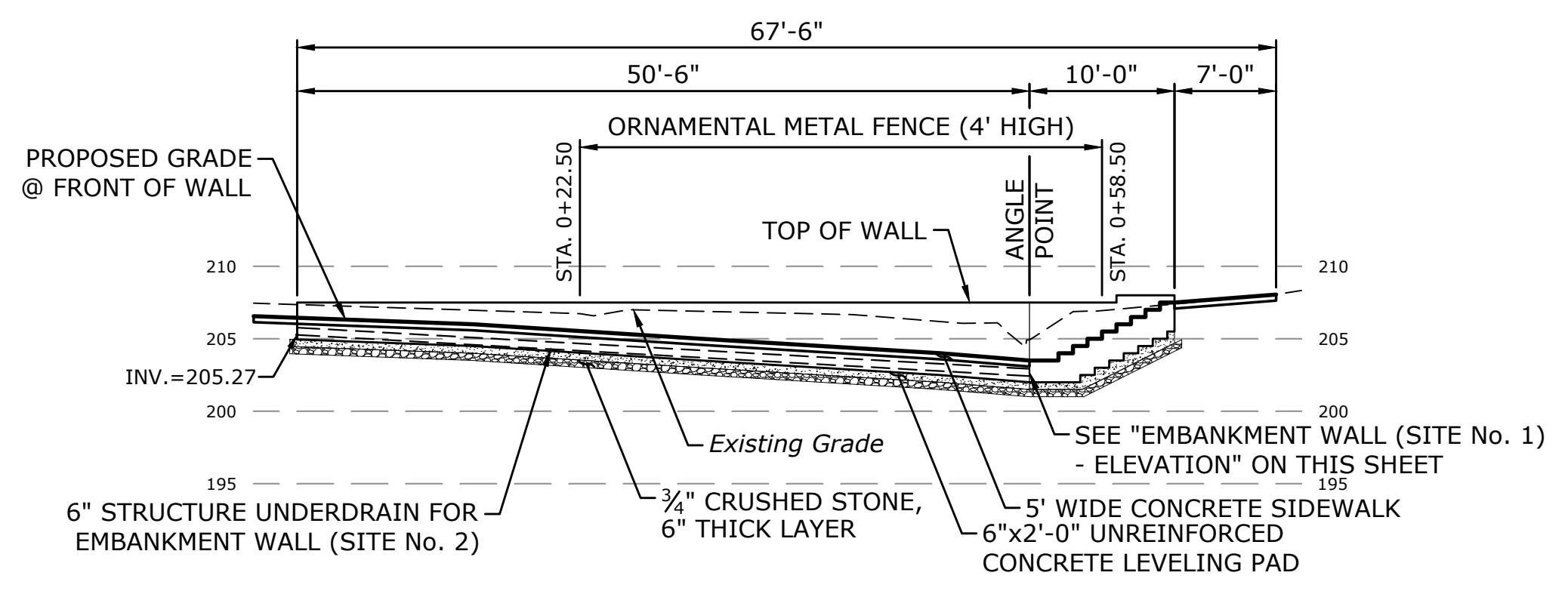




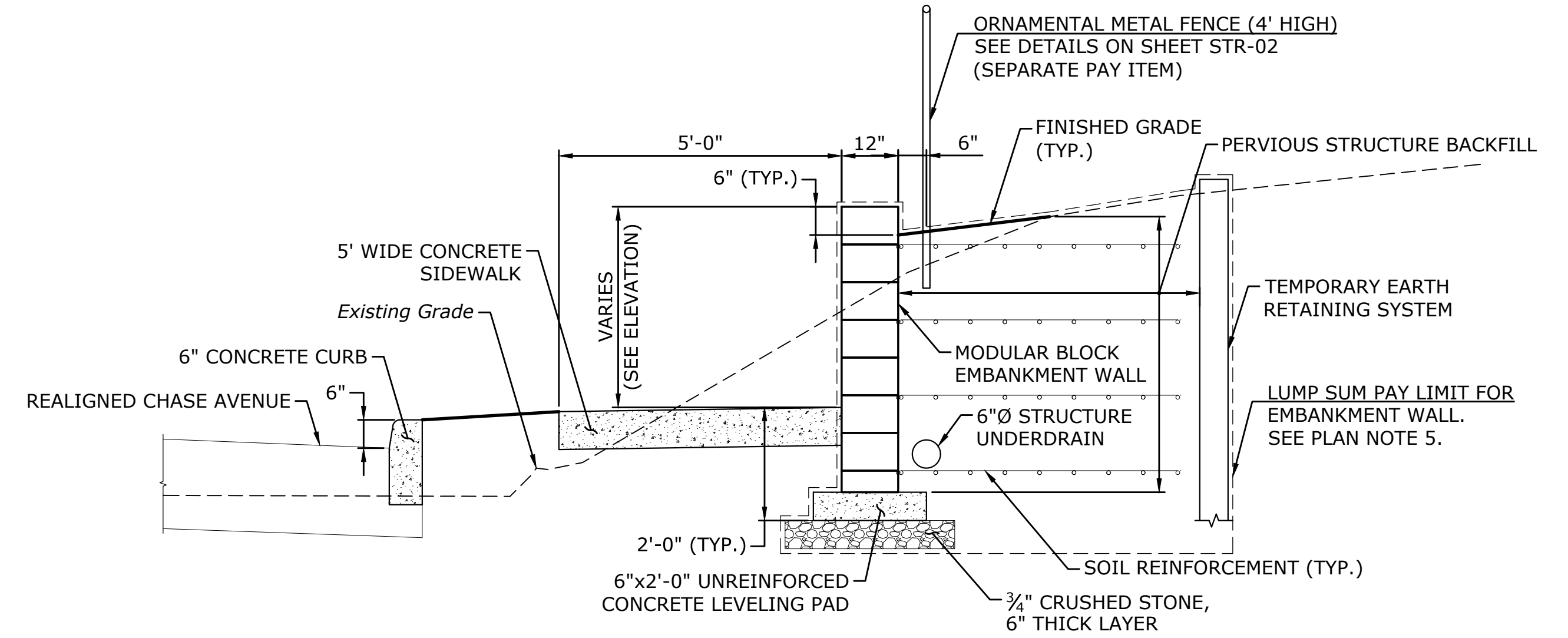
**EMBankment Wall - Plan**  
(SITE No. 1 & SITE No. 2)  
SCALE: 1"=10'-0"



**EMBankment Wall (Site No. 1) - Elevation**  
SCALE: 1"=10'-0"



**EMBankment Wall (Site No. 2) - Elevation**  
SCALE: 1"=10'-0"



**TYPICAL EMBANKMENT WALL SECTION**  
SCALE: 1/2"=1'-0"

**NOTES**

1. THE EMBANKMENT WALL SHALL BE DESIGNED, DETAILED AND CONSTRUCTED IN ACCORDANCE WITH THE SPECIAL PROVISION "EMBANKMENT WALL (SITE NO. 1/SITE NO. 2)".
2. TEMPORARY EARTH RETAINING SYSTEM BELOW PAY LIMITS AND ANY TIEBACKS AND BRACING ASSOCIATED WITH THE SHEET PILING SHALL BE INCLUDED IN THE LUMP SUM COST OF THE WALL.
3. THE CONTRACTOR SHALL SELECT, DESIGN (FOR PROPRIETARY WALLS ONLY) AND CONSTRUCT ONE OF THE EMBANKMENT WALL OPTIONS AS LISTED IN THE SPECIAL PROVISION "EMBANKMENT WALL (SITE NO. 1/SITE NO. 2)".
4. THE COLOR OF THE DRY CAST BLOCK SHALL BE COORDINATED AND APPROVED BY THE BOROUGH OF NAUGATUCK.
5. ANY ADDITIONAL PERVIOUS STRUCTURE BACKFILL REQUIRED OUTSIDE THIS LIMIT SHALL ALSO BE INCLUDED IN THE LUMP SUM PRICE.
6. ALL ITEMS AND WORK WITHIN PAY LIMITS SHOWN SHALL BE PAID FOR UNDER "EMBANKMENT WALL (SITE No. 1)" & "EMBANKMENT WALL (SITE No. 2)", INCLUDING BUT NOT LIMITED TO WALL, BACKFILL, LEVELING PAD, UNDERDRAIN, REMOVAL OF TREES, REMOVAL OF EXISTING STONE WALL, REMOVAL OF EXISTING STEPS, ASSOCIATED EXCAVATION AND GRADING NECESSARY FOR SUPPORT OF THE EMBANKMENT WALL TO THE SATISFACTION OF THE ENGINEER. TOPSOIL AND TURF ESTABLISHMENT TO BE PAID SEPARATELY.
7. EMBANKMENT WALL SHALL BE FURNISHED WITH UNDERDRAIN AND OR WEEPHOLES PER MANUFACTURER OF WALL, PAYMENT SHALL BE INCLUDED IN ITEM "EMBANKMENT WALL (SITE No. 1)" & "EMBANKMENT WALL (SITE No. 2)".
8. MODULAR BLOCK STAIRWAY TO BE PAID FOR UNDER ITEM "EMBANKMENT WALL (SITE No. 1)".

**TABLE OF COORDINATES**

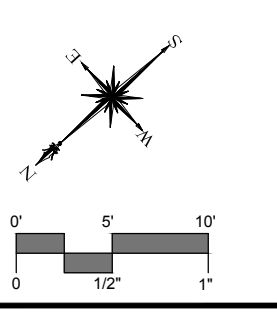
WP #	DESCRIPTION	NORTHING	EASTING
1	WP-01	732170.0041	918778.2904
2	WP-02	732208.8786	918813.5720
3	WP-03	732211.8406	918816.2603
4	WP-04	732249.2374	918850.2007

**RETAINING WALL QUANTITIES**

DESCRIPTION	UNITS	QUANTITY
EMBANKMENT WALL (SITE No. 1)	L.S.	1
EMBANKMENT WALL (SITE No. 2)	L.S.	1
ORNAMENTAL METAL FENCE (4' HIGH)	L.F.	64

**LEGEND**

- ⊕ B-1 BORING LOCATION
- + WP-01 WORKING POINT



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BY \_\_\_\_\_  
DATE \_\_\_\_\_  
DESCRIPTION \_\_\_\_\_

STRUCTURAL PLANS - EMBANKMENT WALL 101  
RECONSTRUCTION OF CROSS STREET

NAUGATUCK, CONNECTICUT

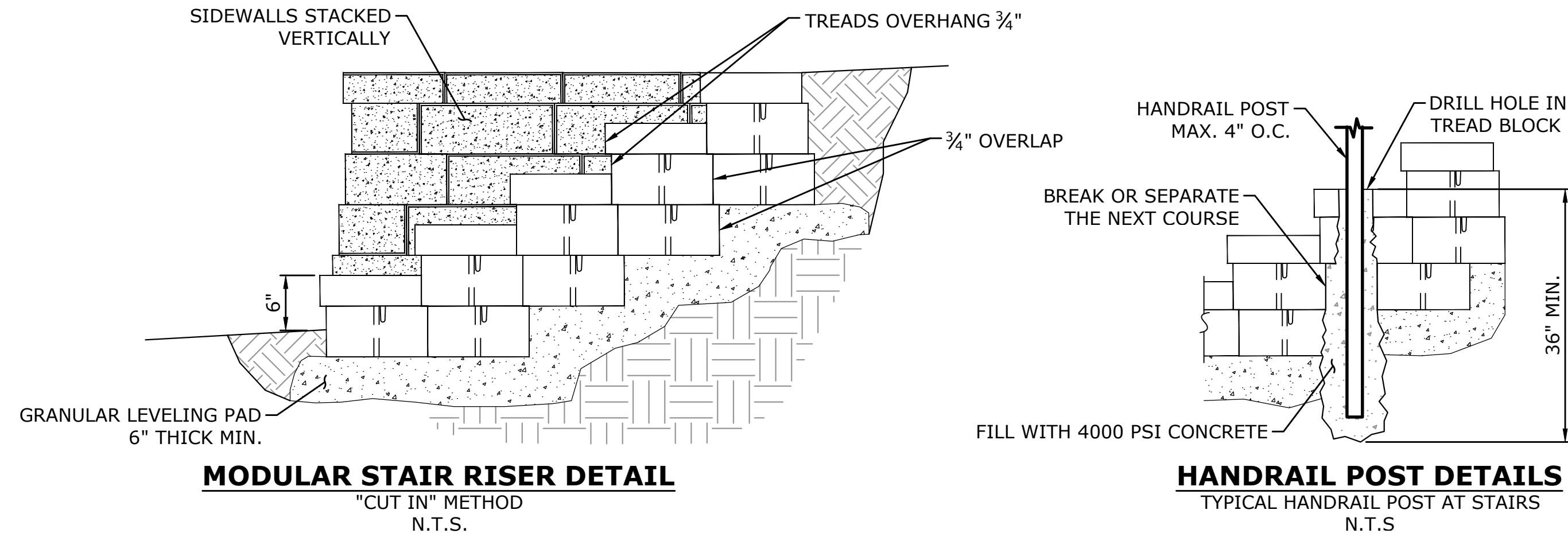
KP	NP	KP
DESIGNED	DRAWN	CHECKED
AS NOTED		
SEPTEMBER 7, 2018		
DATE		
PROJECT NO. 87-145		
DRAWING NO. STR-01		

**90**



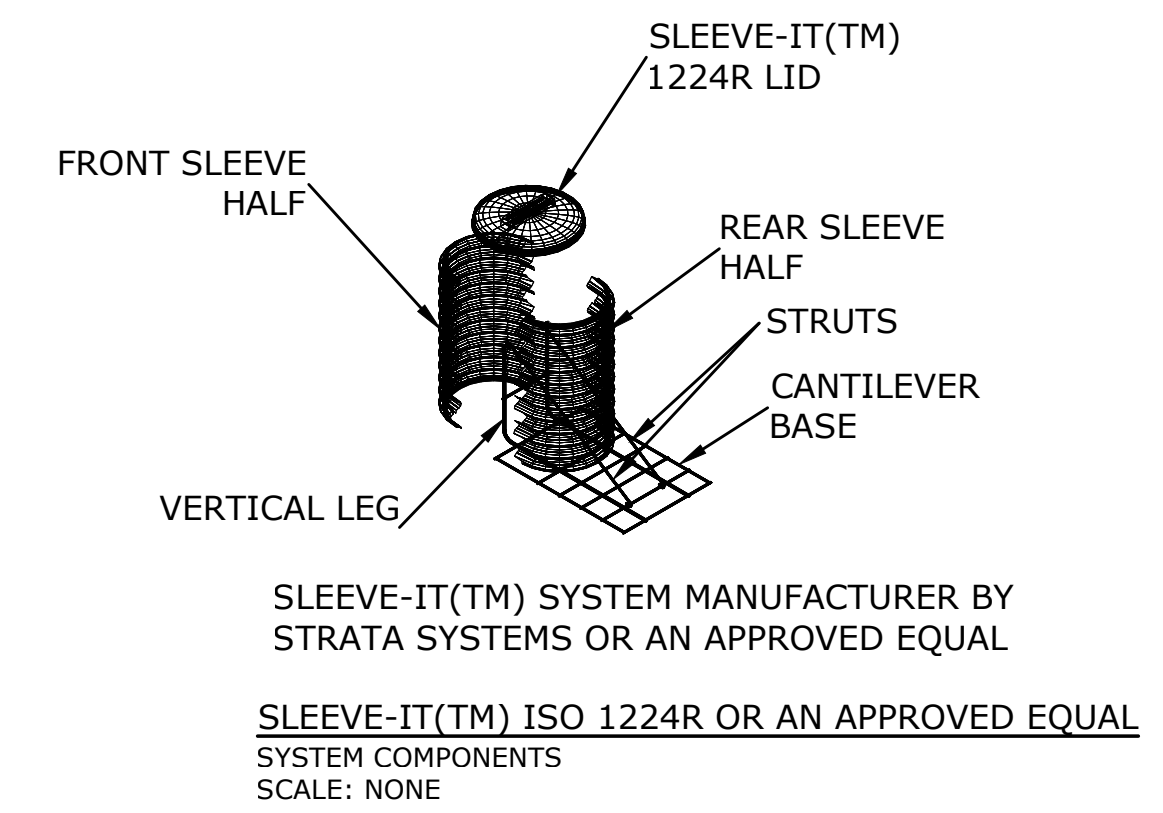
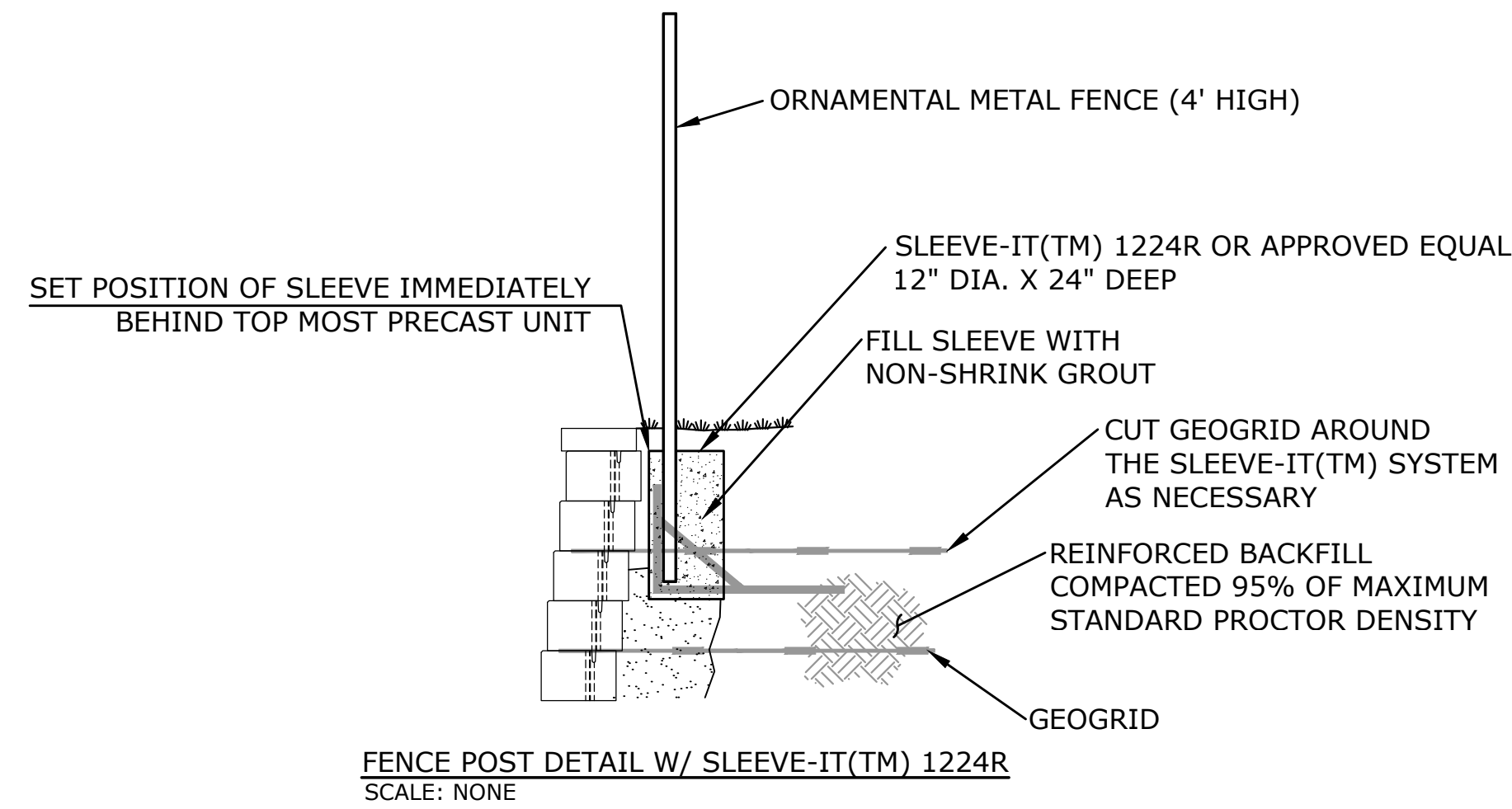
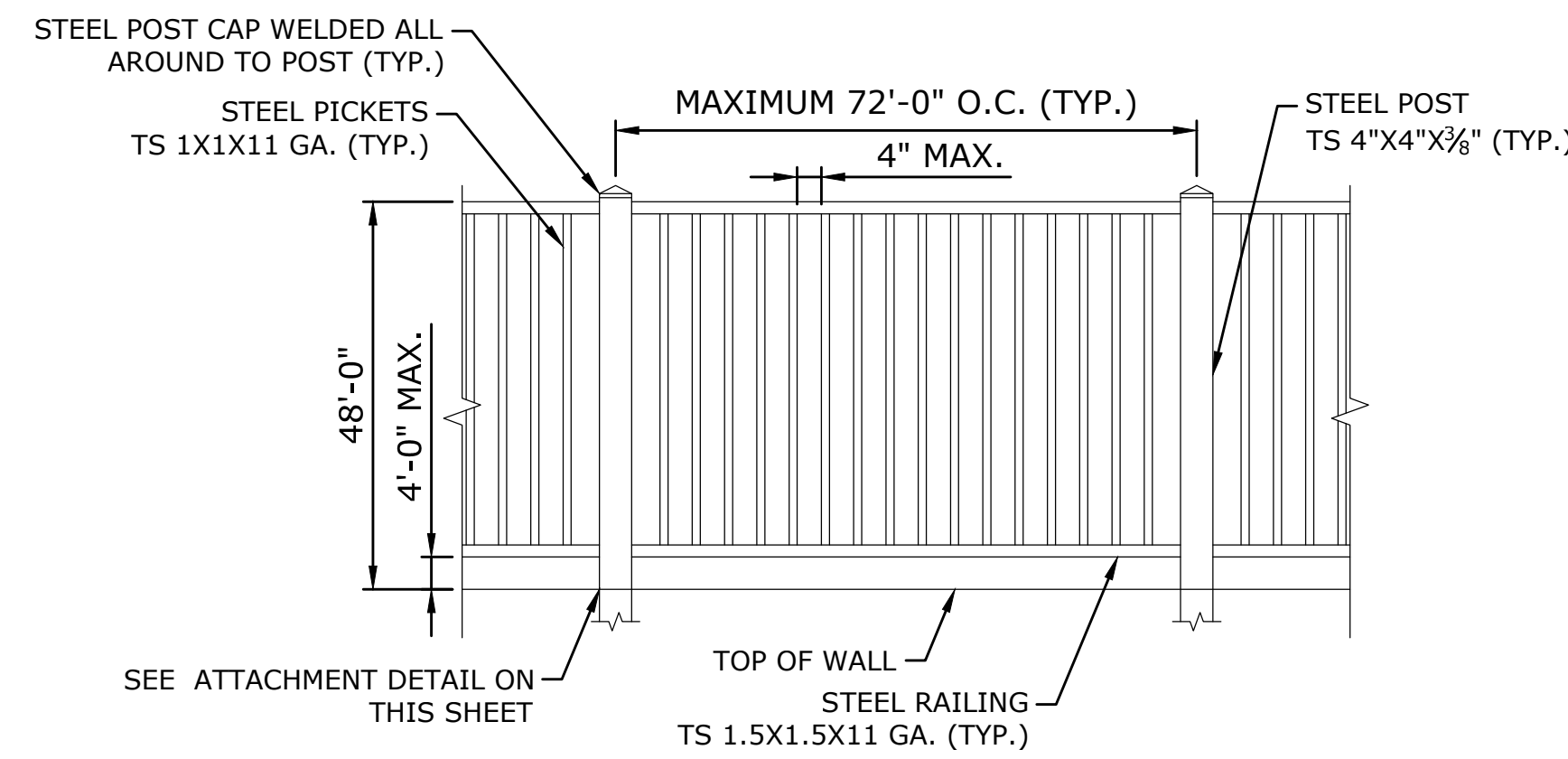
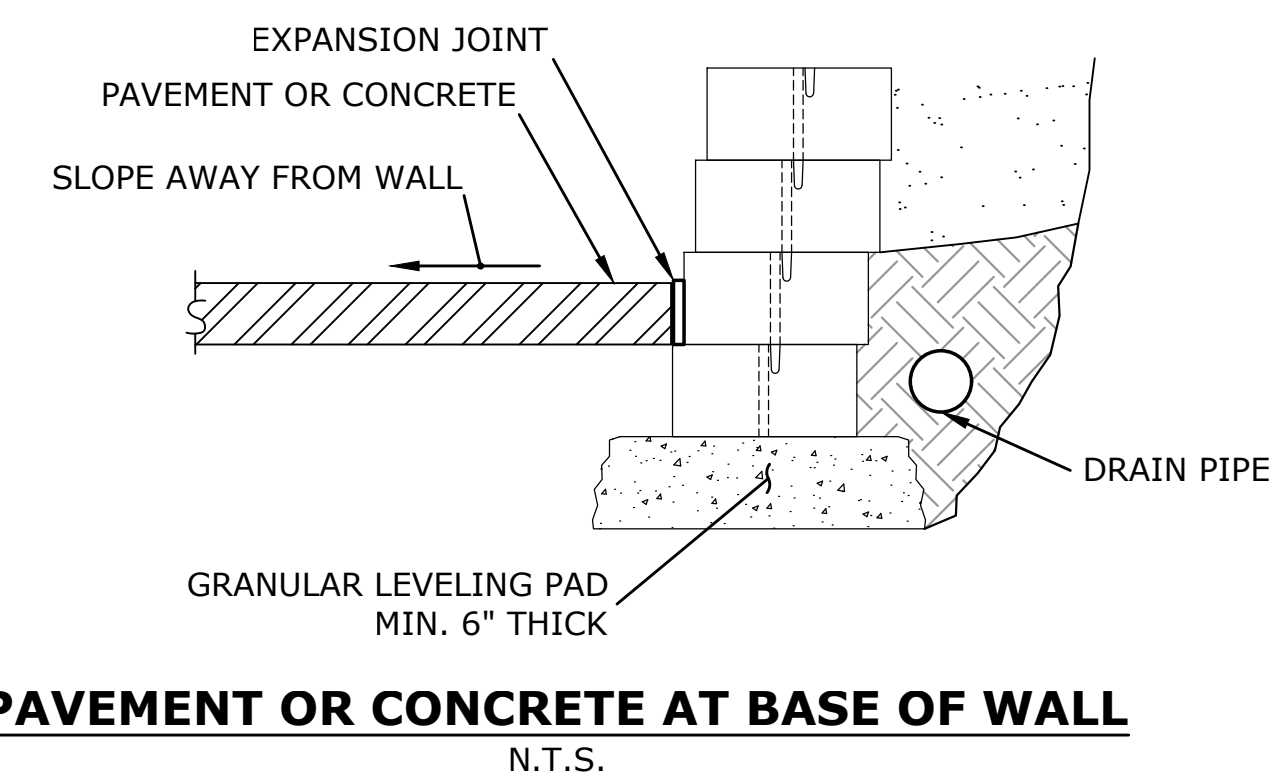
**NOTE**

TYPICAL MODULAR BLOCK DETAIL SHOWN.  
FACE OF BOTTOM STAIR OFFSET 2' BEHIND FACE OF EMBANKMENT WALL.  
SEE "TYPICAL STAIRS & HANDRAIL SECTION" ON THIS SHEET.

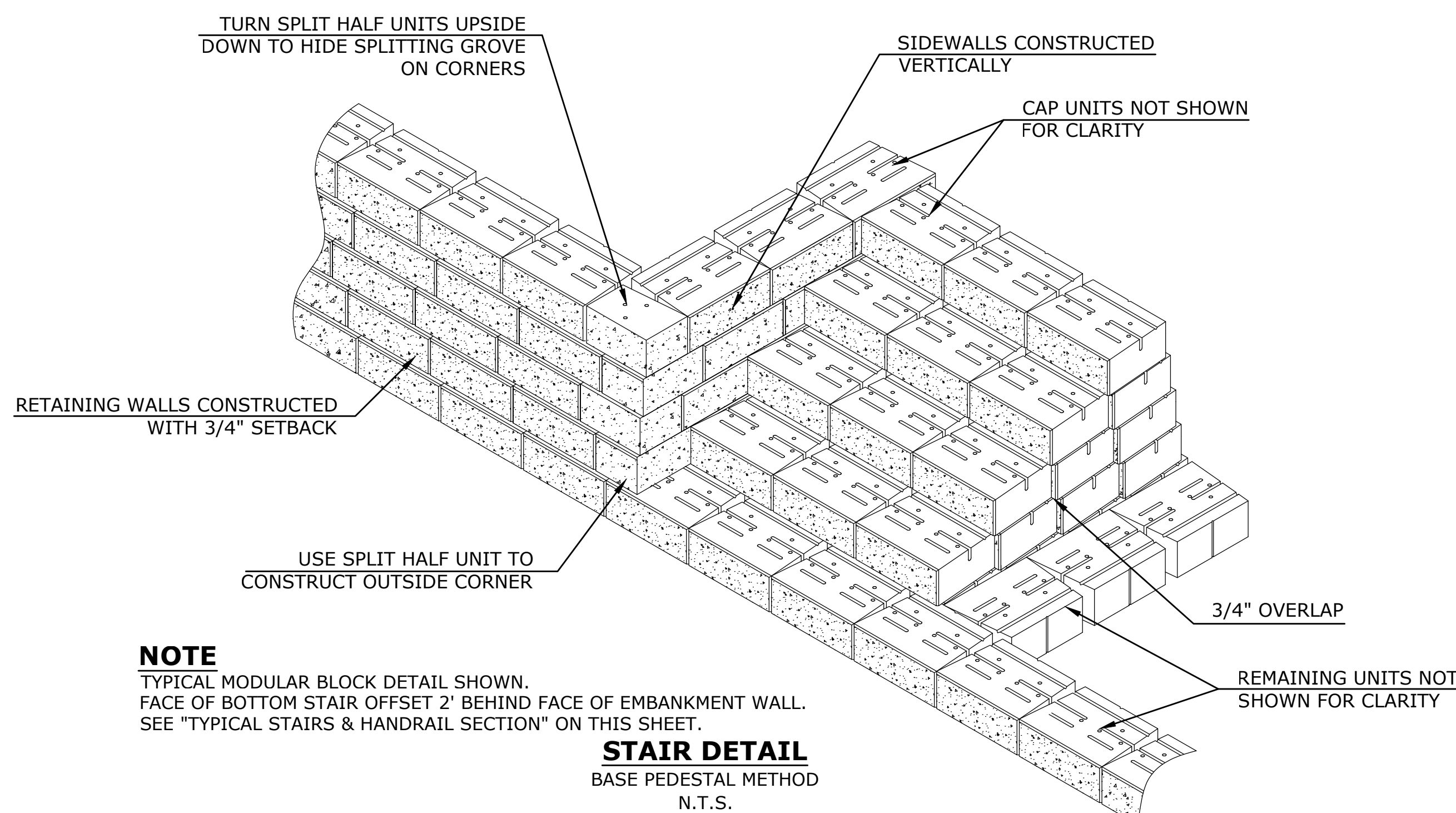
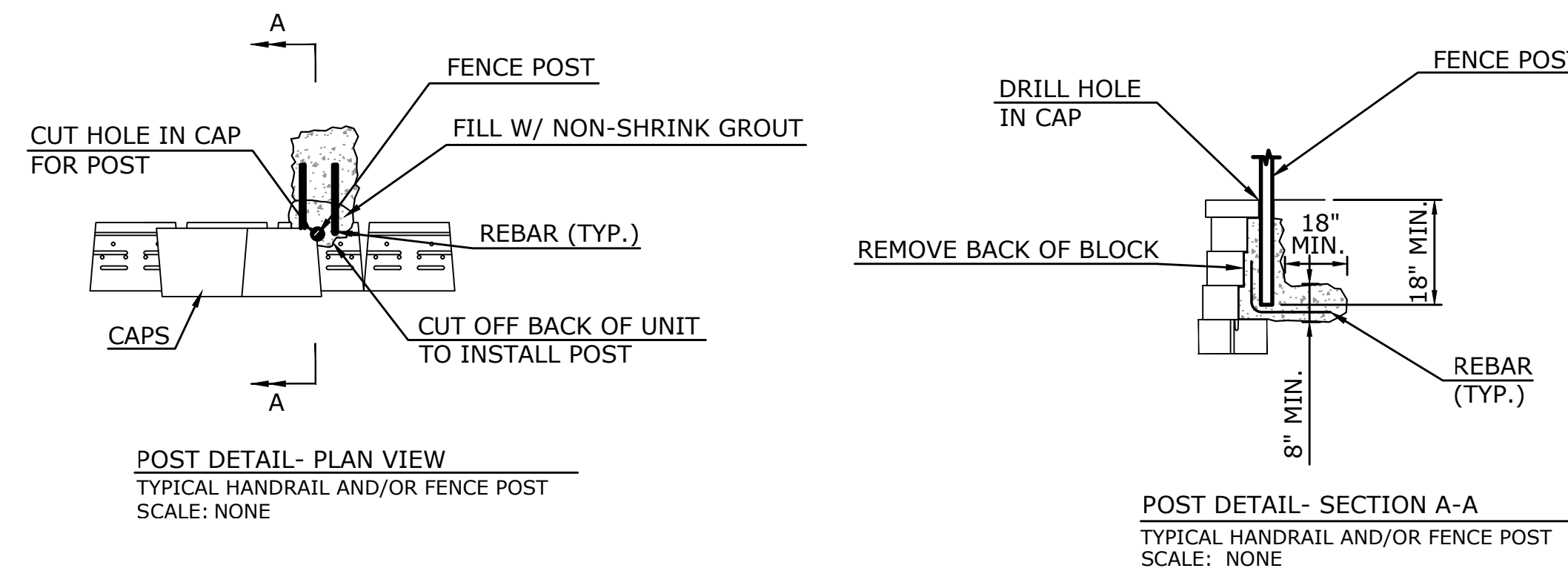


**STAIR NOTES:**

- USE SAME LEVELING PAD MATERIAL FOR STAIR PEDESTAL AS RETAINING WALL
- CONSTRUCT STAIR RISERS FIRST, INSTALL CAPS FOR TREADS SECOND, AND THEN INSTALL SIDEWALLS LAST
- DO NOT PIN STAIR UNITS
- CONSTRUCT BASE PEDESTAL IN 4 TO 5 RISER INCREMENTS
- STACK SIDEWALLS VERTICALLY, WITHOUT PINNING



\*FENCING SYSTEMS APPROVED FOR USE WITH SLEEVE-IT(TM) 1224R ARE LIMITED TO THE FOLLOWING HEIGHTS: CHAIN LINK-UP TO 8-FT, PRIVACY-UP TO 6-FT (WOODEN, PVC, METAL). POST SIZE 4"x4" MAX.



**NOTE**

1. CONTRACTOR SHALL CONFIRM ALL ELEVATIONS AND DIMENSIONS OF GRANITE TREADS PRIOR TO MANUFACTURING.
2. MANUFACTURER TO PROVIDE SHOP DRAWINGS FOR REVIEW AND APPROVAL BY ENGINEER.
3. RAILING AND RAILING POSTS TO BE GALVANIZED STEEL AND PAINTED "BLACK".

DESCRIPTION	DATE	BY

KP	NP	KP
DESIGNED	DRAWN	CHECKED
AS NOTED		
SEPTEMBER 7, 2018		
DATE		
PROJECT NO. 87-145		
DRAWING NO. STR-02		

**BEGIN FULL-DEPTH CONSTRUCTION**  
STATION 10+12.57  
MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
STATION 50+97.60  
MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
STATION 70+65  
MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
STATION 50+06.96  
MATCH EXISTING

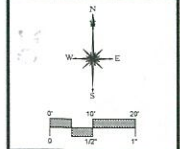
**LIMIT OF CONSTRUCTION**  
STATION 60+71  
MATCH EXISTING

**LIMIT OF CONSTRUCTION**  
STATION 80+31  
MATCH EXISTING

*No WORK REQUIRED*

**PROPOSED LEGEND**

- CRUSHED STONE
- CONCRETE SIDEWALK
- CONCRETE SIDEWALK - 8" THICK
- BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
- BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
- CUT BITUMINOUS CONCRETE PAVEMENT
- APPROX. CUT/FILL LIMIT
- SEDIMENTATION CONTROL SYSTEM
- PROPOSED TAKING LINE
- PROPOSED EASEMENT LINE
- PROPOSED STREETLINE
- PROPOSED UTILITY POLE W/ STREET LIGHT
- SHADE TREE
- ORNAMENTAL TREE



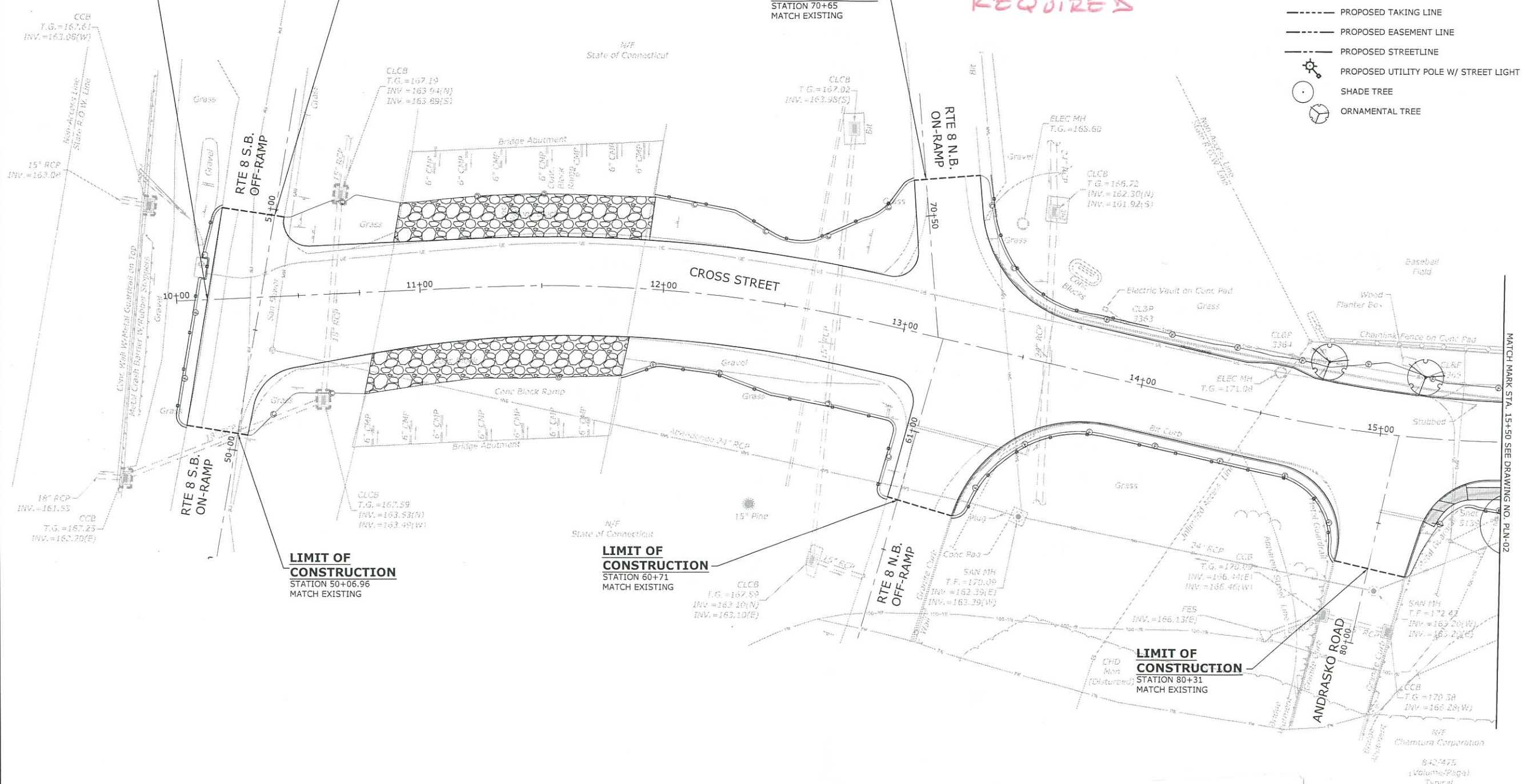
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DATE	BY

ROADWAY PLAN - FRONTIER  
RECONSTRUCTION OF CROSS STREET  
**FRONTIER POLE RELOCATION PLAN**  
NAUGATUCK, CONNECTICUT















DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE JULY 5, 2017		
PROJECT NO. 87-145		
DRAWING NO. PLN-01		
SHEET NO. <b>POLE 1</b>		

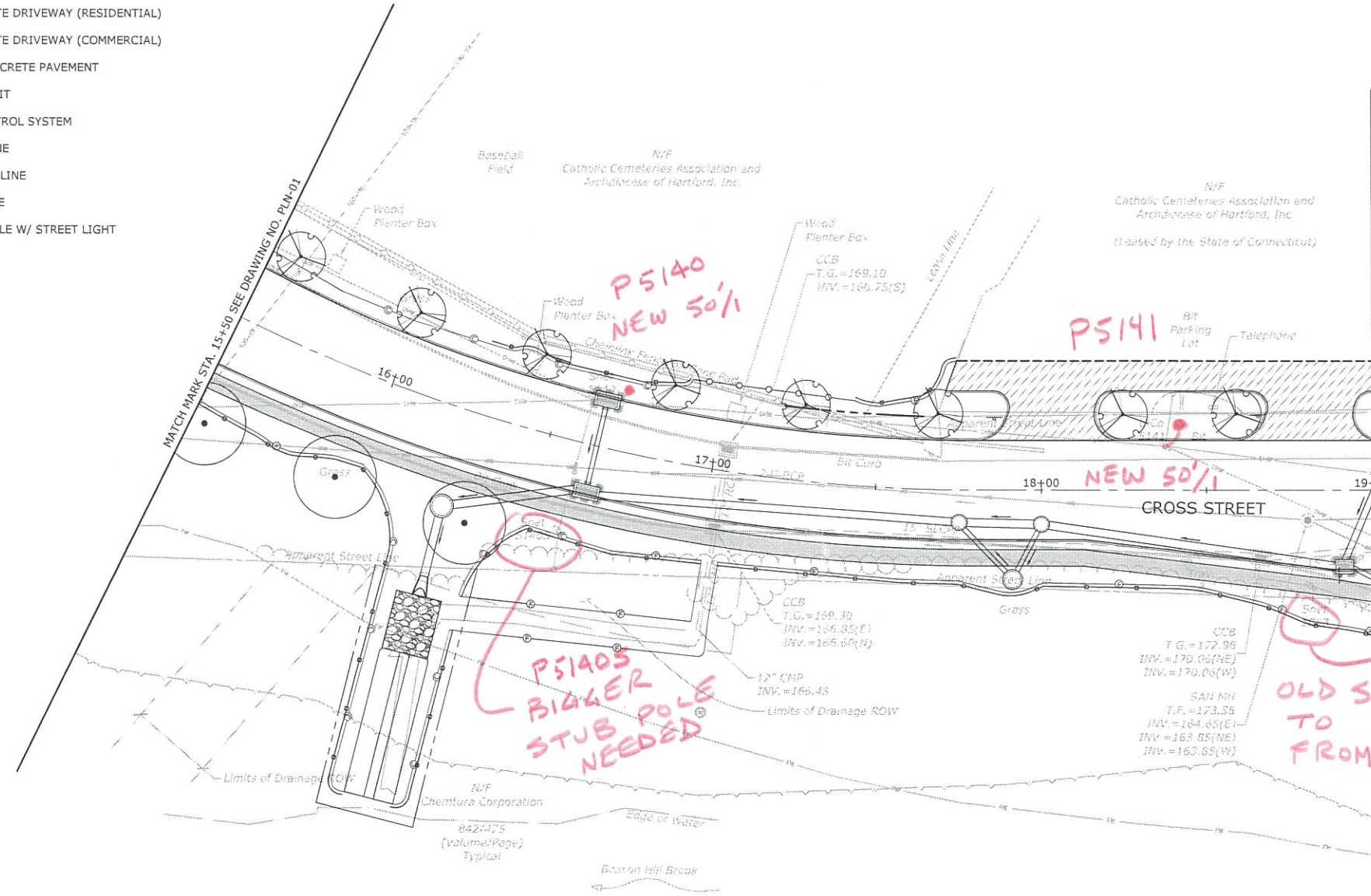
"For Information only"





**PROPOSED LEGEND**

-  CRUSHED STONE
-  CONCRETE SIDEWALK
-  CONCRETE SIDEWALK - 8" THICK
-  BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
-  BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
-  CUT BITUMINOUS CONCRETE PAVEMENT
-  APPROX. CUT/FILL LIMIT
-  SEDIMENTATION CONTROL SYSTEM
-  PROPOSED TAKING LINE
-  PROPOSED EASEMENT LINE
-  PROPOSED STREETLINE
-  PROPOSED UTILITY POLE W/ STREET LIGHT
-  SHADE TREE
-  ORNAMENTAL TREE



P5140  
NEW 50'!

P5141

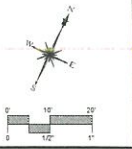
NEW 50'!

P5140S  
BIGGER  
STUB POLE  
NEEDED

OLD SERVICE  
TO PUMP HOUSE  
FROM P1093 NOT REQUIRED  
REMOVE POLE

ON ALL 45' POLE ES BUYING TOP 5'  
ON ALL 50' POLE ES BUYING TOP 10'

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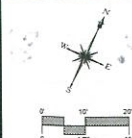
DESCRIPTION	DATE	BY

ROADWAY PLAN - FRONTIER  
RECONSTRUCTION OF CROSS STREET  
FRONTIER POLE RELOCATION PLAN  
NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=20'		
DATE JULY 5, 2017		
PROJECT NO. 87-145		
DRAWING NO. PLN-02		
<b>POLE 2</b>		
SHEET NO.		



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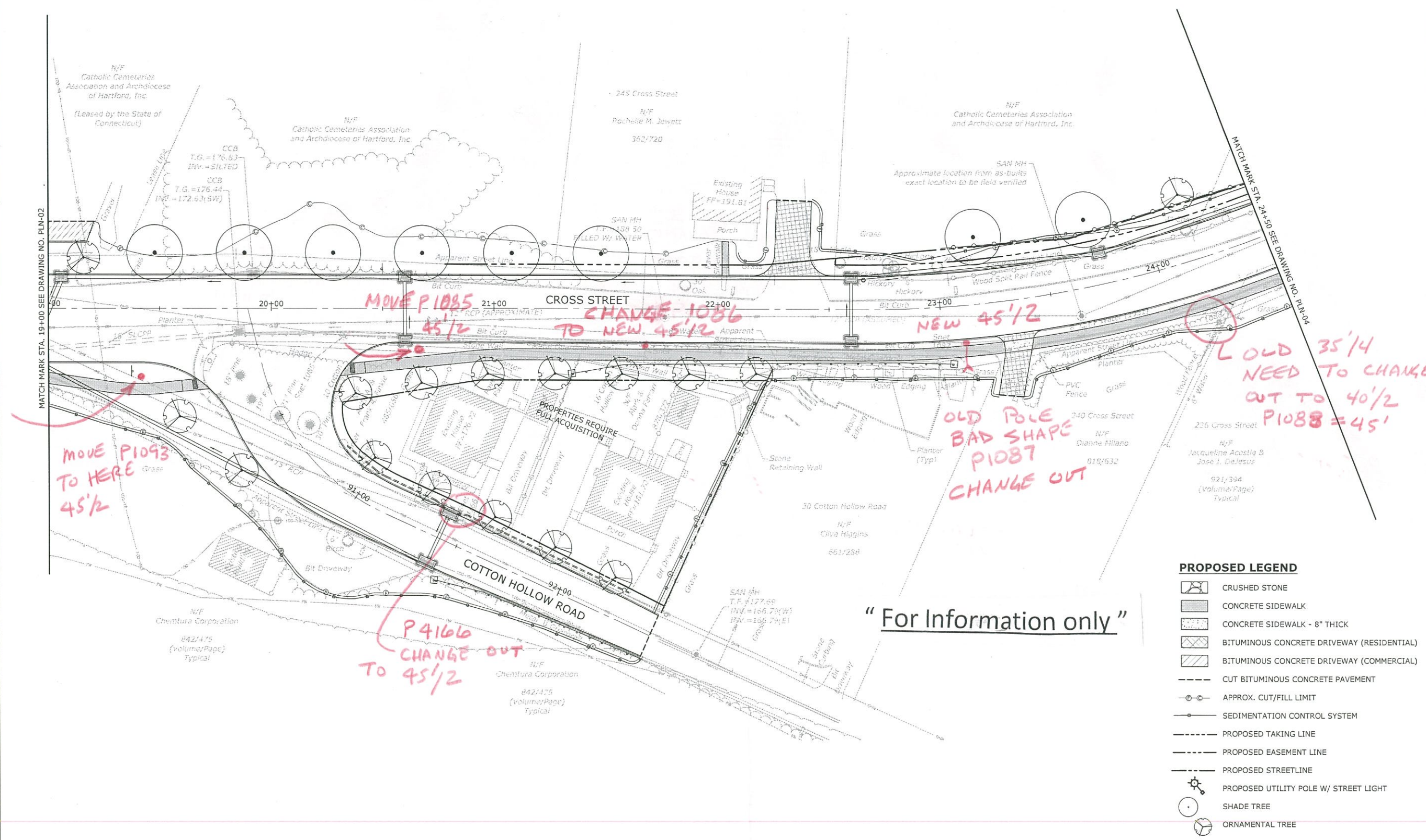


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DESCRIPTION	DATE	BY

ROADWAY PLAN - FRONTIER  
 RECONSTRUCTION OF CROSS STREET  
 FRONTIER POLE RELOCATION PLAN  
 NAUGATUCK, CONNECTICUT

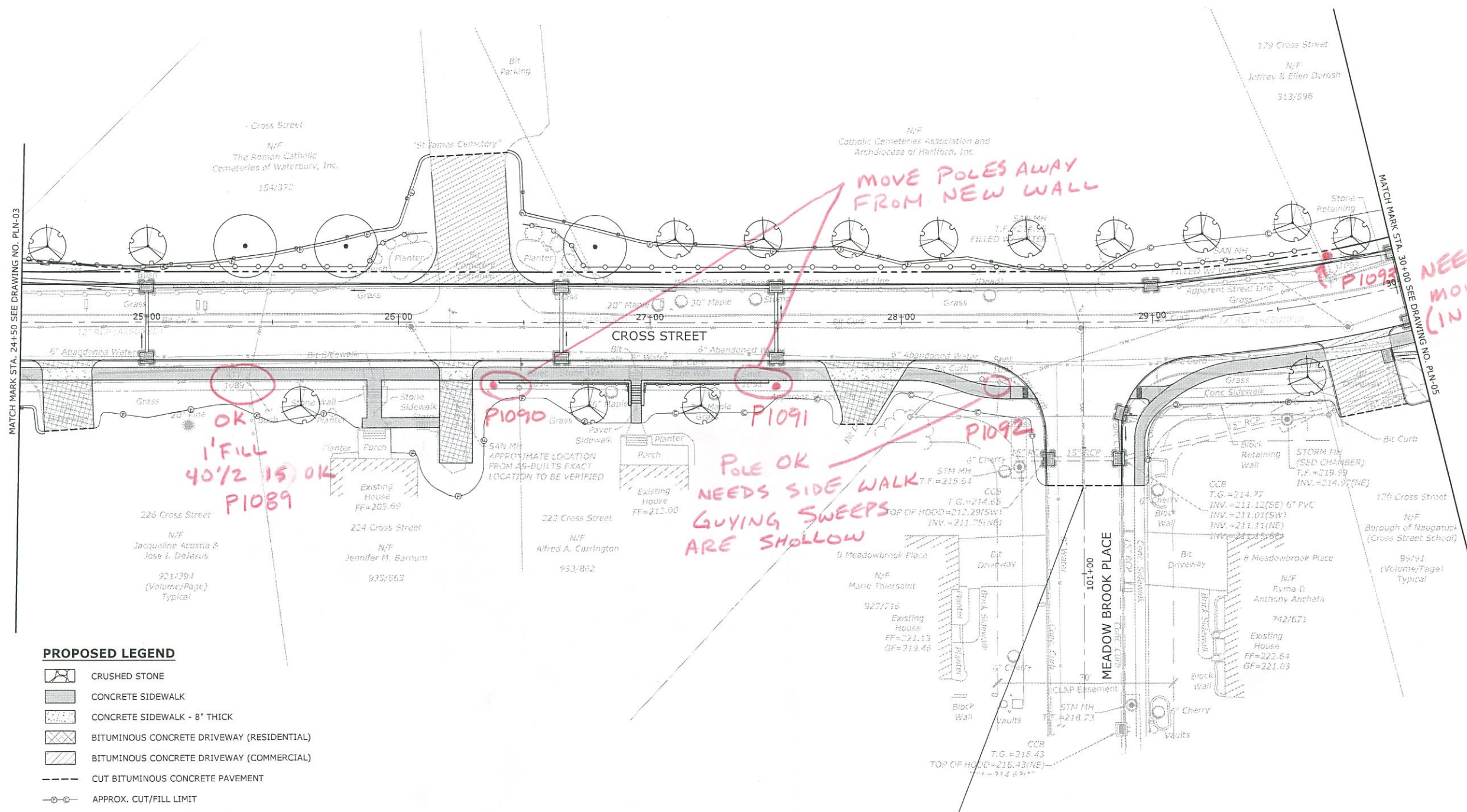
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DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
DATE JULY 5, 2017		
PROJECT NO. 87-145		
DRAWING NO. PLN-03		
SHEET NO. <b>POLE 3</b>		



**"For Information only"**

- PROPOSED LEGEND**
- CRUSHED STONE
  - CONCRETE SIDEWALK
  - CONCRETE SIDEWALK - 8" THICK
  - BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
  - BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
  - CUT BITUMINOUS CONCRETE PAVEMENT
  - APPROX. CUT/FILL LIMIT
  - SEDIMENTATION CONTROL SYSTEM
  - PROPOSED TAKING LINE
  - PROPOSED EASEMENT LINE
  - PROPOSED STREETLINE
  - PROPOSED UTILITY POLE W/ STREET LIGHT
  - SHADE TREE
  - ORNAMENTAL TREE





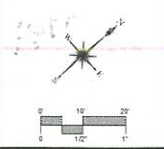
MATCH MARK STA. 24+50 SEE DRAWING NO. PLN-03

MATCH MARK STA. 30+00 SEE DRAWING NO. PLN-05

- PROPOSED LEGEND**
- CRUSHED STONE
  - CONCRETE SIDEWALK
  - CONCRETE SIDEWALK - 8" THICK
  - BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
  - BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
  - CUT BITUMINOUS CONCRETE PAVEMENT
  - APPROX. CUT/FILL LIMIT
  - SEDIMENTATION CONTROL SYSTEM
  - PROPOSED TAKING LINE
  - PROPOSED EASEMENT LINE
  - PROPOSED STREETLINE
  - PROPOSED UTILITY POLE W/ STREET LIGHT
  - SHADE TREE
  - ORNAMENTAL TREE

**LIMIT OF CONSTRUCTION**  
STATION 100+65  
MATCH EXISTING

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











DESCRIPTION	DATE	BY

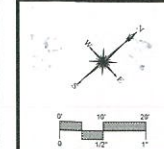
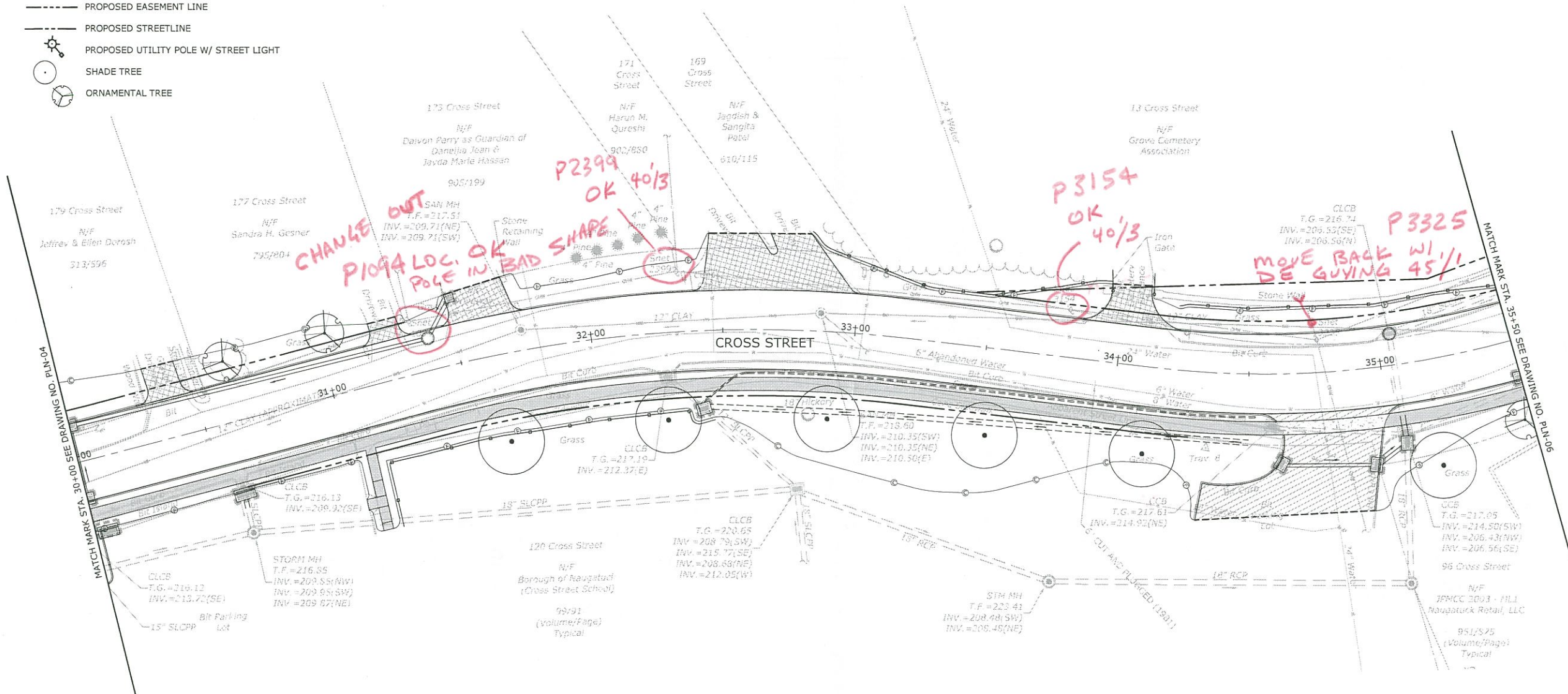
**ROADWAY PLAN - FRONTIER**  
**RECONSTRUCTION OF CROSS STREET**  
**FRONTIER POLE RELOCATION PLAN**  
NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
SCALE: 1"=20'		
DATE: JULY 5, 2017		
PROJECT NO.: 87-145		
DRAWING NO.: PLN-04		
SHEET NO.: <b>Pole 4</b>		



**PROPOSED LEGEND**

-  CRUSHED STONE
-  CONCRETE SIDEWALK
-  CONCRETE SIDEWALK - 8" THICK
-  BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
-  BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
-  CUT BITUMINOUS CONCRETE PAVEMENT
-  APPROX. CUT/FILL LIMIT
-  SEDIMENTATION CONTROL SYSTEM
-  PROPOSED TAKING LINE
-  PROPOSED EASEMENT LINE
-  PROPOSED STREETLINE
-  PROPOSED UTILITY POLE W/ STREET LIGHT
-  SHADE TREE
-  ORNAMENTAL TREE



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DESCRIPTION	DATE	BY

ROADWAY PLAN - FRONTIER  
 RECONSTRUCTION OF CROSS STREET  
 FRONTIER POLE RELOCATION PLAN  
 NAUGATUCK, CONNECTICUT

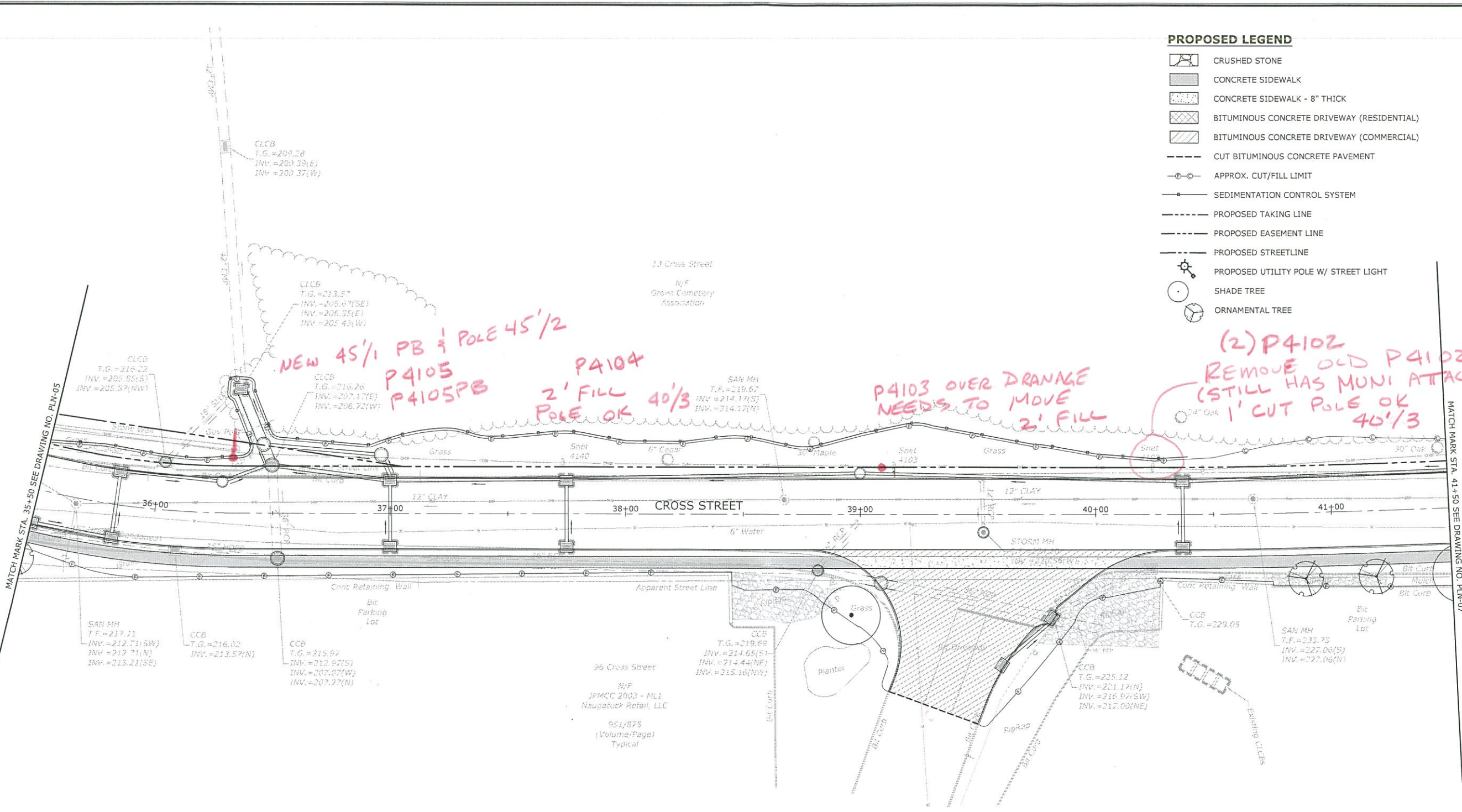
"For Information only"

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED
1"=20'		
SCALE		
JULY 5, 2017		
DATE		
87-145		
PROJECT NO.		
PLN-05		
DRAWING NO.		
<b>Pole 5</b>		
SHEET NO.		

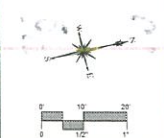


**PROPOSED LEGEND**

- CRUSHED STONE
- CONCRETE SIDEWALK
- CONCRETE SIDEWALK - 8" THICK
- BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
- BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
- CUT BITUMINOUS CONCRETE PAVEMENT
- APPROX. CUT/FILL LIMIT
- SEDIMENTATION CONTROL SYSTEM
- PROPOSED TAKING LINE
- PROPOSED EASEMENT LINE
- PROPOSED STREETLINE
- PROPOSED UTILITY POLE W/ STREET LIGHT
- SHADE TREE
- ORNAMENTAL TREE



"For Information only"



**MILONE & MACBROOM**  
 99 Realty Drive  
 Cheshire, Connecticut 06410  
 (203) 271-1773 Fax (203) 272-9733  
 www.miloneandmacbroom.com














DATE	DESCRIPTION

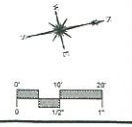
ROADWAY PLAN - FRONTIER  
 RECONSTRUCTION OF CROSS STREET  
 FRONTIER POLE RELOCATION PLAN  
 NAUGATUCK, CONNECTICUT

DP DESIGNED	TDS DRAWN	MJJ CHECKED
SCALE 1"=20'		
DATE JULY 5, 2017		
PROJECT NO. 87-145		
DRAWING NO. PLN-06		
SHEET NO. <b>POLE 6</b>		



**PROPOSED LEGEND**

-  CRUSHED STONE
-  CONCRETE SIDEWALK
-  CONCRETE SIDEWALK - 8" THICK
-  BITUMINOUS CONCRETE DRIVEWAY (RESIDENTIAL)
-  BITUMINOUS CONCRETE DRIVEWAY (COMMERCIAL)
-  CUT BITUMINOUS CONCRETE PAVEMENT
-  APPROX. CUT/FILL LIMIT
-  SEDIMENTATION CONTROL SYSTEM
-  PROPOSED TAKING LINE
-  PROPOSED EASEMENT LINE
-  PROPOSED STREETLINE
-  PROPOSED UTILITY POLE W/ STREET LIGHT
-  SHADE TREE
-  ORNAMENTAL TREE



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 99 Reedy Drive  
 Cheshire, Connecticut 06410  
 (203) 271-1773 Fax (203) 272-9733  
 www.miloneandmacbroom.com

DATE	BY

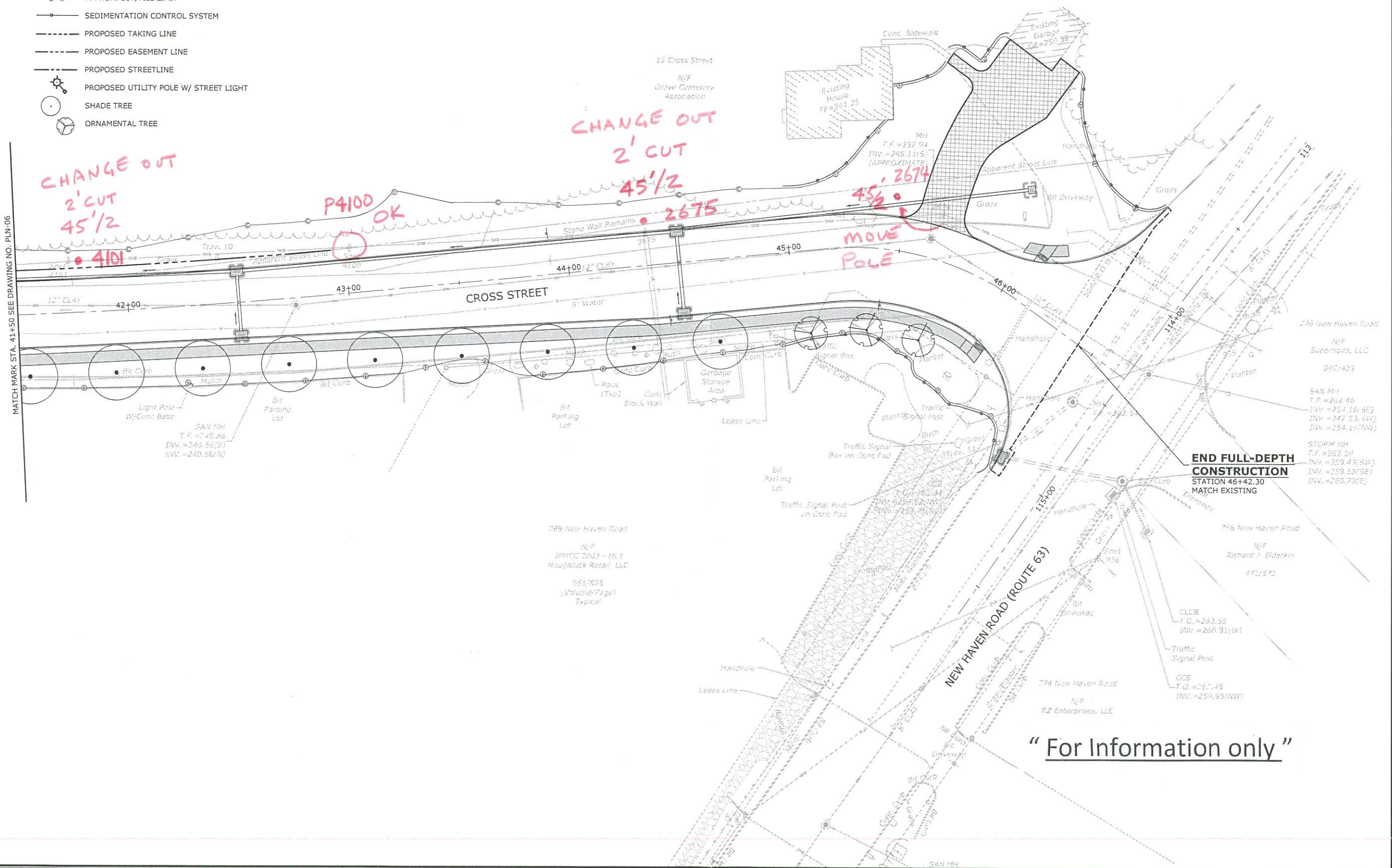
DESCRIPTION

ROADWAY PLAN - FRONTIER  
 RECONSTRUCTION OF CROSS STREET  
 FRONTIER POLE RELOCATION PLAN  
 NAUGATUCK, CONNECTICUT

DP	TDS	MJJ
DESIGNED	DRAWN	CHECKED

SCALE: 1"=20'  
 DATE: JULY 5, 2017  
 PROJECT NO.: 87-145  
 DRAWING NO.: PLN-07

**POLE 7**



**"For Information only"**

MATCH MARK STA. 41+50 SEE DRAWING NO. PLN-06



\*ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT # 87-145

\*\*REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**
<input type="checkbox"/>	HW-506_01	ENDWALLS, SLOPE PAVED INLETS AND OUTLETS	1-26-12
<input type="checkbox"/>	HW-506_02	TYPE "D-G" & "L" ENDWALLS	7-13-12
<input type="checkbox"/>	HW-506_03	ENDWALLS FOR PIPE ARCH	9-18-09
<input checked="" type="checkbox"/>	HW-507_01	TYPE "C", "C-L" & DROP INLET CATCH BASIN	7-24-13
<input type="checkbox"/>	HW-507_02	TYPE "C", "C-L" & DOUBLE GRATE TYPE - I	7-24-13
<input checked="" type="checkbox"/>	HW-507_03	TYPE "C", "C-L" & DOUBLE GRATE TYPE - II	7-24-13
<input checked="" type="checkbox"/>	HW-507_04	TYPE "C", "C-L" & ROUND PRECAST CONCRETE CB	11-10-11
<input type="checkbox"/>	HW-507_05	TYPE "C" & "C-L" PRECAST CONCRETE CB DOUBLE GRATE TYPE - I	11-10-11
<input checked="" type="checkbox"/>	HW-507_06	TYPE "C" & "C-L" PRECAST CONCRETE CB DOUBLE GRATE TYPE - II	11-10-11
<input checked="" type="checkbox"/>	HW-507_07	TYPE "C" & "C-L" CATCH BASIN TOPS AND CURBS	11-10-11
<input checked="" type="checkbox"/>	HW-507_08	CATCH BASIN FRAMES AND GRATES	9-18-09
<input type="checkbox"/>	HW-507_09	HEAVY DUTY LOCK DOWN TOPS	7-12-12
<input checked="" type="checkbox"/>	HW-507_10	MANHOLE - FRAME & COVER	7-24-13
<input checked="" type="checkbox"/>	HW-651_01	C.C.M. PIPE INSTALLATIONS IN FILL & ROCK SLOPES & PIPE TRENCH DETAIL	7-24-13
<input type="checkbox"/>	HW-651_02	SLOTTED DRAIN PIPE 12"- 15"-18"-24"-30" (305-381-457-610-762)	7-12-12
<input type="checkbox"/>	HW-652_01	PIPE ENDS	7-24-13
<input checked="" type="checkbox"/>	HW-751_01	UNDERDRAINS AND UNDERDRAIN OUTLETS	7-12-12
<input type="checkbox"/>	HW-803_01a	PAVED APRONS	6-07-17
<input type="checkbox"/>	HW-803_01b	PAVED DITCHES AND PAVED CHANNELS	6-07-17
<input checked="" type="checkbox"/>	HW-811_01	CONCRETE CURBING	6-07-17
<input type="checkbox"/>	HW-813_01	GRANITE STONE TRANSITION CURBING	7-24-13
<input type="checkbox"/>	HW-813_02	STONE CURBING	6-07-17
<input checked="" type="checkbox"/>	HW-815_01	BITUMINOUS CONCRETE CURBING	6-07-17
<input type="checkbox"/>	HW-821_01a	TRANSITION - 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 1	1-26-12
<input type="checkbox"/>	HW-821_01b	TRANSITION - 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 2	10-18-10
<input type="checkbox"/>	HW-821_01c	TRANSITION - 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 3	1-26-12
<input type="checkbox"/>	HW-821_02a	45" (1145) F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 1	7-24-13
<input type="checkbox"/>	HW-821_02b	45" (1145) F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 2	7-24-13
<input type="checkbox"/>	HW-821_03a	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 1	1-26-12
<input type="checkbox"/>	HW-821_03b	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 2	10-18-10
<input type="checkbox"/>	HW-821_03c	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 3	10-18-10
<input type="checkbox"/>	HW-821_03d	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 4	10-18-10
<input type="checkbox"/>	HW-821_03e	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) F-SHAPE	7-24-13

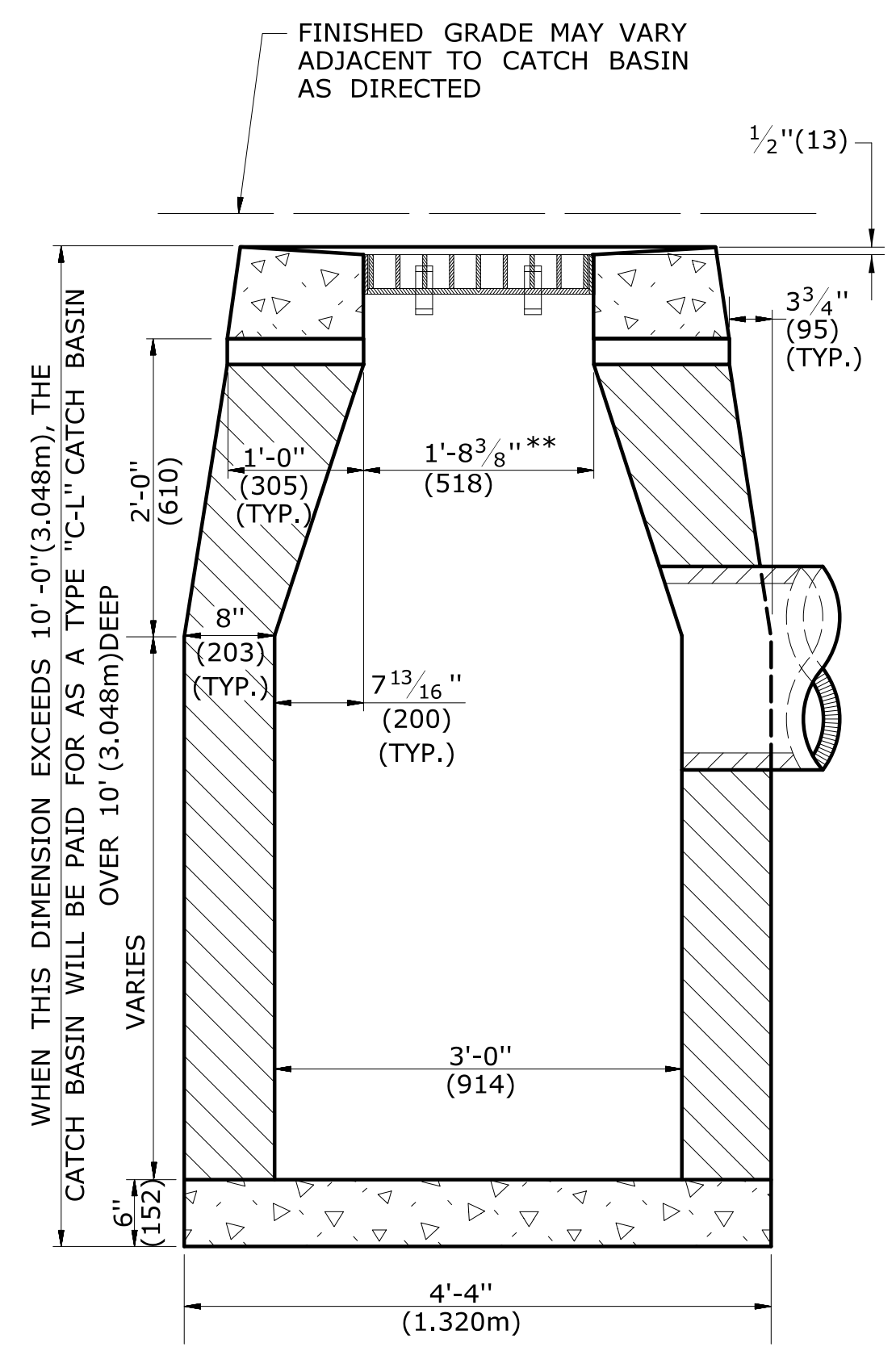
✓*	SHEET NO.	TITLE	APPROVAL DATE**
<input type="checkbox"/>	HW-821_04a	MERRITT PARKWAY NARROW MEDIAN BARRIER	6-09-11
<input type="checkbox"/>	HW-821_04b	MERRITT PARKWAY - 2' (610) WIDE MEDIAN BARRIER AND ROADSIDE BARRIER	7-24-13
<input type="checkbox"/>	HW-821_05a	TRANSITION - 45" (1145) F-SHAPE TO 54" (1372) VERTICAL SHAPE SHEET 1	1-26-12
<input type="checkbox"/>	HW-821_05b	TRANSITION - 45" (1145) F-SHAPE TO 54" (1372) VERTICAL SHAPE SHEET 2	1-26-12
<input type="checkbox"/>	HW-821_06	54" (1372) VERTICAL SHAPE BARRIER	2-06-12
<input type="checkbox"/>	HW-821_07	MISCELLANEOUS DETAILS FOR BARRIER TRANSITIONS	7-12-12
<input type="checkbox"/>	HW-822_01	TEMPORARY PRECAST CONCRETE BARRIER CURB	7-24-13
<input type="checkbox"/>	HW-905_01	STONE WALL, FARM WALL AND WIRE FENCES	6-07-17
<input checked="" type="checkbox"/>	HW-910_01	W-BEAM METAL BEAM RAIL HARDWARE	6-09-11
<input type="checkbox"/>	HW-910_02	METAL BEAM RAIL (TYPE R-B 350) GUIDERAIL	6-09-11
<input type="checkbox"/>	HW-910_03	METAL BEAM RAIL (TYPE MD-B 350)	6-09-11
<input type="checkbox"/>	HW-910_04	METAL BEAM RAIL (TYPE R-B 350) SYSTEMS 5, 5A, & 6	6-09-11
<input type="checkbox"/>	HW-910_05	METAL BEAM RAIL R-B 350 SPAN TYPE I, II, III SECTIONS	7-24-13
<input type="checkbox"/>	HW-910_06	R-B 350 BRIDGE ATTACHMENT SAFETY SHAPE PARAPET	6-09-11
<input type="checkbox"/>	HW-910_07	R-B 350 BRIDGE ATTACHMENT VERTICAL SHAPE PARAPET	6-09-11
<input type="checkbox"/>	HW-910_08	R-B 350 BRIDGE ATTACHMENT TRAILING END	6-09-11
<input type="checkbox"/>	HW-910_09a	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 1	1-26-12
<input type="checkbox"/>	HW-910_09b	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 2	7-25-12
<input type="checkbox"/>	HW-910_10	METAL BEAM RAIL 8" (203) X 6" (152) BOX BEAM	7-24-13
<input type="checkbox"/>	HW-910_11	CURVED GUIDERAIL TREATMENT DETAIL	7-25-12
<input type="checkbox"/>	HW-910_12a	MERRITT PARKWAY GUIDERAIL ATTACHMENT - SYSTEM 2 & 3	7-24-13
<input type="checkbox"/>	HW-910_12b	MERRITT PARKWAY GUIDERAIL	7-24-13
<input type="checkbox"/>	HW-910_12c	MERRITT PARKWAY GUIDERAIL TRAILING END ATTACHMENTS	7-24-13
<input type="checkbox"/>	HW-910_12d	MERRITT PARKWAY MEDIAN GUIDERAIL AND END ANCHOR	6-09-11
<input type="checkbox"/>	HW-910_13a	THRIE-BEAM METAL BEAM RAIL HARDWARE	7-24-13
<input type="checkbox"/>	HW-910_13b	THRIE-BEAM TRANSITIONS	7-24-13
<input type="checkbox"/>	HW-910_14a	THRIE-BEAM 350 BRIDGE ATTACHMENT	6-09-11
<input type="checkbox"/>	HW-910_14b	THRIE-BEAM 350 GUIDERAIL TRANSITION TO R-B 350 GUIDERAIL	6-09-11
<input type="checkbox"/>	HW-910_15	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE I	6-09-11
<input type="checkbox"/>	HW-910_16	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE II	6-09-11
<input type="checkbox"/>	HW-910_17	R-B TERMINAL SECTION	7-24-13
<input type="checkbox"/>	HW-910_18	METAL BEAM RAIL (TYPE MD-I)	10-18-10
<input type="checkbox"/>	HW-910_19a	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE I	7-24-13

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

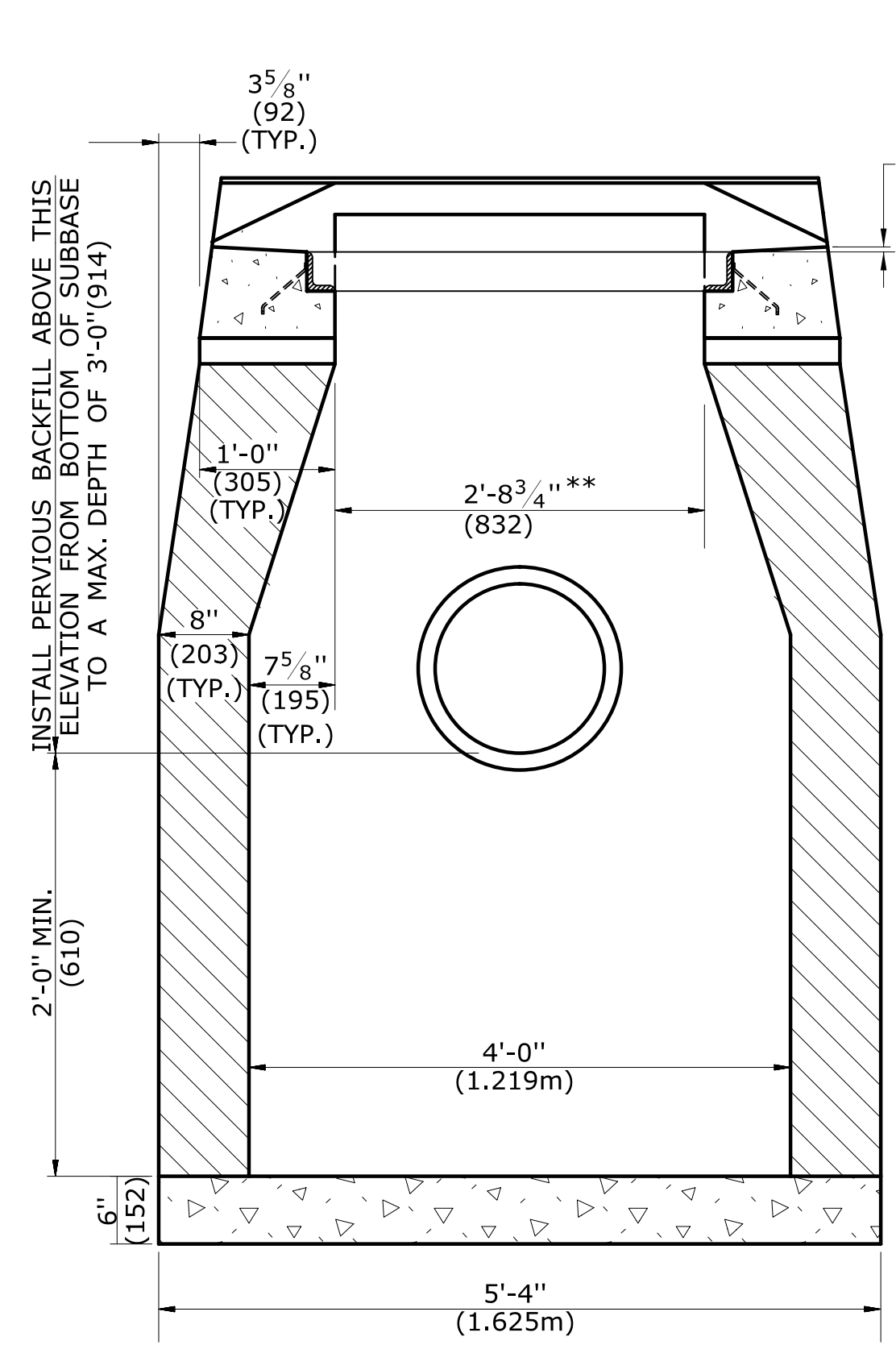
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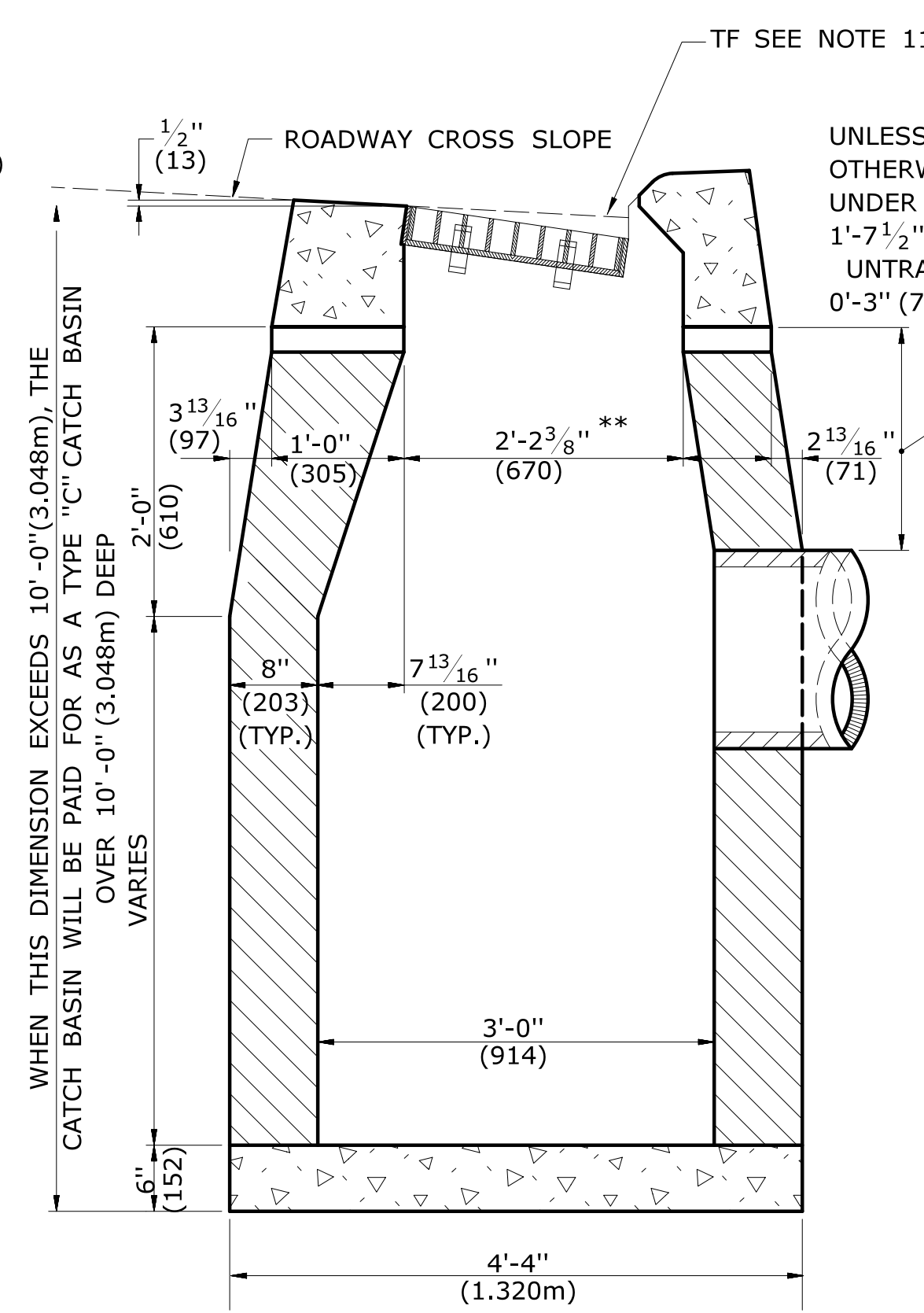




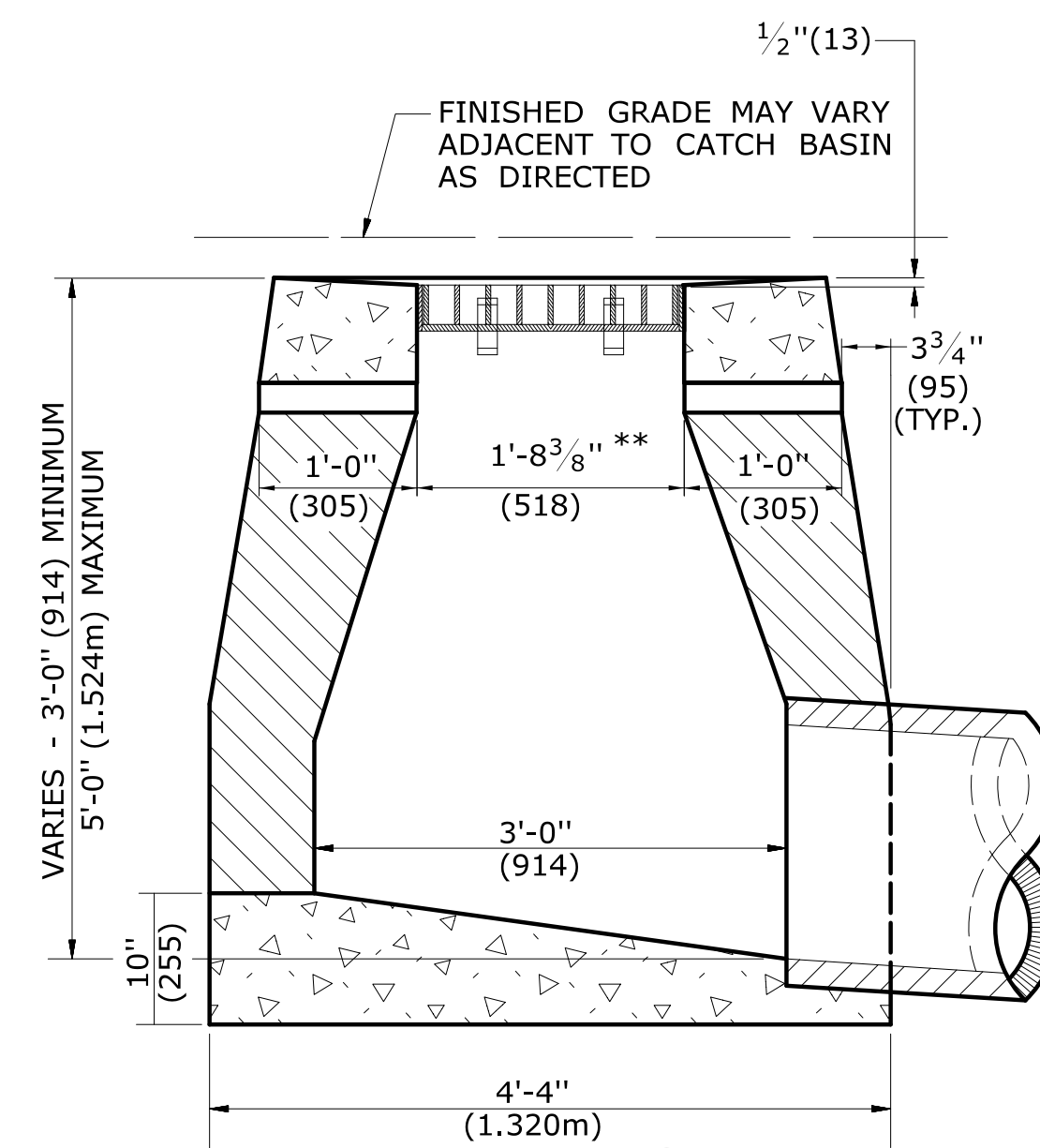
**SECTION B**  
**TYPE "C-L" CATCH BASIN**



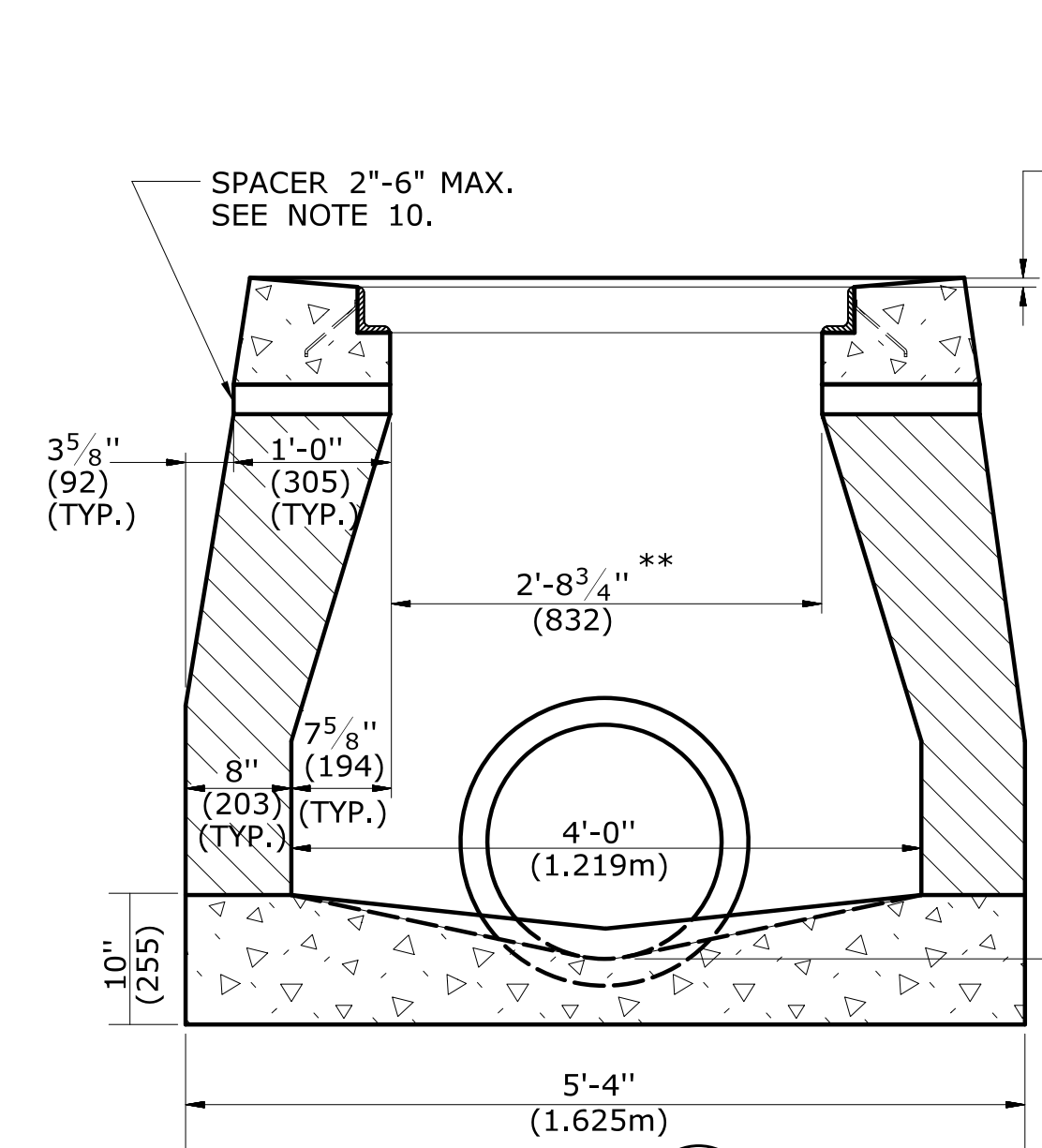
**SECTION A**  
**TYPE "C" & "C-L" CATCH BASIN  
(TYPE "C" TOP SHOWN)**



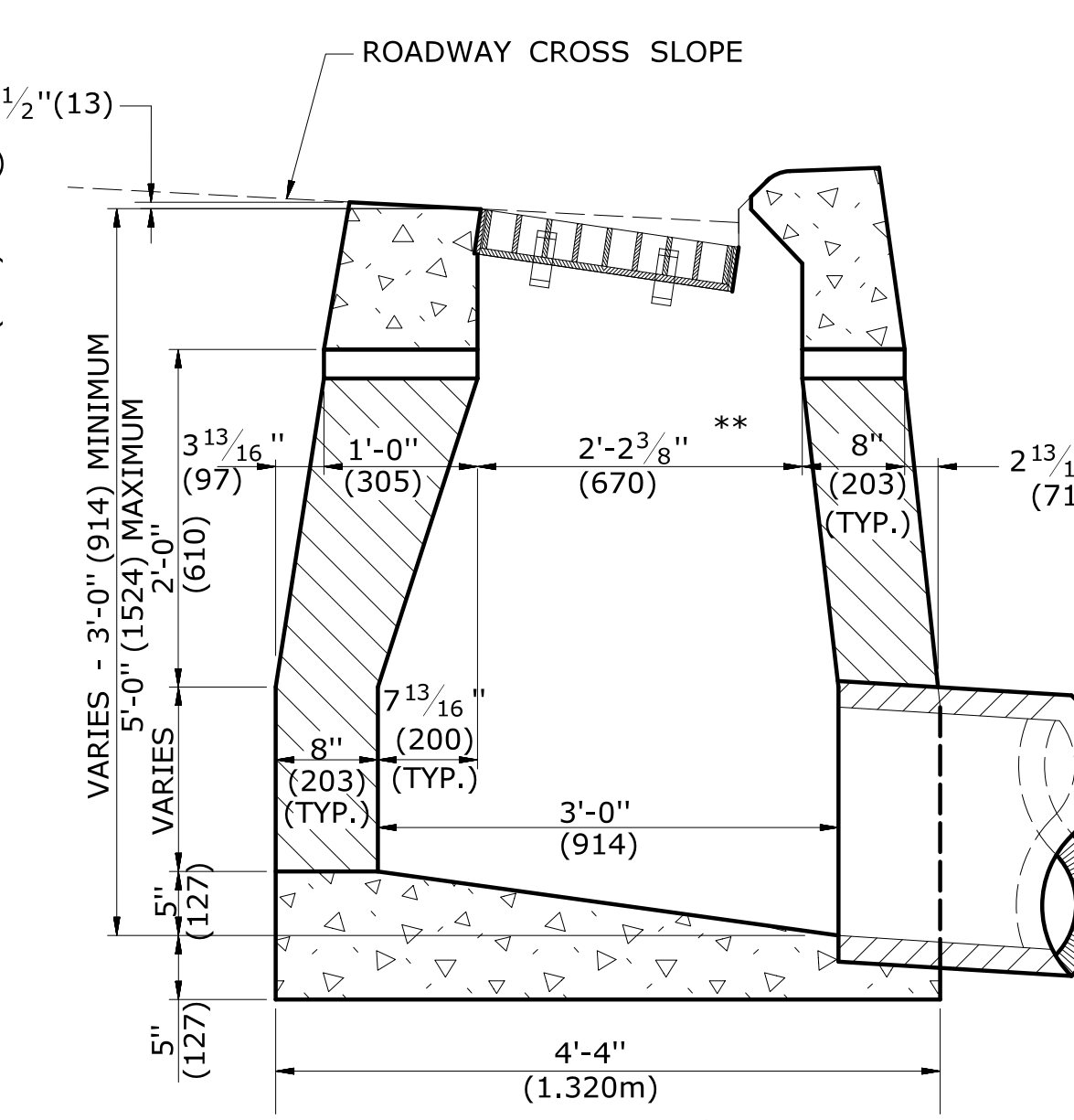
**SECTION B**  
**TYPE "C" CATCH BASIN**



**SECTION B**  
**TYPE "C-L" DROP INLET**



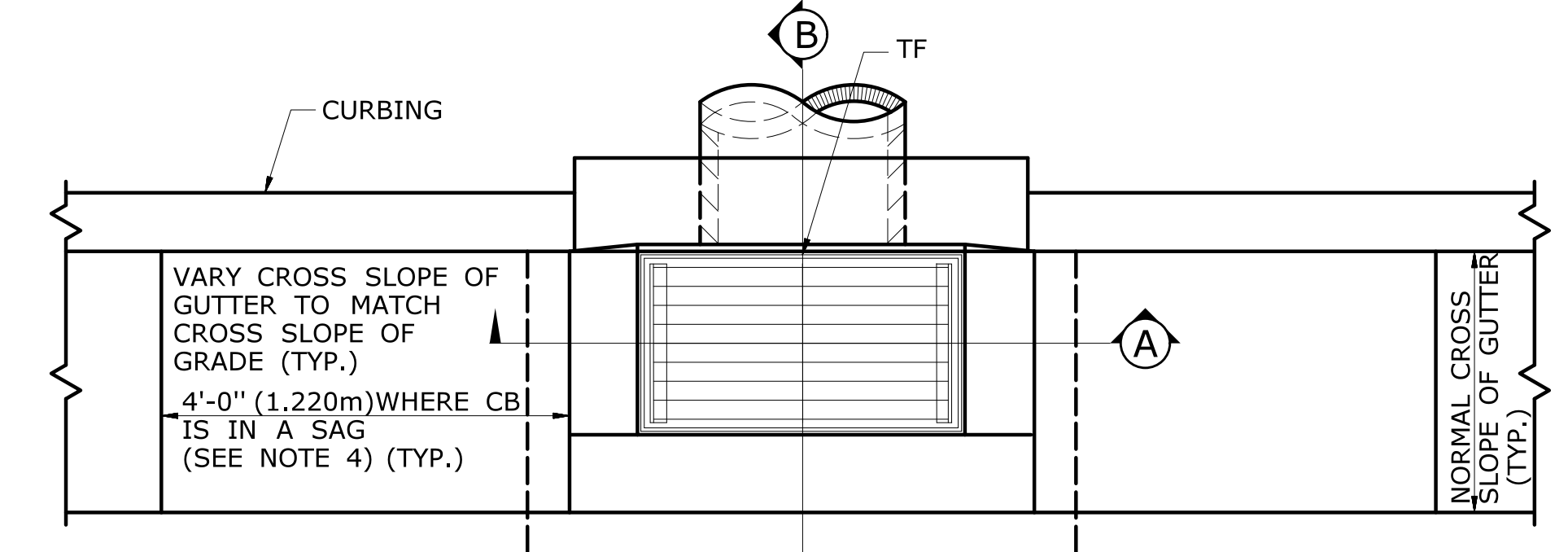
**SECTION A**  
**TYPE "C" & "C-L" DROP INLET  
(TYPE "C-L" TOP SHOWN)**



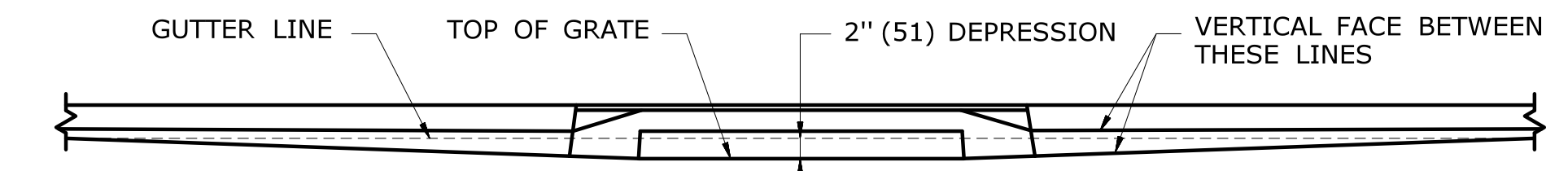
**SECTION B**  
**TYPE "C" DROP INLET**

**GENERAL NOTES:**

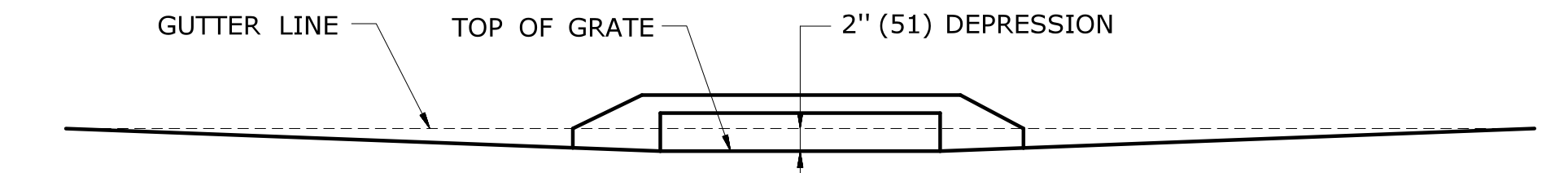
- FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507-08.
- USE APPROPRIATE CONCRETE TOP FOR CURBING SHOWN ON PLANS. IF CURBING IS NOT SPECIFIED ON THE PLANS, IT SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.
- ALL FACES OF STRUCTURES IN CONTACT WITH CONCRETE PAVEMENT SHALL BE COVERED WITH A LAYER OF TAR PAPER OR APPROVED EQUAL. THE COST FOR THE PAPER SHALL BE INCLUDED IN THE BID PRICE FOR THE TYPE OF CATCH BASIN INSTALLED.
- USE 6'-0" (1.830m) ON UPGRADE SIDE OF CONTINUOUS GRADE AND 1'-0" (305mm) ON DOWNGRADE SIDE OF CONTINUOUS GRADE OR AS DIRECTED.
- IF MASONRY UNITS ARE REQUIRED, THE BASIN SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE OVER ALL DIMENSIONS SHOWN HERE AND SECTION 5.07 OF THE STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS. CORBELLING SHALL BE PERMITTED TO A MAXIMUM OF 3" (75mm.) NO PROJECTION SHALL EXTEND INSIDE THE LIMITS NOTED BY \*\*.
- WALL THICKNESS OF ALL CB'S OVER 10' (3.048m) DEEP SHALL BE INCREASED TO 12" (305mm) THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. 12" (305mm) THICKNESS WILL START AFTER THE FIRST 10' (3.048m).
- TO CONVEY SUBSURFACE DRAINAGE, OPENINGS SHALL BE FORMED IN THE FOUR WALLS AT OR IMMEDIATELY ABOVE THE BOTTOM OF THE PERVIOUS BACKFILL.
- MINIMUM CONCRETE COMPRESSIVE STRENGTH OF F<sub>c</sub> = 4000 PSI (27,580 kPa) SHALL BE OBTAINED PRIOR TO SHIPPING.
- LATEST STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
- SPACER MAY BE CMU OR PRECAST WITH REQUIRED REINFORCING (RECOMMENDED BY THE MANUFACTURER) AS NEEDED TO PROVIDE PROPER GRADE SHOWN ON PLANS.
- TOP OF FRAME (TF) ELEVATION SHALL BE MEASURED IN THE CENTER OF GRATE @ GUTTER LINE.



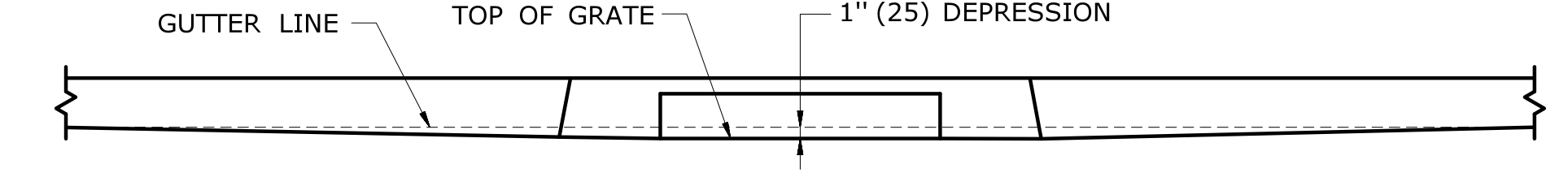
**PLAN**



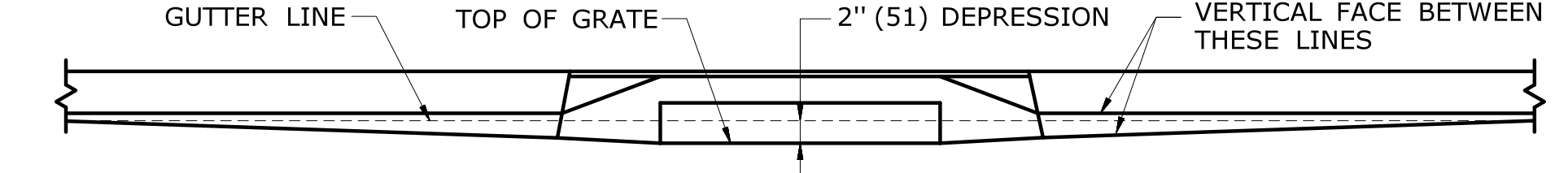
**FOR CATCH BASINS IN A LINE OF 4" (102) CONCRETE PARK CURBING OR 4" (102) BITUMINOUS CONCRETE PARK CURBING**



**FOR CATCH BASINS WHERE NO CURBING OF ANY TYPE EXISTS OR IS PROPOSED**



**FOR CATCH BASINS IN A LINE OF 6" (152) CONCRETE CURBING OR 6" (152) STONE CURBING**



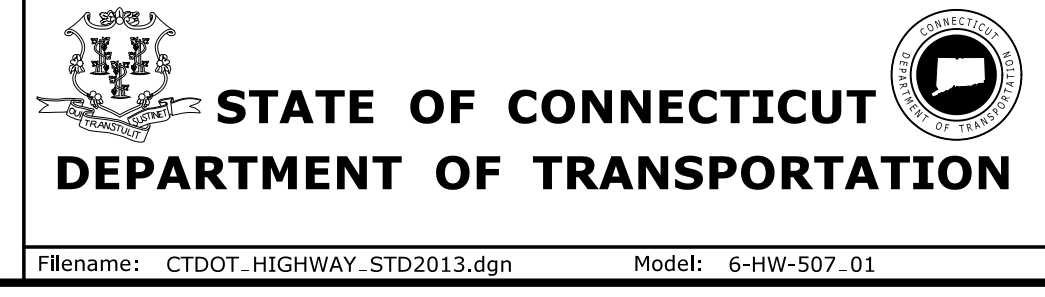
**FOR CATCH BASINS IN A LINE OF 6" (152) BITUMINOUS CONCRETE LIP CURBING (MACHINE FORMED)**

**DETAILS OF DEPRESSED GUTTER STRIP FOR TYPE "C" CATCH BASIN**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

4	7/13	ADD NOTE 11	-
3	9/30/11	ADD SPACERS AND NOTE 10.	-
2	9/15/11	MODIFIED DETAILS TO BE CONSISTANT WITH PRECAST	-
1	7/28/11	REMOVE MIN. DROP NOTE	-
-	-	-	-
REV.	DATE	REVISION DESCRIPTION	

NOT TO SCALE

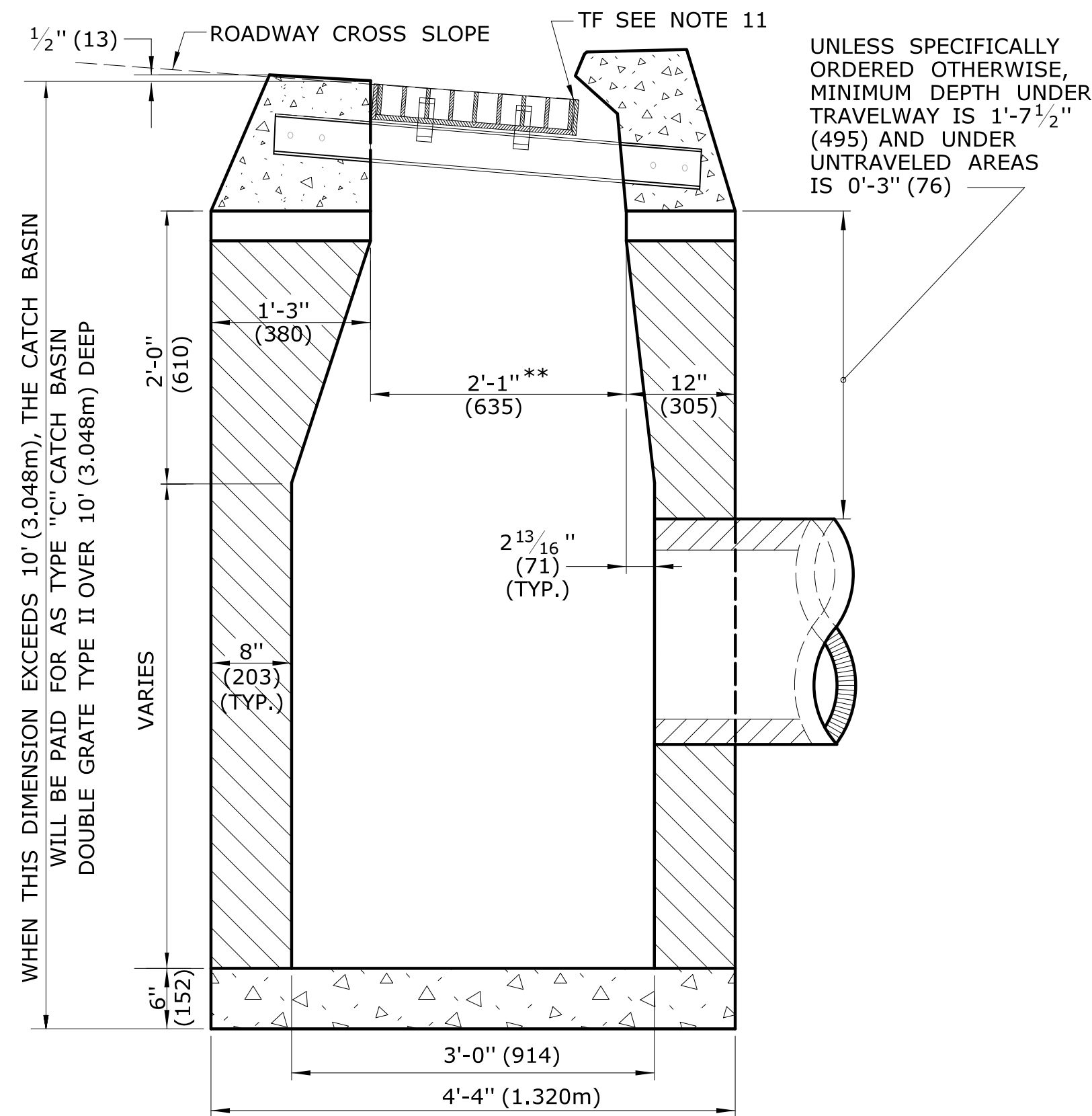


SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

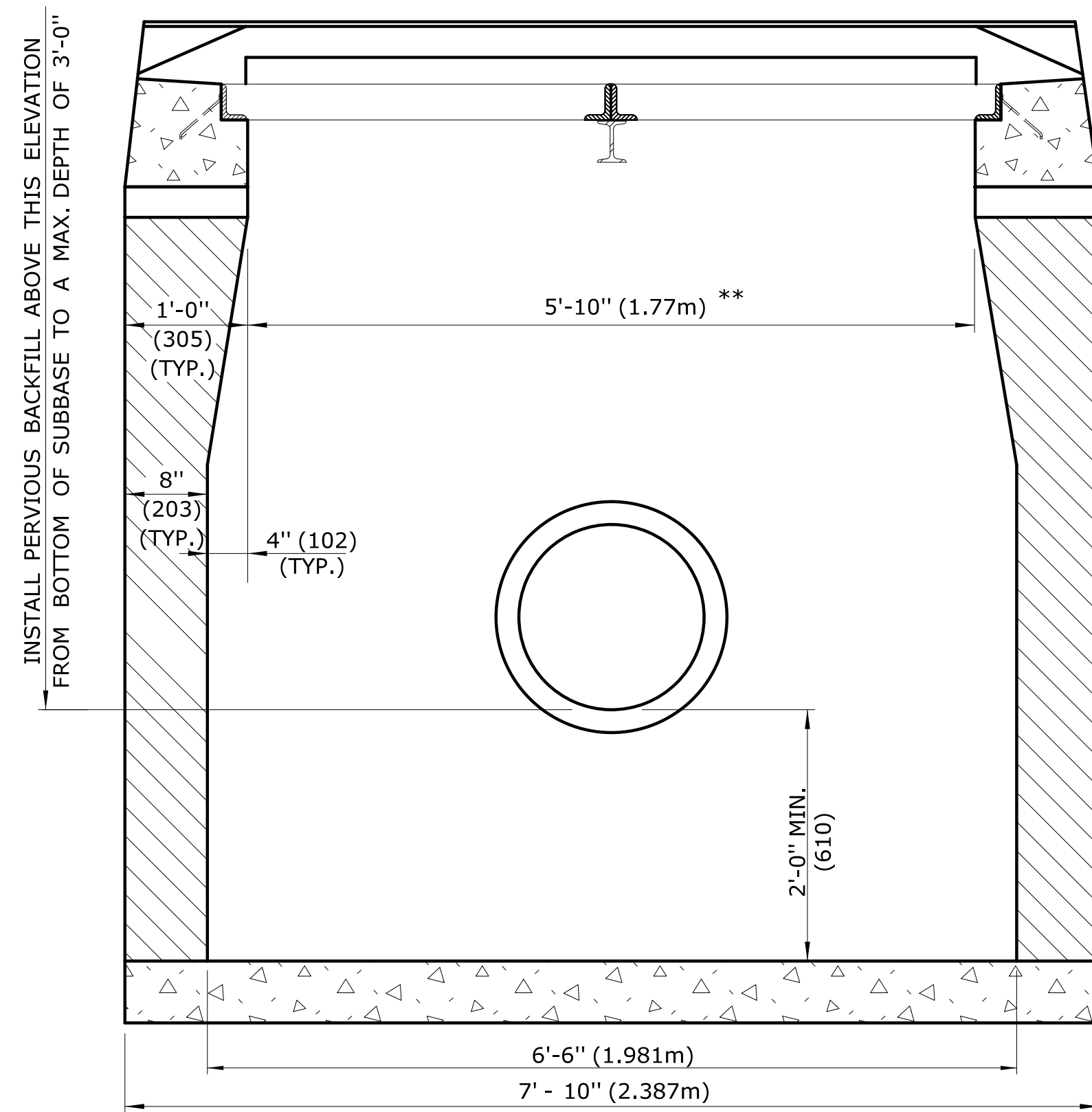
**CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING**

STANDARD SHEET TITLE:  
**TYPE "C", "C-L" &  
DROP INLET CATCH BASIN**

STANDARD SHEET NO.:  
**HW-507\_01**

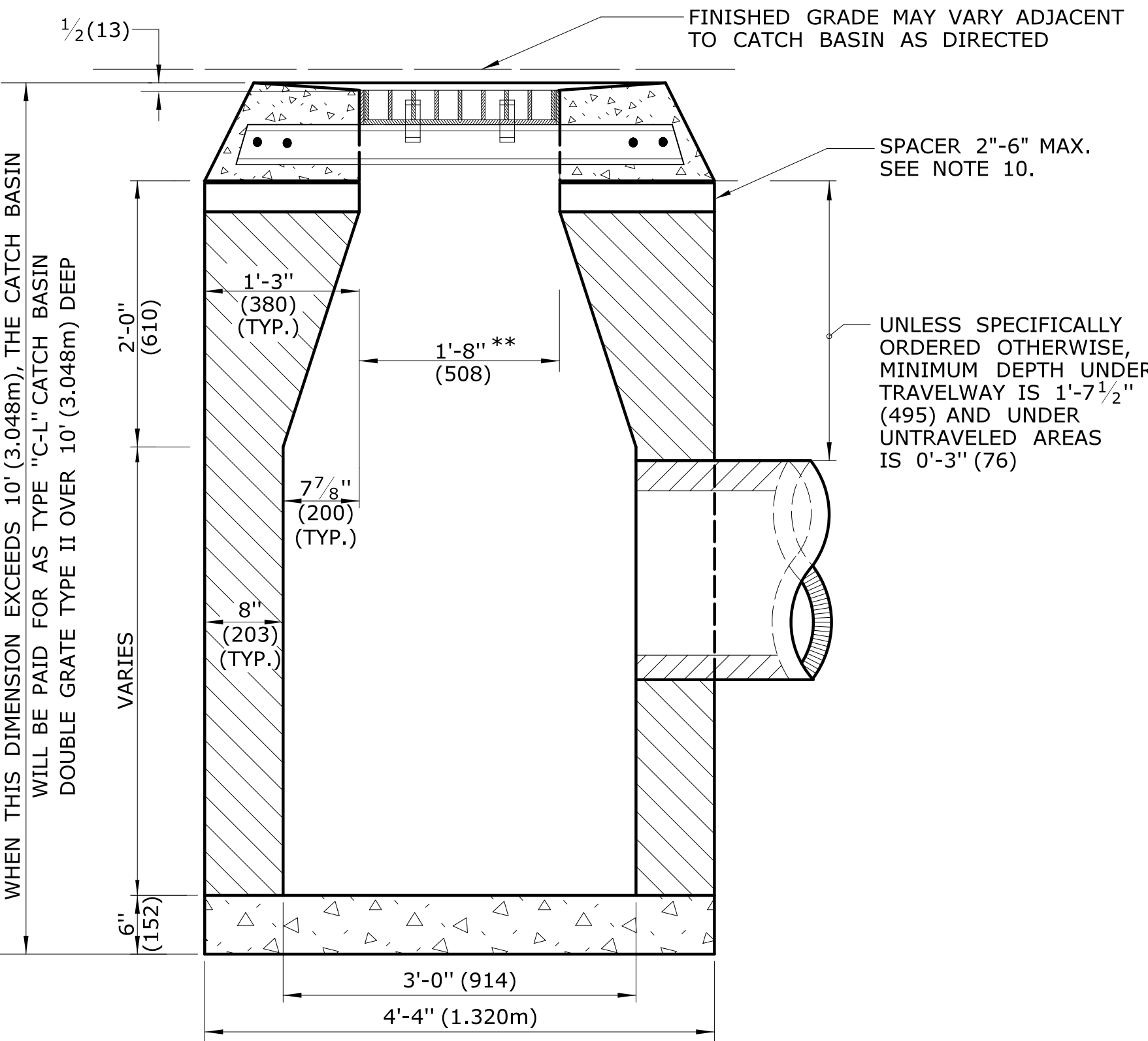


**SECTION B**

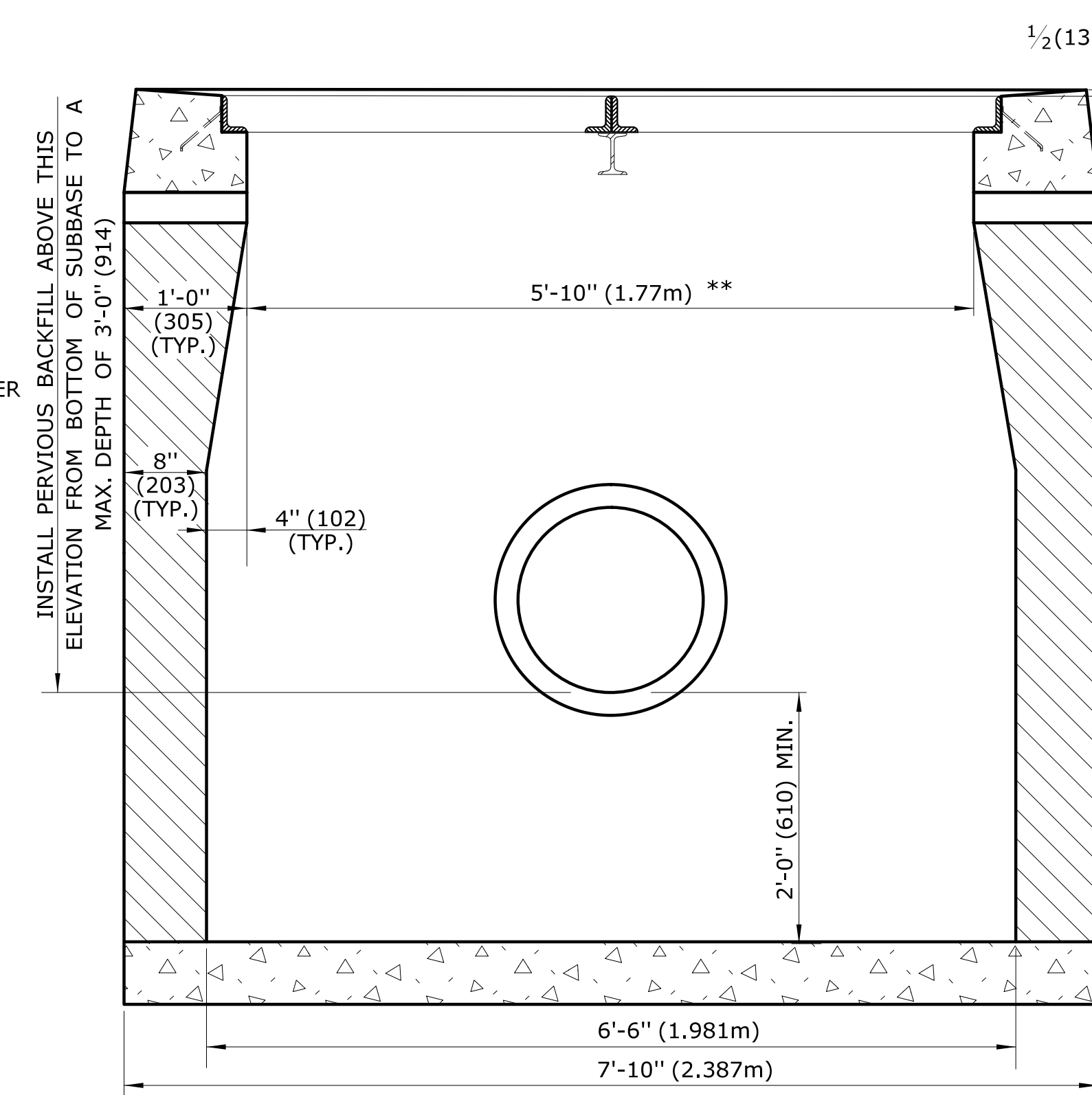


**SECTION A**

**TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE II**



**SECTION B**

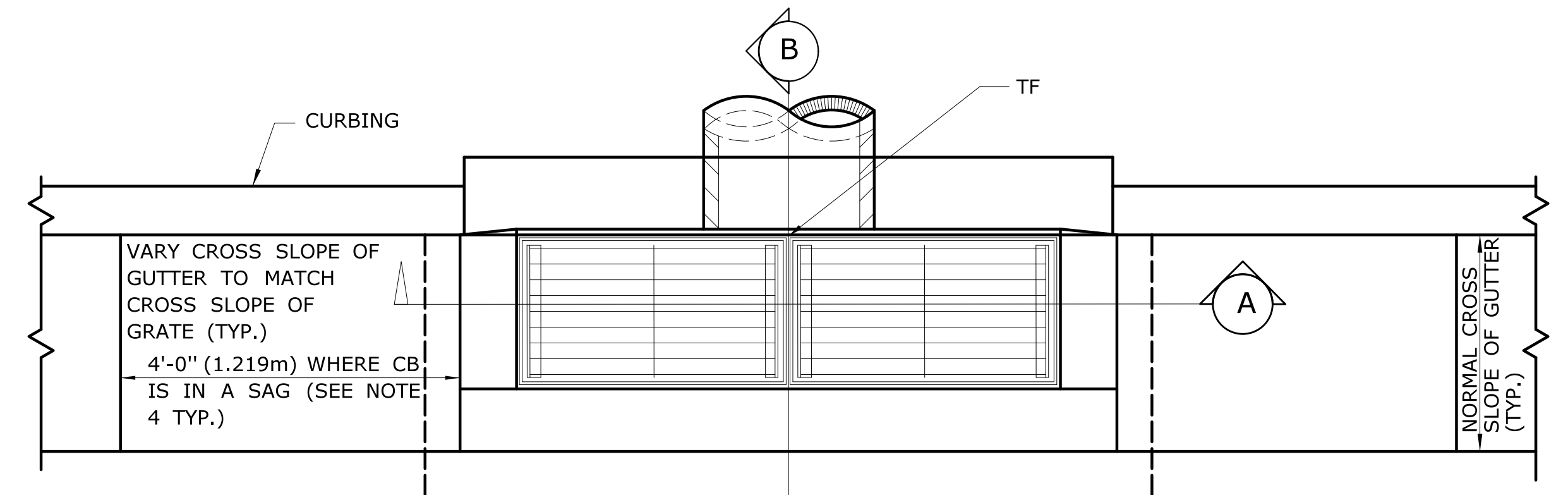


**SECTION A**

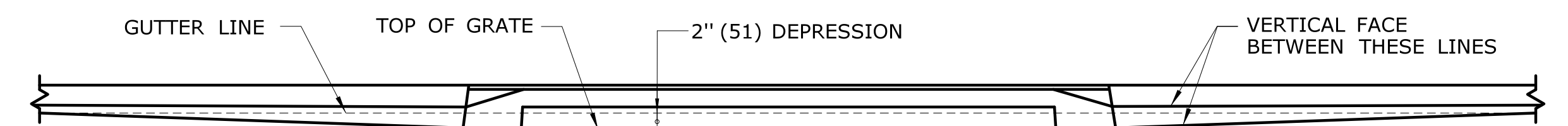
**TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE II**

**GENERAL NOTES:**

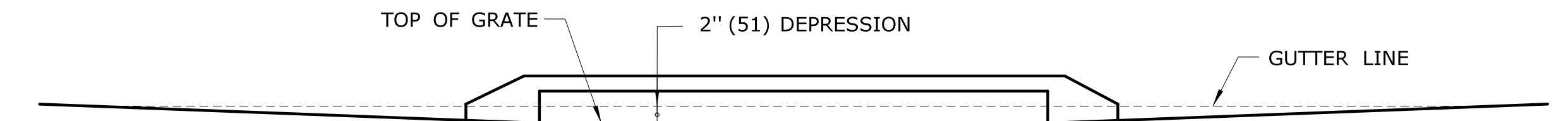
- FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507\_08.
- USE APPROPRIATE CONCRETE TOP FOR CURBING SHOWN ON PLANS. IF CURBING IS NOT SPECIFIED ON THE PLANS, IT SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.
- ALL FACES OF STRUCTURES IN CONTACT WITH CONCRETE PAVEMENT SHALL BE COVERED WITH A LAYER OF TAR PAPER OR APPROVED EQUAL. THE COST FOR THE PAPER SHALL BE INCLUDED IN THE BID PRICE FOR THE TYPE OF CATCH BASIN INSTALLED.
- USE 6'-0" (1.830m) ON UPGRADE SIDE OF CONTINUOUS GRADE AND 1'-0" (305) ON DOWNGRADE SIDE OF CONTINUOUS GRADE OR AS DIRECTED.
- IF MASONRY UNITS ARE REQUIRED, THE BASIN SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE OVER ALL DIMENSIONS SHOWN HERE AND SECTION 5.07 OF THE STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS. CORBELLING SHALL BE PERMITTED TO A MAXIMUM OF 3" (75). NO PROJECTION SHALL EXTEND INSIDE THE LIMITS NOTED BY \*\*.
- WALL THICKNESS OF ALL CB'S OVER 10' (3.048m) DEEP SHALL BE INCREASED TO 12" (305) THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. (12" (305) THICKNESS WILL START AFTER THE FIRST 10' (3.048m)).
- TO CONVEY SUBSURFACE DRAINAGE, OPENINGS SHALL BE FORMED IN THE FOUR WALLS AT OR IMMEDIATELY ABOVE THE BOTTOM OF THE PERVIOUS BACKFILL.
- MINIMUM CONCRETE COMPRESSIVE STRENGTH OF F'c = 4000 PSI (27,580 kPa) SHALL BE OBTAINED PRIOR TO SHIPPING.
- LATEST STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
- SPACER MAY BE CMU OR PRECAST WITH REQUIRED REINFORCING (RECOMMENDED BY THE MANUFACTURER) AS NEEDED TO PROVIDE PROPER GRADE SHOWN ON PLANS.
- TOP OF FRAME (TF) ELEVATION SHALL BE MEASURED IN BETWEEN BOTH GRATES @ THE GUTTER.



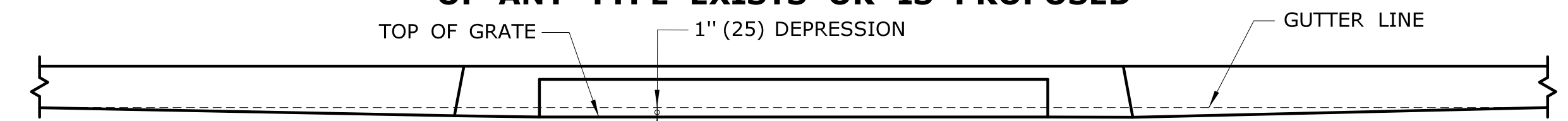
**PLAN**



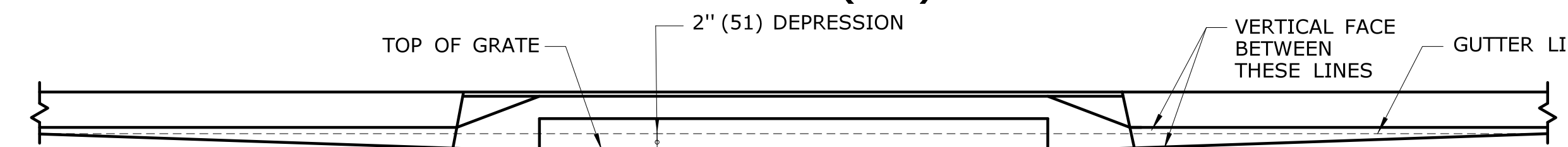
**FOR CATCH BASINS IN A LINE OF 4" (102) CONCRETE PARK CURBING OR 4" (102) BITUMINOUS CONCRETE PARK CURBING**



**FOR CATCH BASINS WHERE NO CURBING OF ANY TYPE EXISTS OR IS PROPOSED**



**FOR CATCH BASINS IN A LINE OF 6" (152) CONCRETE CURBING OR 6" (152) STONE CURBING**



**FOR CATCH BASINS IN A LINE OF 6" (152) BITUMINOUS CONCRETE LIP CURBING (MACHINE FORMED)**

**DETAILS OF DEPRESSED GUTTER STRIP FOR TYPE "C" CATCH BASIN DOUBLE GRATE TYPE II**

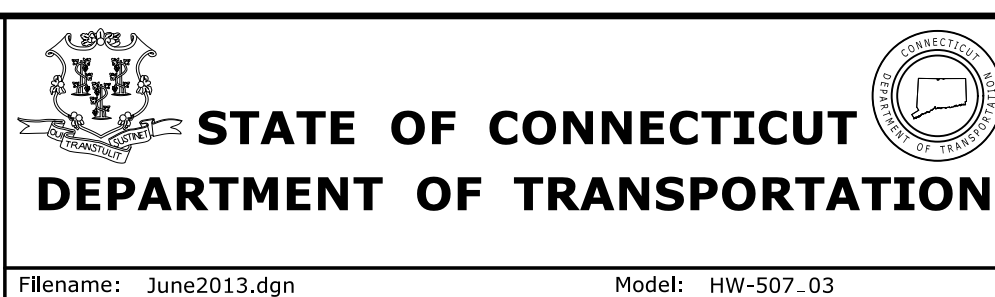
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
4	7/13	ADD NOTE 11
3	10/3/11	ADD SPACERS AND NOTE 10.
2	9/15/11	MODIFIED DETAILS TO BE CONSISTANT WITH PRECAST
1	7/28/11	REMOVE MIN. DROP NOTE

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 3/21/2013

NOT TO SCALE



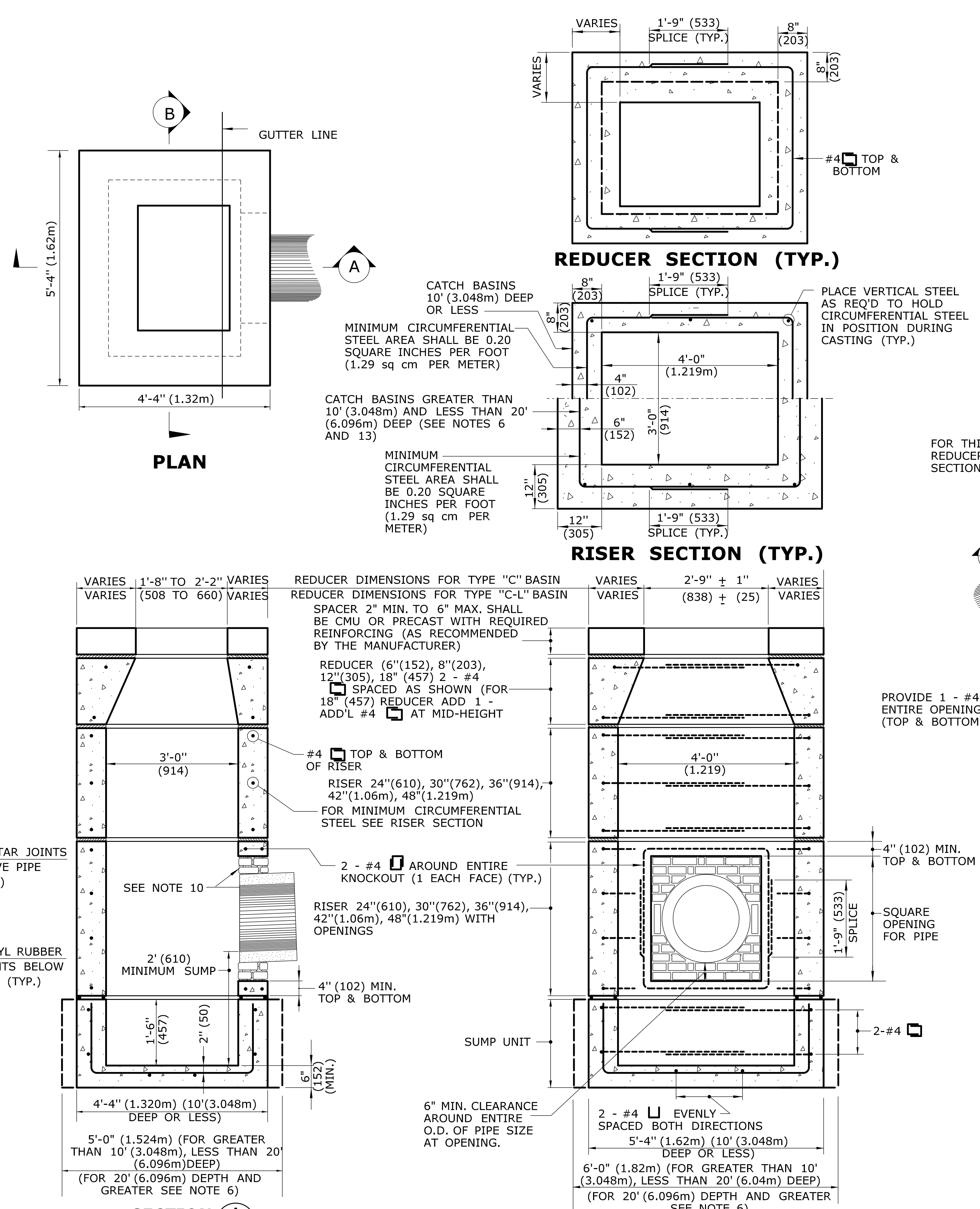
SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

**CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING**

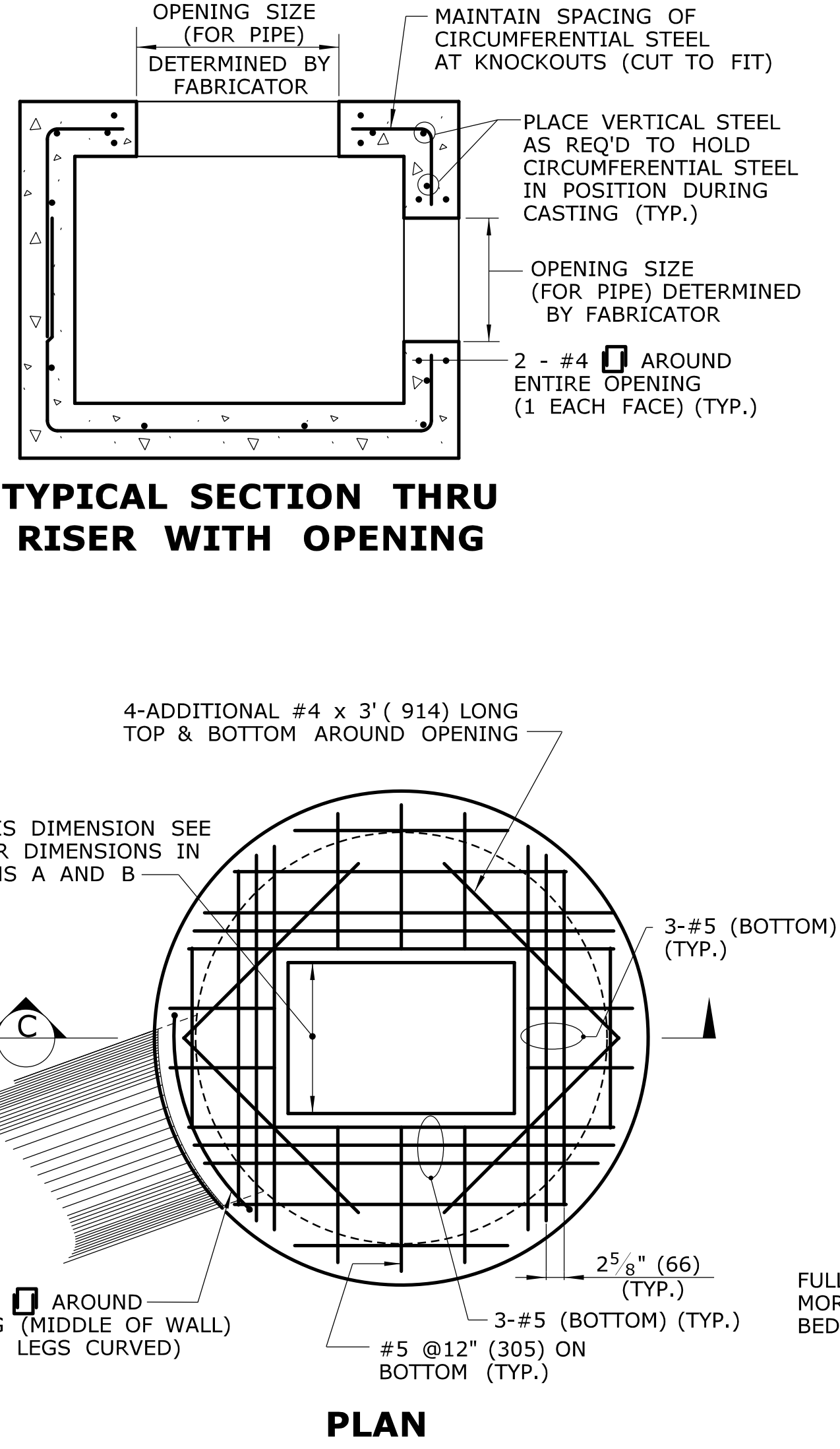
STANDARD SHEET TITLE:  
**TYPE "C" , "C-L" &  
DOUBLE GRATE TYPE - II**

STANDARD SHEET NO.:  
**HW-507\_03**





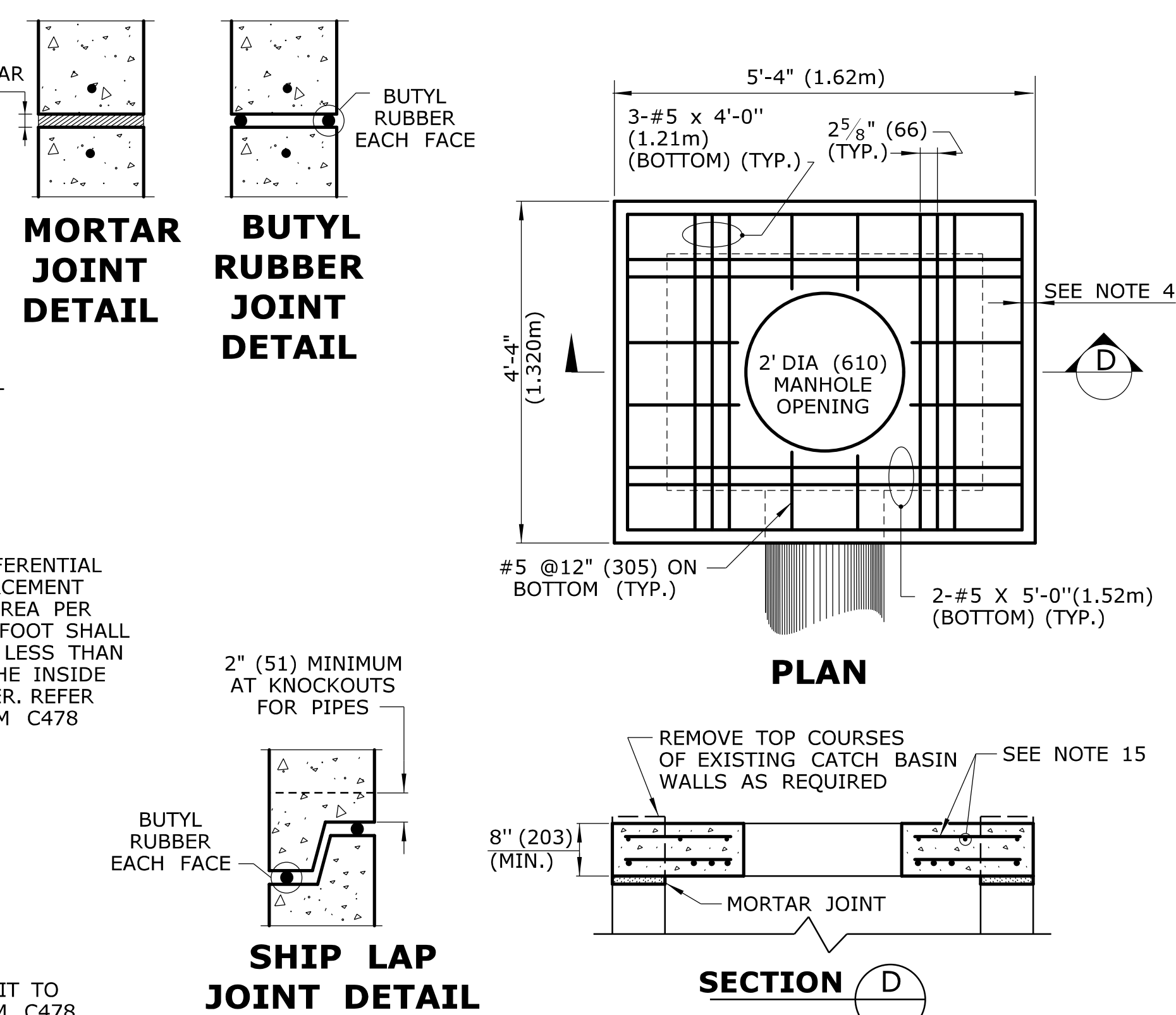
**SECTION A**  
**SECTION B**  
**PRECAST CONCRETE TYPE "C" & "C-L" CATCH BASIN**  
(UNDER 10' (3.04m) DEEP SHOWN)



**SECTION C**  
**PRECAST CONCRETE TYPE "C" & "C-L" ROUND STRUCTURE**  
(SEE NOTE 9)

**GENERAL NOTES:**

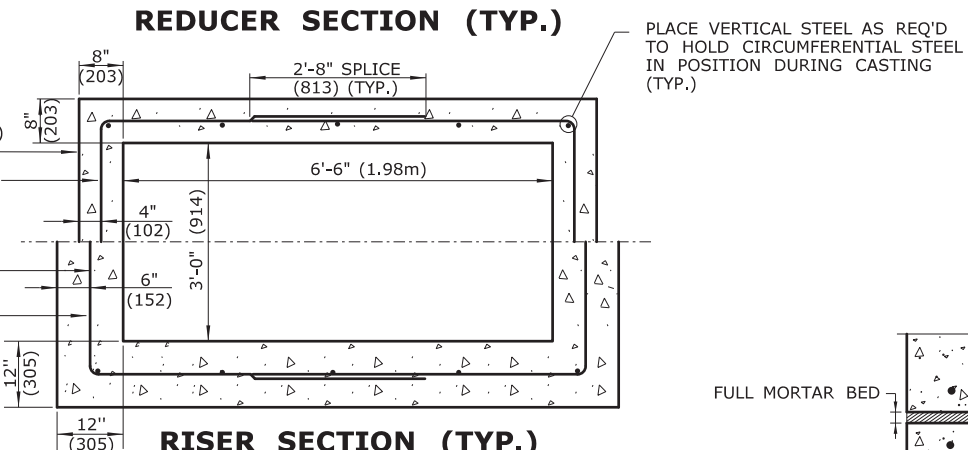
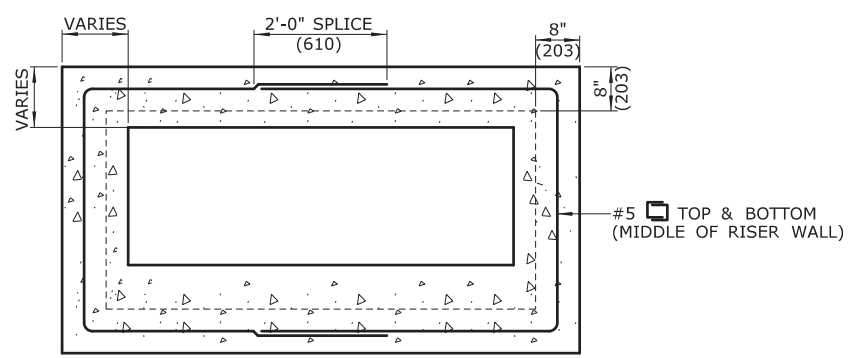
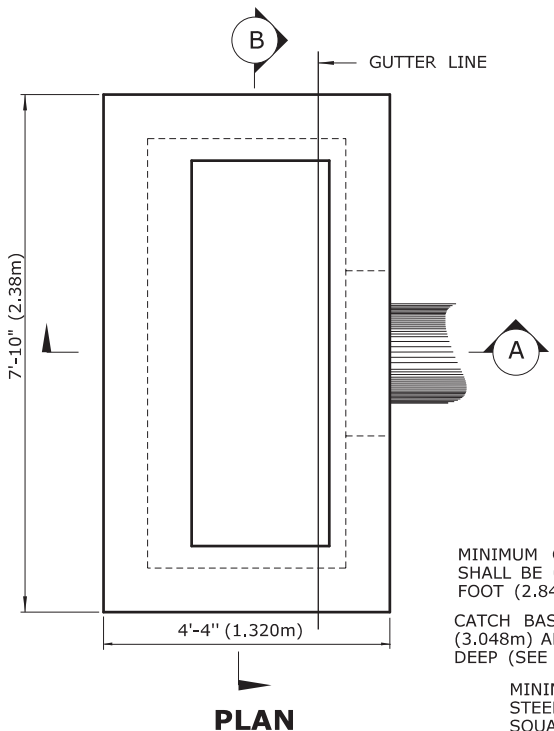
1. REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60.
2. DETAILS ON THIS SHEET SHOW STANDARD REINFORCEMENT. WELDED WIRE FABRIC WITH AN AREA EQUAL TO OR GREATER THAN THE REINFORCING SHOWN MAY BE SUBSTITUTED.
3. ALL LAP SPLICES, DEVELOPMENT LENGTHS, BENDS FOR REINFORCEMENT, AND WELDED WIRE FABRIC SHALL CONFORM TO AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.
4. ALL REINFORCEMENT SHALL HAVE A MINIMUM CLEAR COVER OF 2"(51) , EXCEPT FOR BENEATH BOTTOM REINFORCEMENT IN TOP SLABS, WHERE THE MINIMUM MAY BE 1 1/2"(38).
5. MINIMUM CONCRETE COMPRESSIVE STRENGTH  $F_c' = 4000$  PSI (27,580 kPa) SHALL BE OBTAINED PRIOR TO SHIPPING.
6. BASES AND RISERS AT A DEPTH OF 20' (6.096) AND GREATER SHALL BE DESIGNED BY THE CONTRACTOR AND WORKING DRAWINGS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.
7. SEE STANDARD SHEET HW-507.08 FOR CATCH BASIN FRAMES AND GRATES.
8. FOR DOT MAINTENANCE PERSONNEL, RISERS MAY BE PREFABRICATED WITH PIPE OPENINGS IN ALL FOUR WALLS. ADEQUATE REINFORCING AROUND PIPE OPENINGS CONFORMING TO THESE PLANS SHALL BE PROVIDED. ANY RISERS USED WHERE A PIPE OPENING IS TO REMAIN IN PLACE, MUST BE FORMED UP WITH BRICK AS DIRECTED BY THE ENGINEER.
9. RISERS SHALL NEVER HAVE CORNER PIPE ENTRIES. WHERE THE ALIGNMENT OF THE PIPE WITH RESPECT TO THE CORNER OF THE CATCH BASIN CANNOT BE CHANGED, A ROUND STRUCTURE CONFORMING TO ASTM C478 SHALL BE USED. REINFORCING FOR THE ROUND TOP SLAB WITH A RECTANGULAR OPENING SHALL CONFORM TO DETAILS SHOWN HERE.
10. ALL PIPE OPENINGS SHALL BE CLOSED USING MATERIALS WHICH CONFORM TO STATE OF CONNECTICUT STANDARD SPECIFICATIONS SECTION M.08.02. IF THE ENGINEER DETERMINES THAT THE CLOSURE OF ANY PIPE OPENING IS UNSATISFACTORY, THE CONTRACTOR SHALL RECLOSE SAID OPENING AT NO ADDITIONAL COST TO THE STATE. OPENING FOR PIPE SHALL NOT RESULT IN A REDUCED WALL THICKNESS.
11. THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
12. FOR ADDITIONAL DETAILS, SEE OTHER CATCH BASIN SHEETS.
13. WALL THICKNESS OF ALL CB'S OVER 10' (3.048m) DEEP SHALL BE INCREASED TO 12" (305) THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. (THE 12" (305) THICKNESS WILL START AFTER THE FIRST 10' (3.048m).)
14. BUTYL RUBBER JOINT SEAL SHALL CONFORM TO AASHTO M-198 AND MORTAR SHALL CONFORM TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M11.04.
15. SHRINKAGE AND TEMPERATURE REINFORCEMENT SHALL BE PROVIDED IN THE TOPS OF SLABS. THE TOTAL AREA OF REINFORCEMENT PROVIDED SHALL BE AT LEAST 0.125 SQUARE INCHES PER FOOT (0.8 sq cm PER METER) IN EACH DIRECTION. THE MAXIMUM SPACING OF THIS REINFORCEMENT SHALL NOT EXCEED 18 INCHES (457).
16. THE DETAILS SHOWN IN THE PLAN VIEW FOR PRECAST CONCRETE ROUND STRUCTURES SHALL ALSO BE USED FOR CONVERTING MANHOLES TO CATCH BASINS.



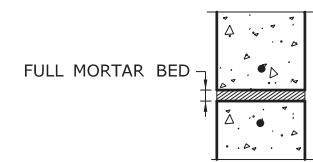
**SECTION D**  
**TOP SLAB TO CONVERT CATCH BASIN TO MANHOLE**  
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

<table border="1"> <tr> <th>REV.</th> <th>DATE</th> <th>REVISION DESCRIPTION</th> </tr> <tr> <td>2</td> <td>10/24/11</td> <td>CHANGE WORD KNOCKOUT TO OPENING &amp; ADD SPACER NOTE</td> </tr> <tr> <td>1</td> <td>6/01/10</td> <td>CHANGE NOTE 7 TO REFERENCE HW-507_08</td> </tr> </table>	REV.	DATE	REVISION DESCRIPTION	2	10/24/11	CHANGE WORD KNOCKOUT TO OPENING & ADD SPACER NOTE	1	6/01/10	CHANGE NOTE 7 TO REFERENCE HW-507_08	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. Plotted Date: 10/22/2011	NOT TO SCALE  <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: working_revisions.dgn Model: 9-HW-507_04	SUBMITTED BY: _____ NAME/DATE/TIME: _____ APPROVED BY: _____ NAME/DATE/TIME: _____	<b>CTDOT</b> <b>STANDARD SHEET</b> <b>OFFICE OF ENGINEERING</b>	STANDARD SHEET TITLE: <b>TYPE "C", "C-L" &amp; ROUND PRECAST CONCRETE CB</b>	STANDARD SHEET NO.: <b>HW-507_04</b>
REV.	DATE	REVISION DESCRIPTION													
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1	6/01/10	CHANGE NOTE 7 TO REFERENCE HW-507_08													

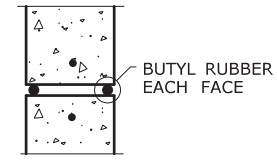




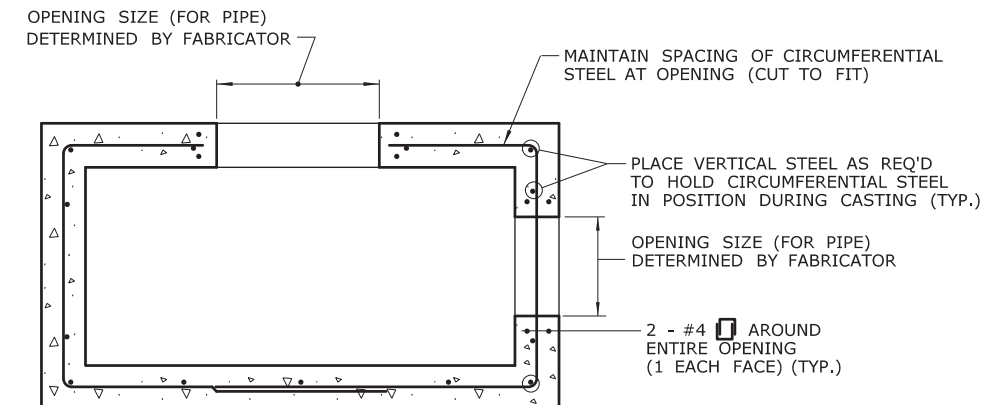
CATCH BASINS 10' (3.048m) DEEP OR LESS  
 MINIMUM CIRCUMFERENTIAL STEEL AREA SHALL BE 0.44 SQUARE INCHES PER FOOT (2.84 sq cm per Meters)  
 CATCH BASINS GREATER THAN 10' (3.048m) AND LESS THAN 20' (6.096m) DEEP (SEE NOTES 6 AND 13)  
 MINIMUM CIRCUMFERENTIAL STEEL AREA SHALL BE 0.44 SQUARE INCHES PER FOOT (2.84sq cm per Meter)



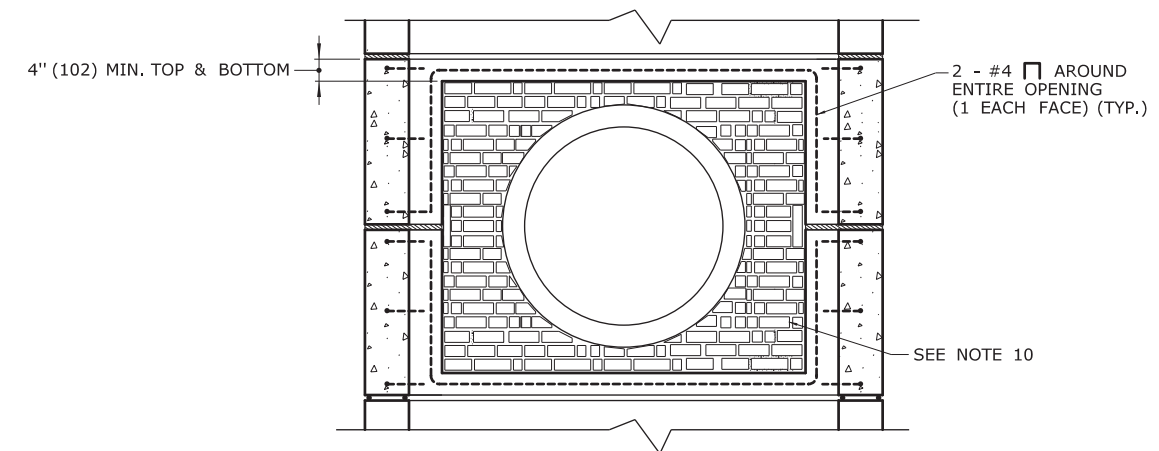
**MORTAR JOINT DETAIL**



**BUTYL RUBBER JOINT DETAIL**



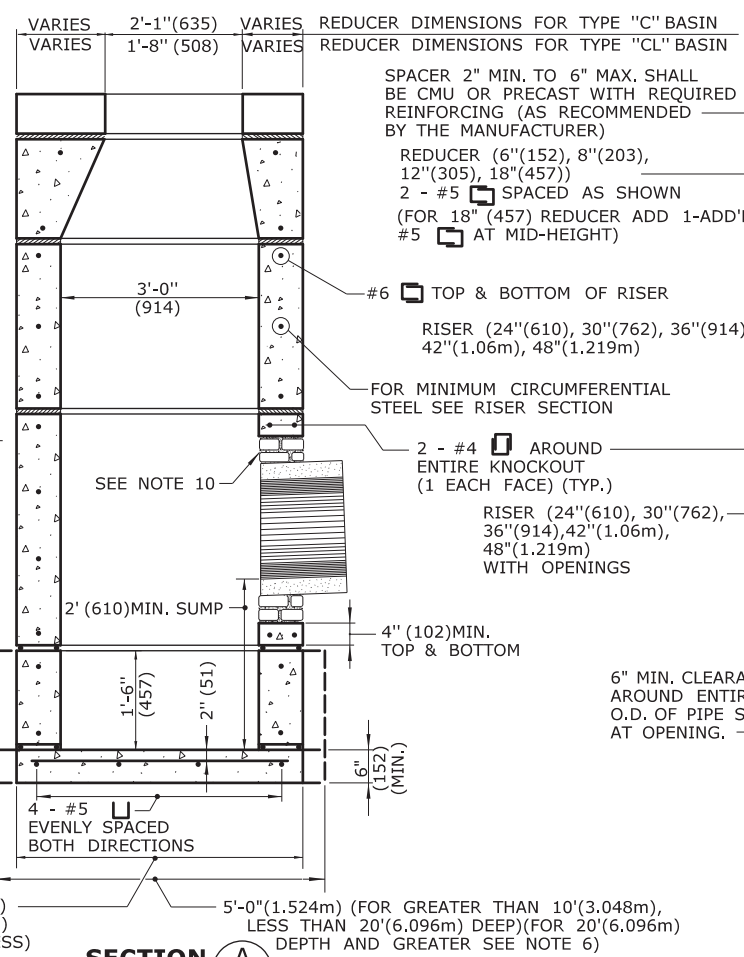
**TYPICAL SECTION THRU SINGLE RISER WITH OPENINGS**



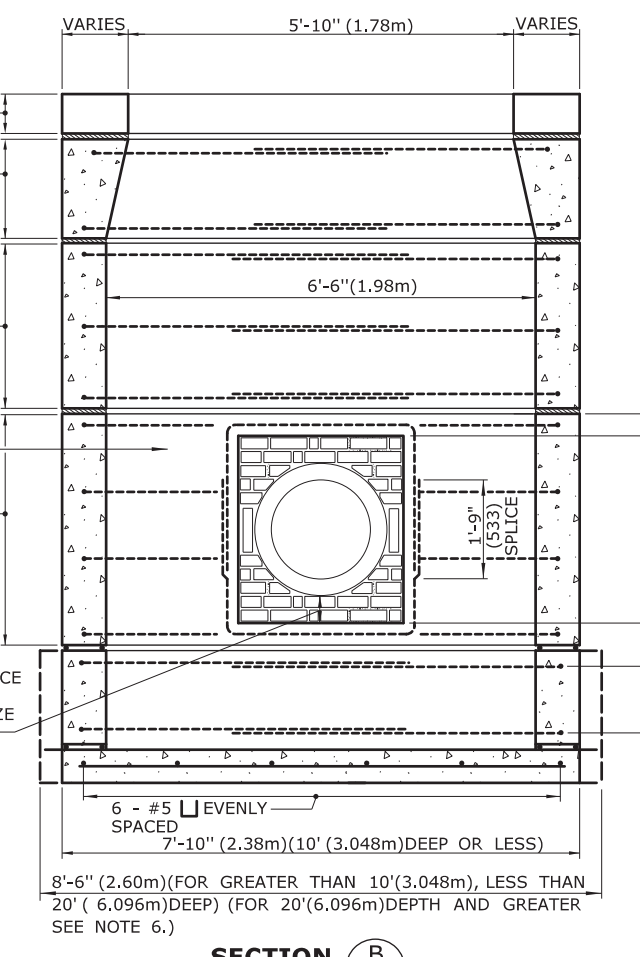
**DOUBLE RISER OPENING (TYP.) PIPES GREATER THAN 24" (610) O.D.**

**GENERAL NOTES:**

1. REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60.
2. DETAILS ON THIS SHEET SHOW STANDARD REINFORCEMENT. WELDED WIRE FABRIC WITH AN AREA EQUAL TO OR GREATER THAN THE REINFORCING SHOWN MAY BE SUBSTITUTED.
3. ALL LAP SPLICES, DEVELOPMENT LENGTHS, BENDS FOR REINFORCEMENT, AND WELDED WIRE FABRIC SHALL CONFORM TO AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.
4. ALL REINFORCEMENT SHALL HAVE A MINIMUM CLEAR COVER OF 2" (51), EXCEPT FOR BENEATH BOTTOM REINFORCEMENT IN TOP SLABS, WHERE THE MINIMUM MAY BE 1 1/2" (38).
5. MINIMUM CONCRETE COMPRESSIVE STRENGTH  $F_c' = 4000$  PSI (27.580 kPa) SHALL BE OBTAINED PRIOR TO SHIPPING.
6. BASES AND RISERS AT A DEPTH OF 20' (6.096m) AND GREATER SHALL BE DESIGNED BY THE CONTRACTOR AND WORKING DRAWINGS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.
7. SEE STANDARD SHEET HW-507\_08 FOR CATCH BASIN FRAMES AND GRATES.
8. FOR DOT MAINTENANCE PERSONNEL, RISERS MAY BE PREFABRICATED WITH PIPE OPENINGS IN ALL FOUR WALLS. ADEQUATE REINFORCING AROUND PIPE OPENINGS CONFORMING TO THESE PLANS SHALL BE PROVIDED. ANY RISERS USED WHERE A PIPE OPENING IS TO REMAIN IN PLACE, MUST BE FORMED UP WITH BRICK AS DIRECTED BY THE ENGINEER.
9. RISERS SHALL NEVER HAVE CORNER PIPE ENTRIES. WHERE THE ALIGNMENT OF THE PIPE WITH RESPECT TO THE CORNER OF THE CATCH BASIN CANNOT BE CHANGED, A ROUND STRUCTURE CONFORMING TO ASTM C478 SHALL BE USED. REINFORCING FOR THE ROUND TOP SLAB WITH A RECTANGULAR OPENING SHALL CONFORM TO DETAILS SHOWN HERE.
10. ALL PIPE OPENINGS SHALL BE CLOSED USING MATERIALS WHICH CONFORM TO STATE OF CONNECTICUT STANDARD SPECIFICATIONS SECTION M.08.02. IF THE ENGINEER DETERMINES THAT THE CLOSURE OF ANY PIPE OPENING IS UNSATISFACTORY, THE CONTRACTOR SHALL RECLOSE SAID OPENING AT NO ADDITIONAL COST TO THE STATE. OPENINGS FOR PIPE SHALL NOT RESULT IN A REDUCED WALL THICKNESS.
11. THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
12. FOR ADDITIONAL DETAILS, SEE OTHER CATCH BASIN SHEETS.
13. WALL THICKNESS OF ALL CB'S OVER 10' (3.048m) DEEP SHALL BE INCREASED TO 12" (305) THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. (THE 12" (305) THICKNESS WILL START AFTER THE FIRST 10' (3.048m).)
14. BUTYL RUBBER JOINT SEAL SHALL CONFORM TO AASHTO M-198 AND MORTAR SHALL CONFORM TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M11.04.



**SECTION A**



**SECTION B**

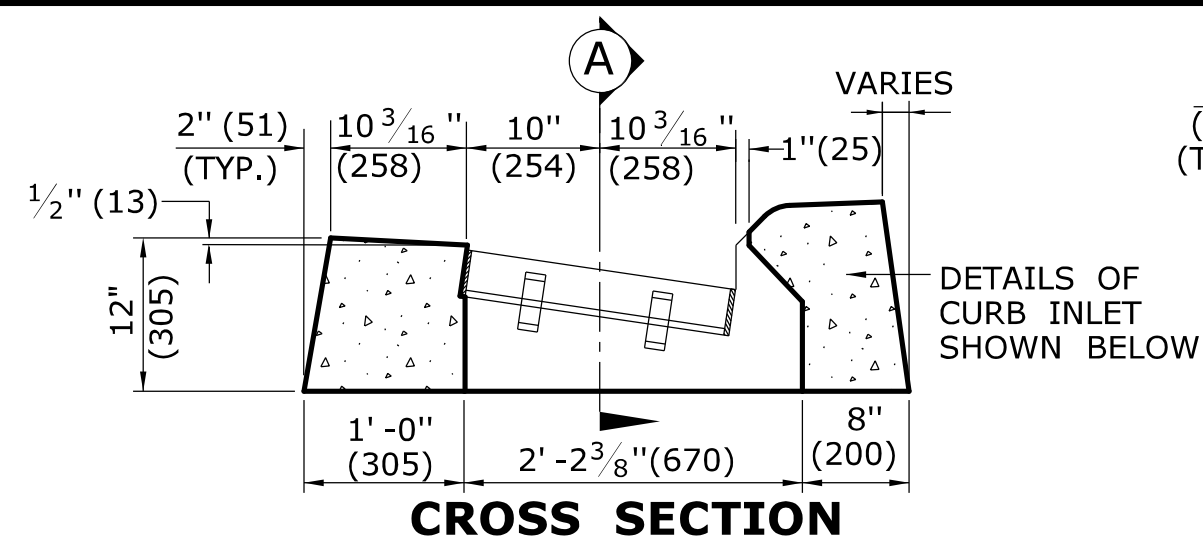
NOTE: REINFORCEMENT IN FAR FACE WALL NOT SHOWN FOR CLARITY

**PRECAST CONCRETE TYPE "C" & "C-L" DOUBLE GRATE TYPE II CATCH BASIN**  
 (UNDER 10' (3.048m) DEEP SHOWN)

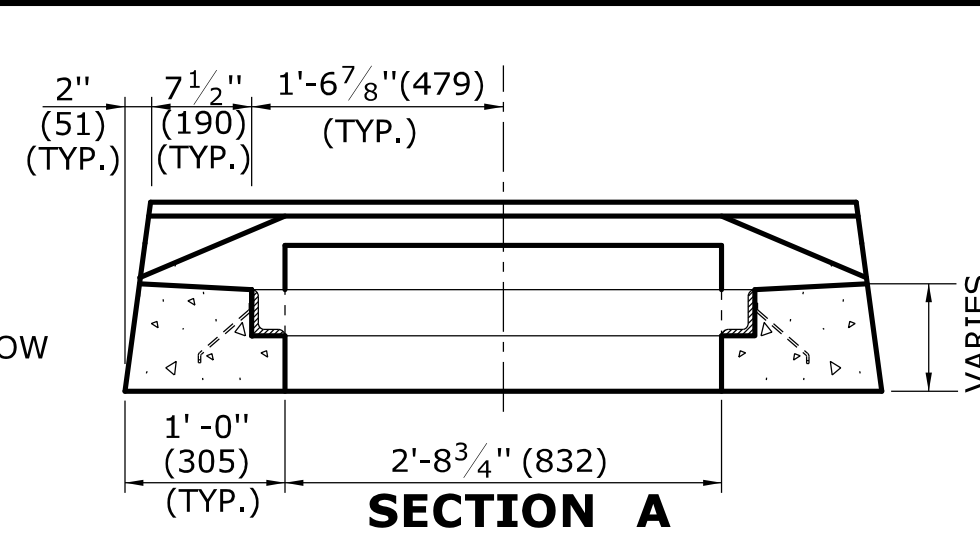
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. Plotted Date: 10/22/2011		NOT TO SCALE	<p><b>STATE OF CONNECTICUT</b>  <b>DEPARTMENT OF TRANSPORTATION</b></p>	SUBMITTED BY: NAME/DATE/TIME: Leo Fontaine 2011.11.03 11:46:39 -04'00'	<b>CTDOT</b> <b>STANDARD SHEET</b> <b>OFFICE OF ENGINEERING</b>	STANDARD SHEET TITLE: <b>TYPE "C" &amp; "C-L" PRECAST CONCRETE</b> <b>CB DOUBLE GRATE TYPE-II</b>	STANDARD SHEET NO.: <b>HW-507_06</b>
REV. DATE 2 10/24/11 1 6/01/10	REVISION DESCRIPTION CHANGE WORD KNOCKOUT TO OPENING & ADD SPACER NOTE CHANGE NOTE 7 TO REFERENCE HW-507_08	Filename: wor\ing-revis\slons.dgn Model: 11-HW-507_06	APPROVED BY: NAME/DATE/TIME: James H. Norman 2011.11.10 07:59:36 -05'00'	REV. DATE 2 10/24/11 1 6/01/10		REVISION DESCRIPTION CHANGE WORD KNOCKOUT TO OPENING & ADD SPACER NOTE CHANGE NOTE 7 TO REFERENCE HW-507_08	STANDARD SHEET TITLE: <b>TYPE "C" &amp; "C-L" PRECAST CONCRETE</b> <b>CB DOUBLE GRATE TYPE-II</b>

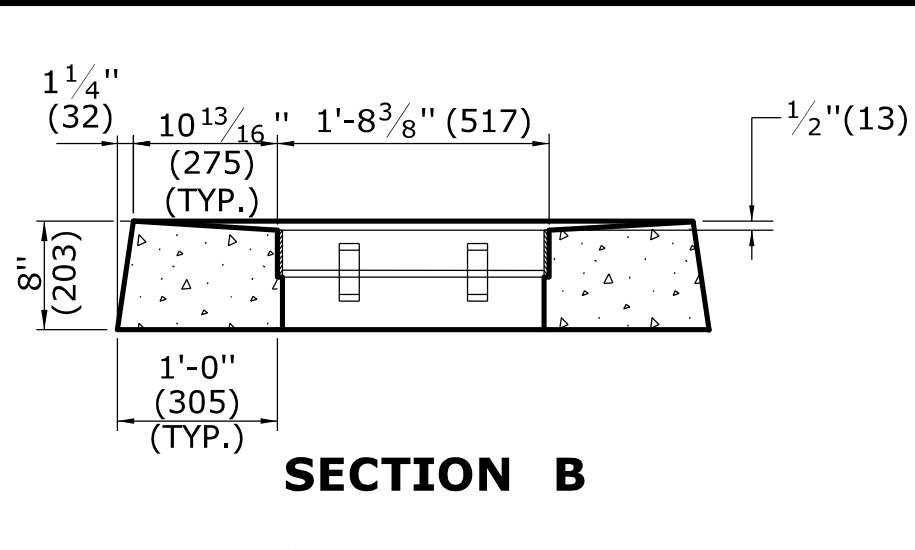
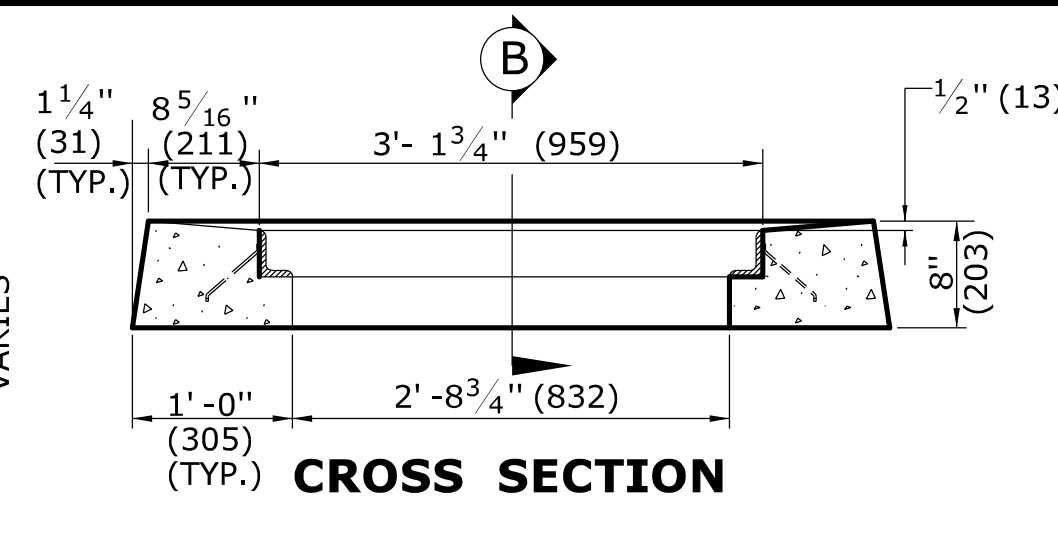




**CROSS SECTION**  
**TYPE "C" CATCH BASIN TOP**

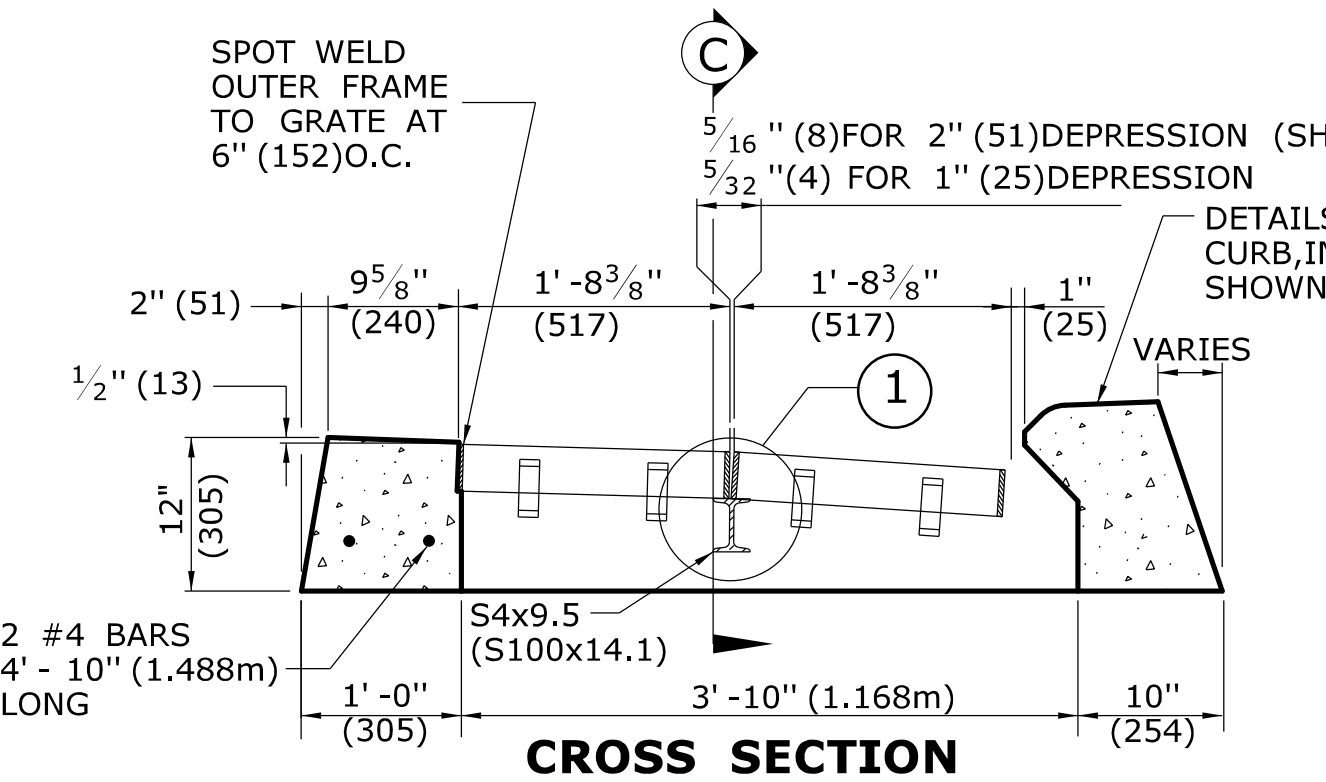


**SECTION A**  
**CROSS SECTION**  
**TYPE "C-L" CATCH BASIN TOP**

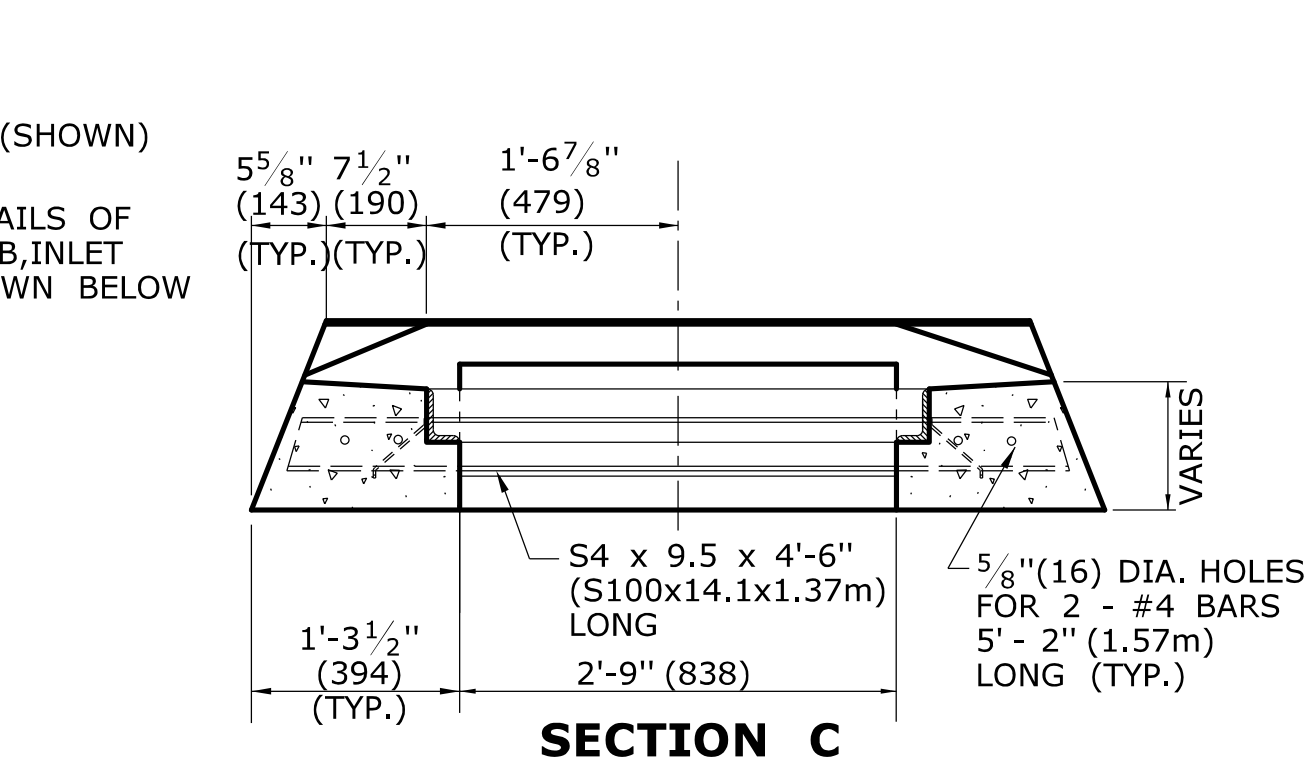


**SECTION B**  
**CROSS SECTION**  
**TYPE "C-L" CATCH BASIN TOP**

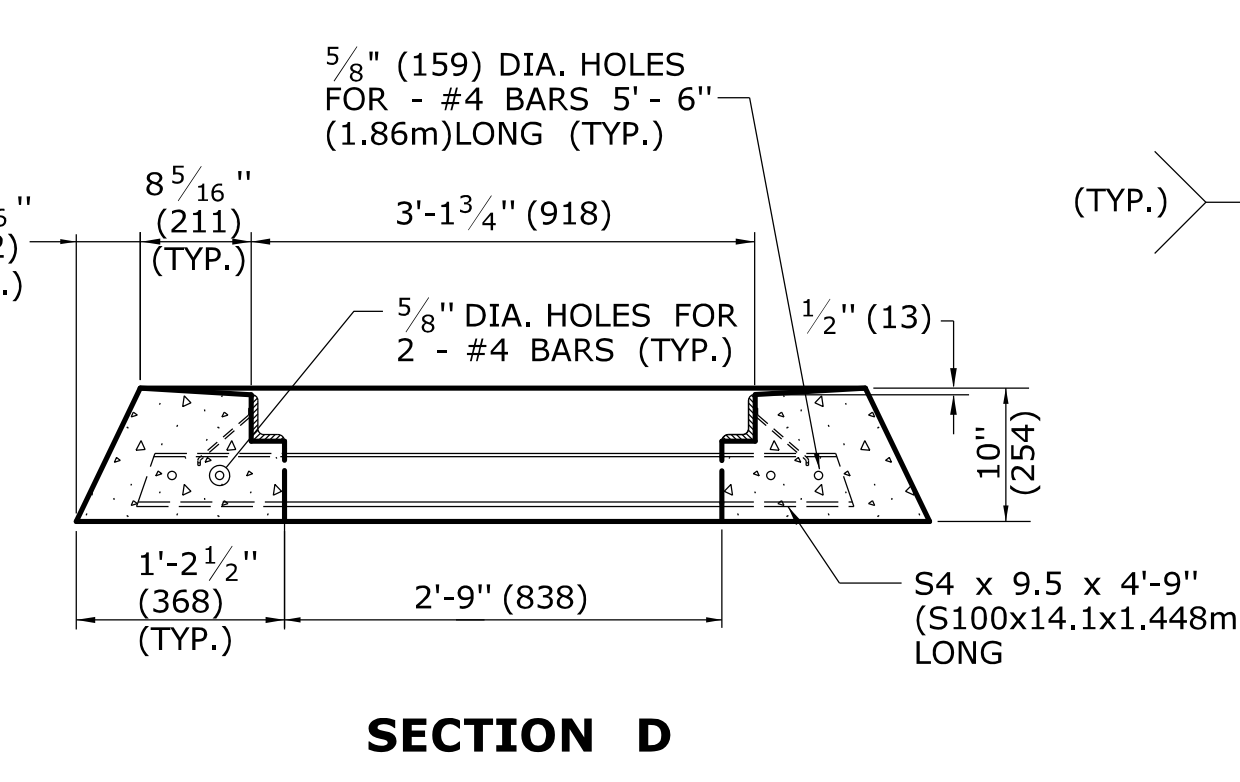
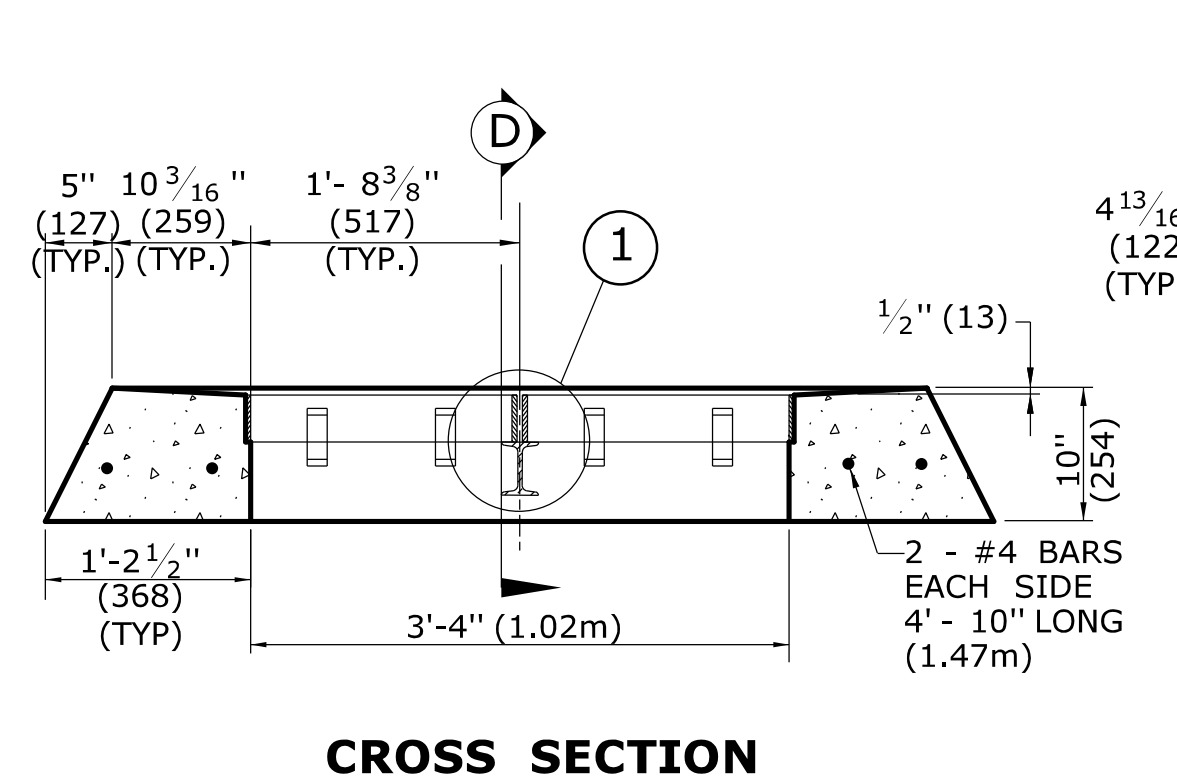
- GENERAL NOTES:**
1. FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507-08.
  2. ALL STEEL, EXCEPT REINFORCING BARS, SHALL BE GALVANIZED IN CONFORMANCE WITH SECTION M06.03 OF CONNECTICUT'S STANDARD SPECIFICATIONS.
  3. ALL BARS SHALL HAVE A MINIMUM 2" (51) COVER.



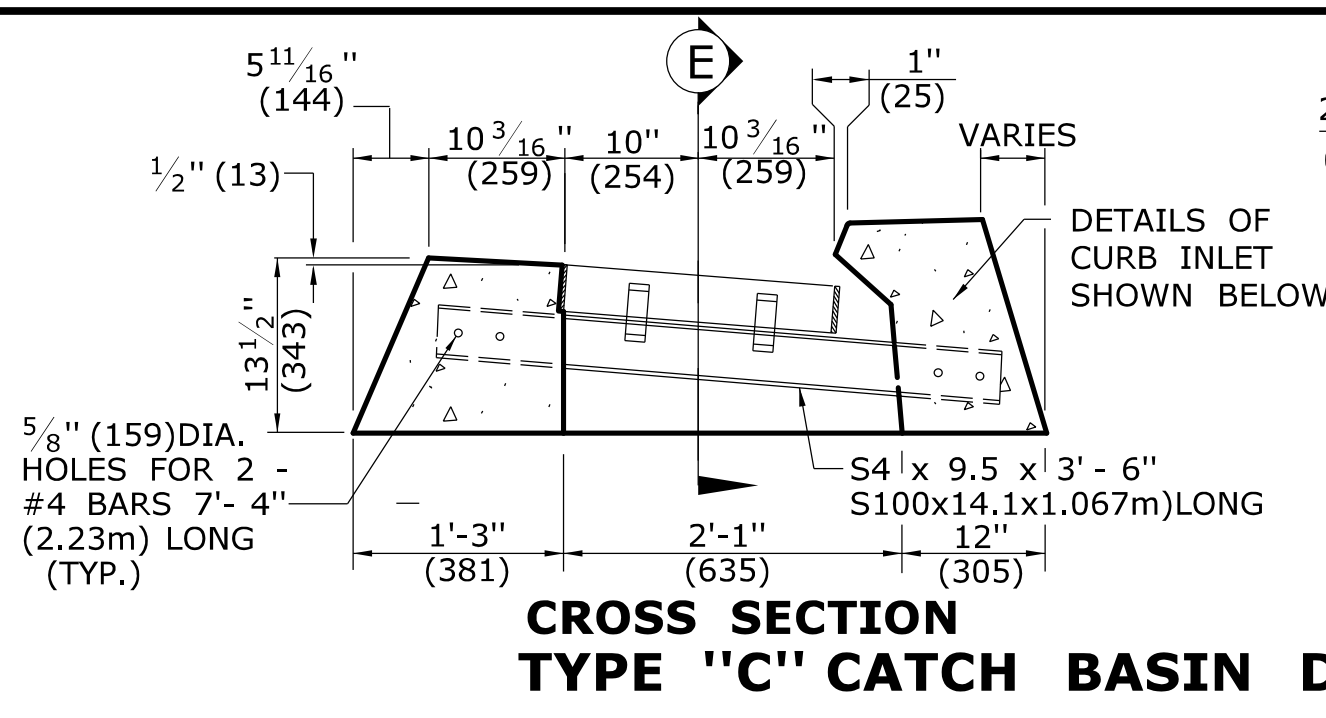
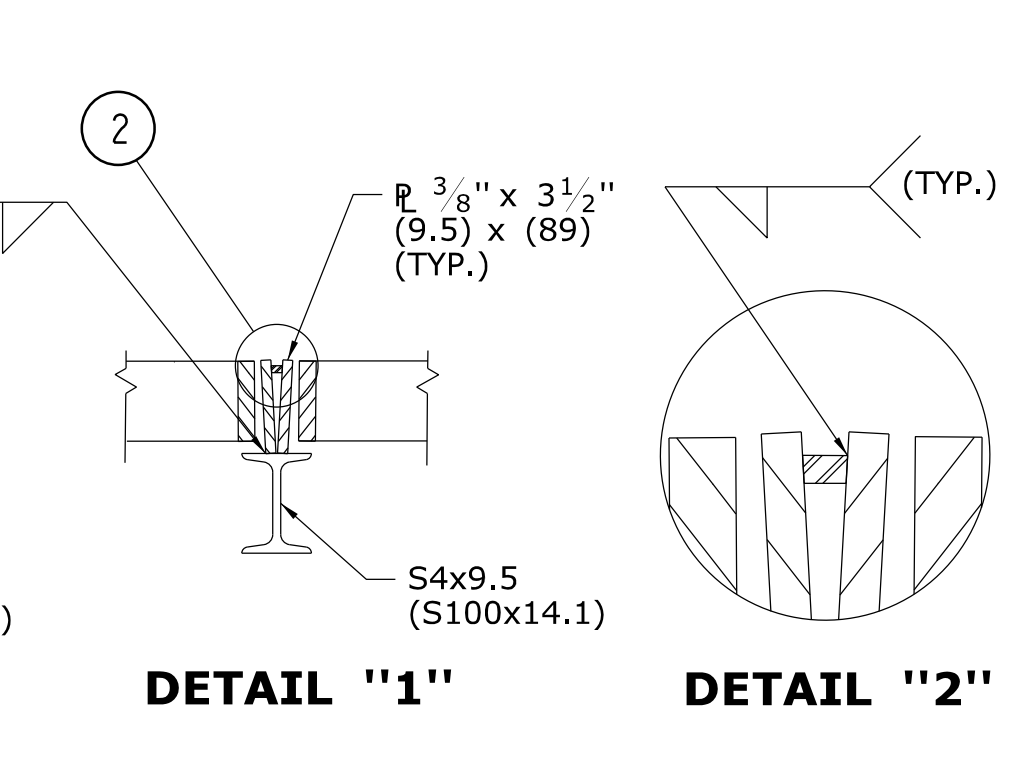
**CROSS SECTION**  
**TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE I TOP**



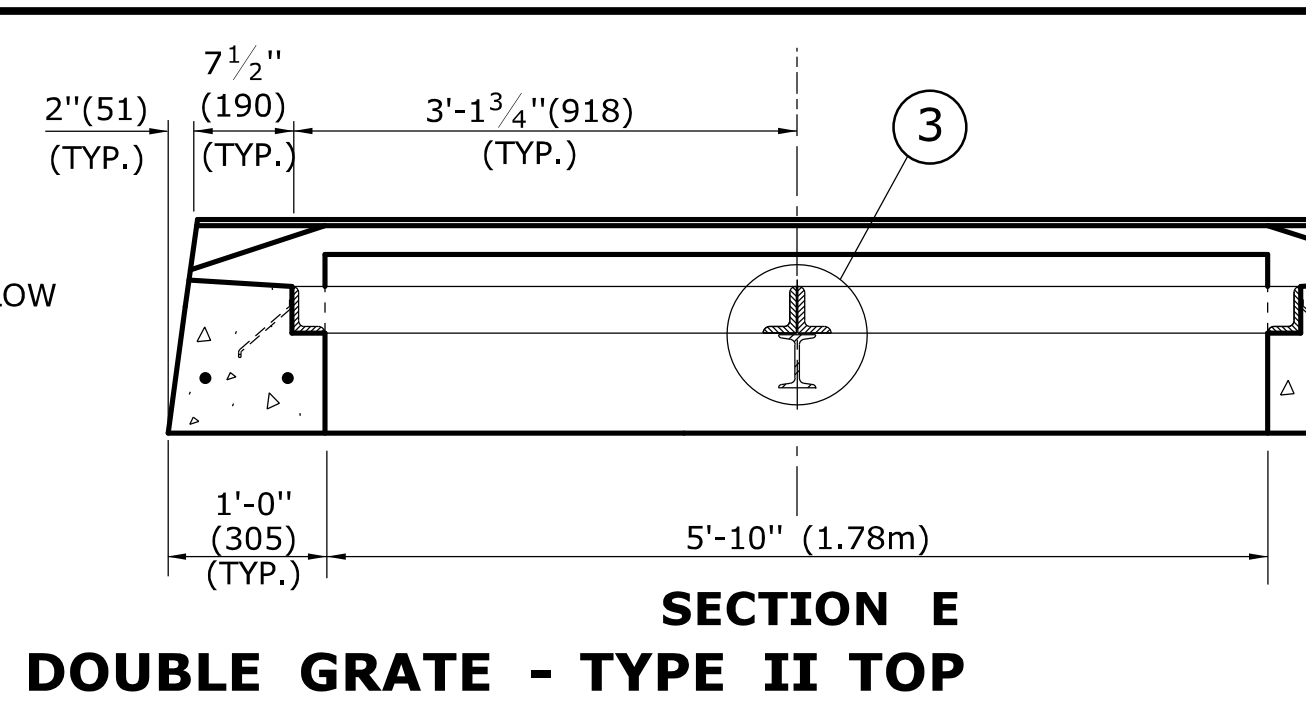
**SECTION C**  
**CROSS SECTION**  
**TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE I TOP**



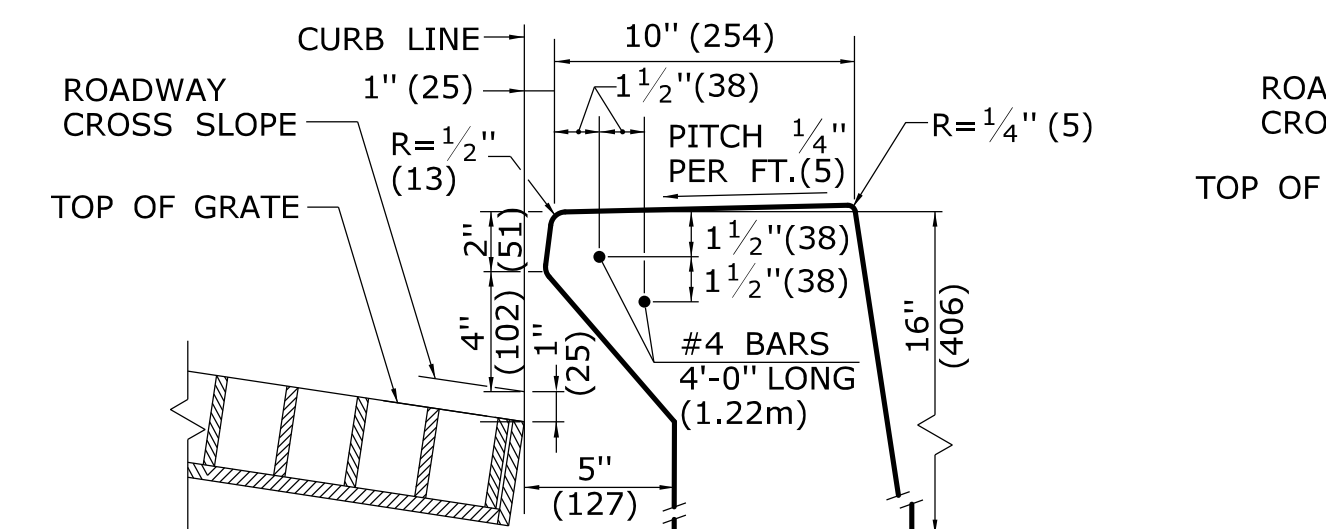
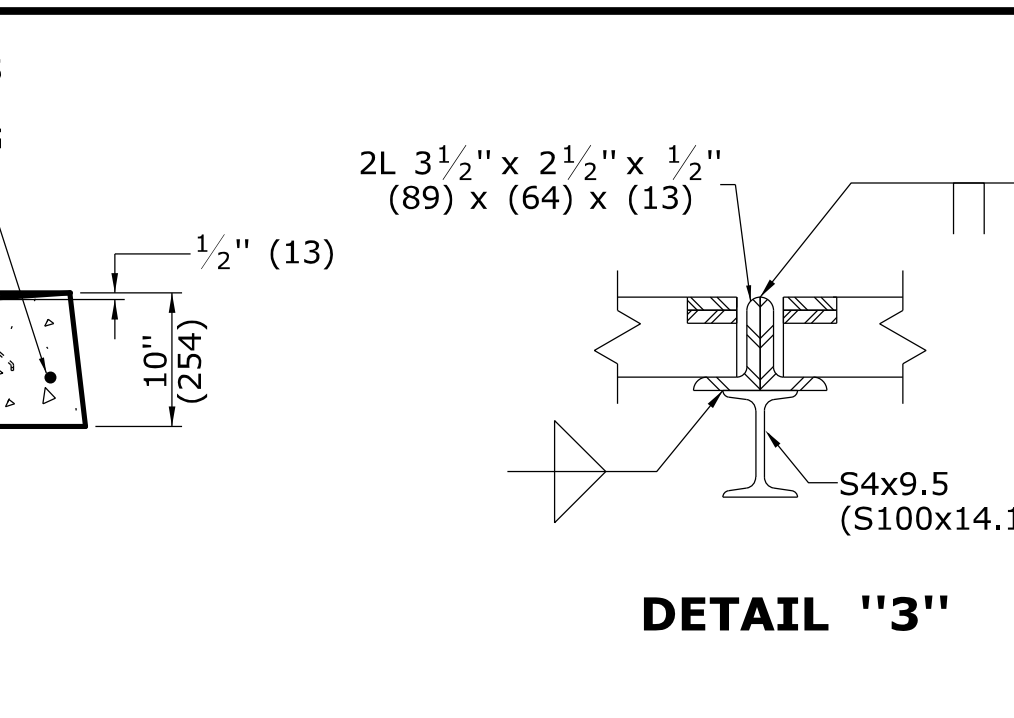
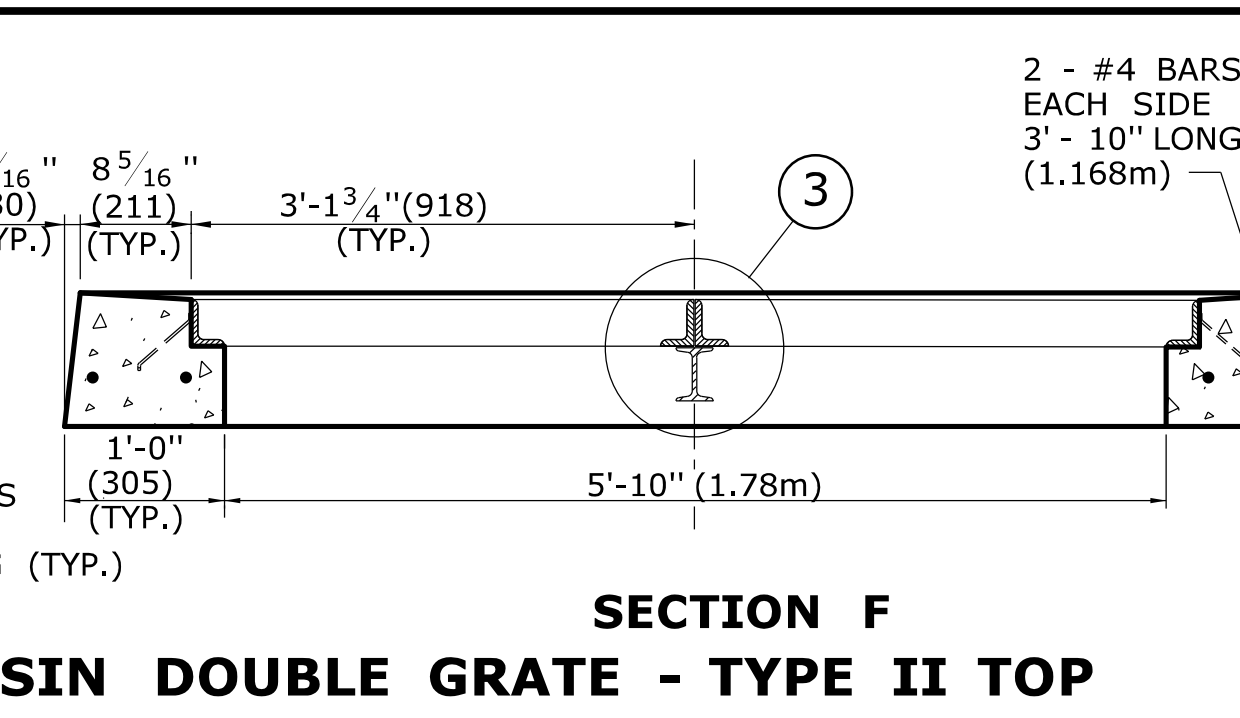
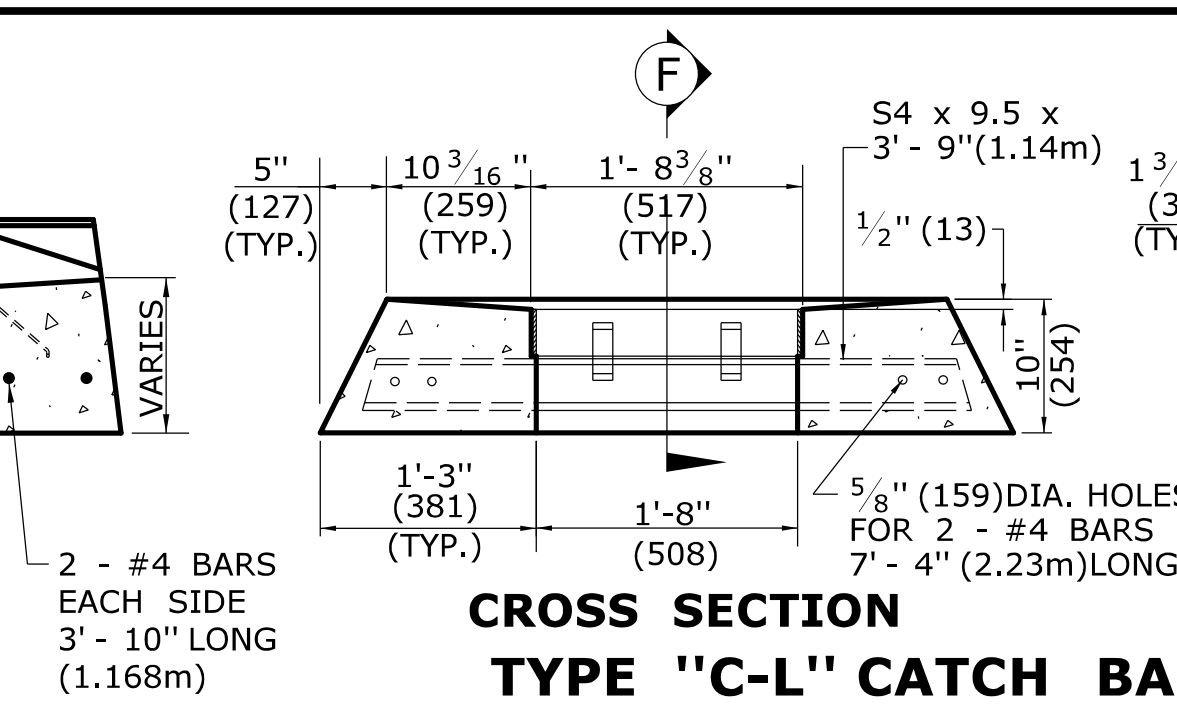
**SECTION D**  
**CROSS SECTION**  
**TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE I TOP**



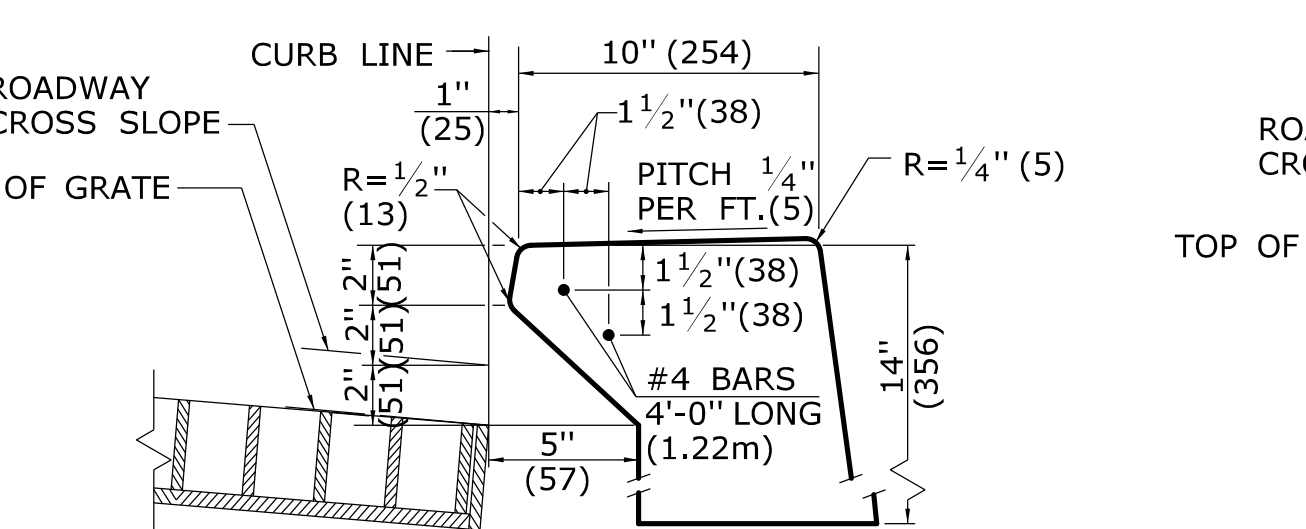
**CROSS SECTION**  
**TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE II TOP**



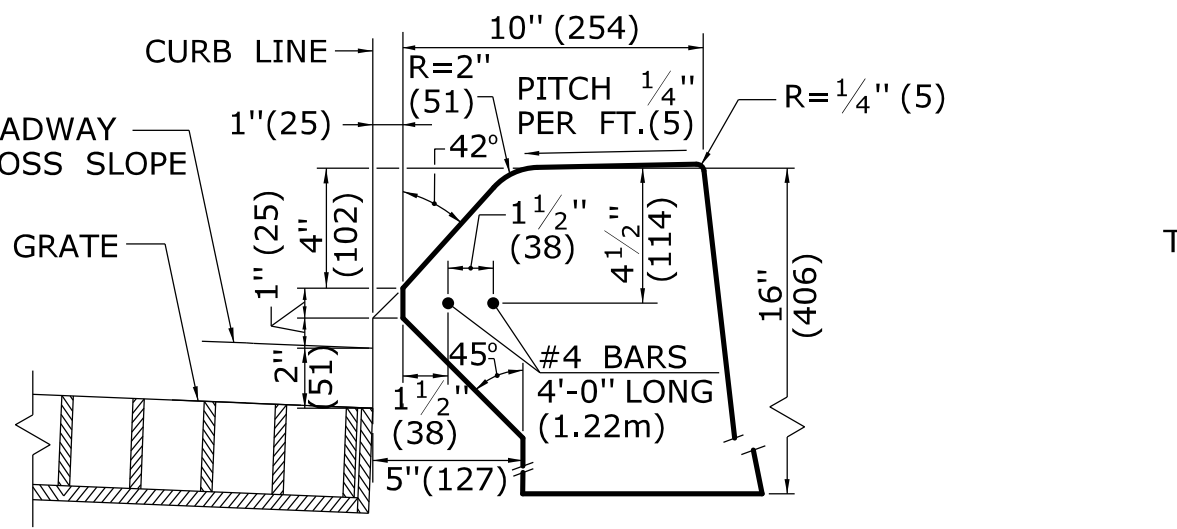
**SECTION E**  
**CROSS SECTION**  
**TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE II TOP**



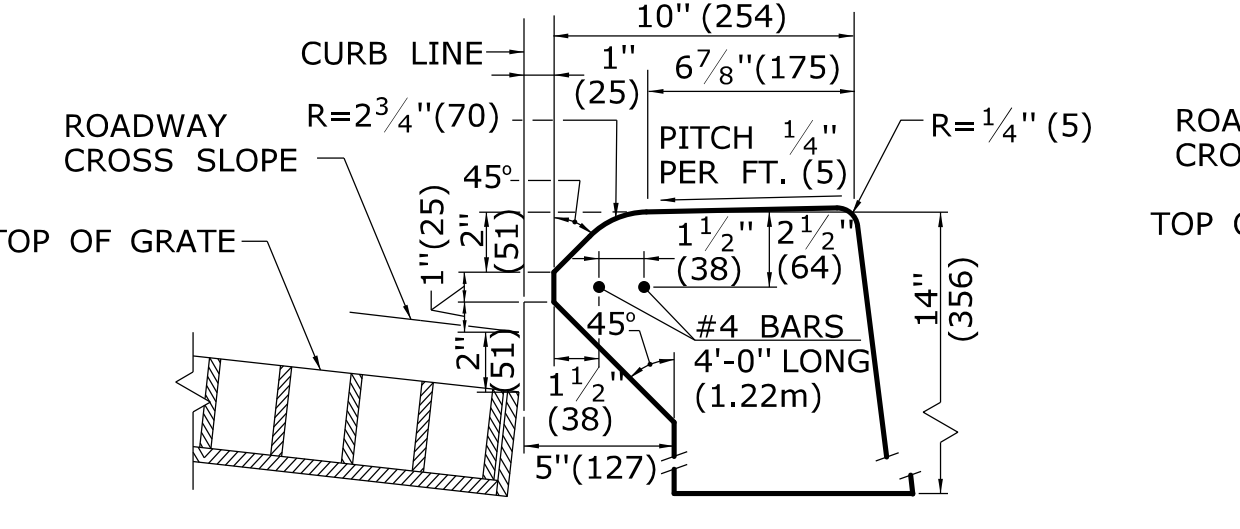
**INLET WITH 6" (152) CONCRETE OR STONE CURBING FOR TYPE "C" CB**



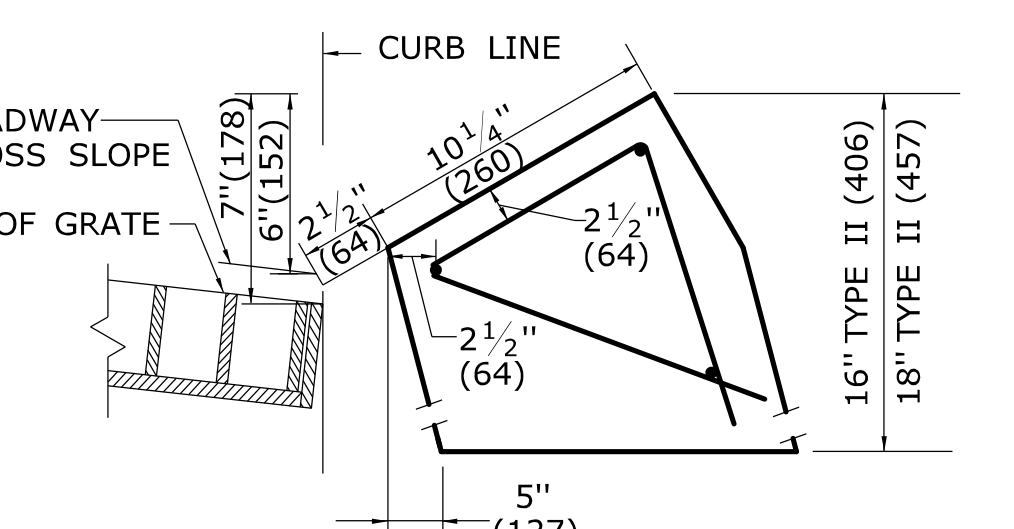
**INLET WITH NO CURBING (PLAIN TYPE) FOR TYPE "C" CB**



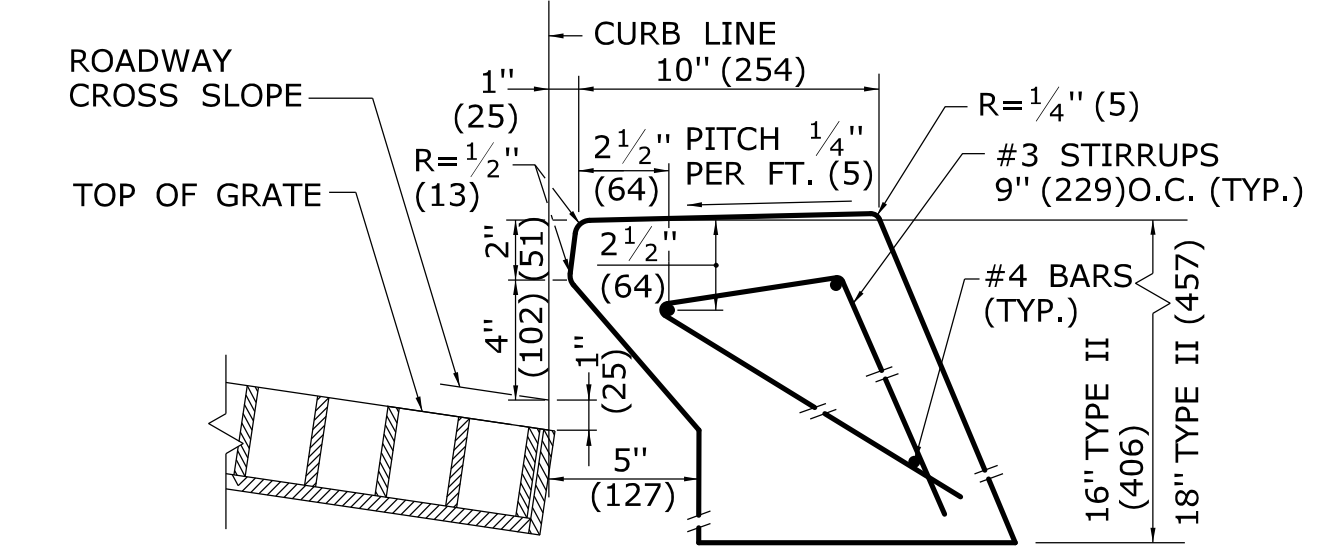
**INLET WITH 6" (152) BITUMINIOUS CONCRETE LIP CURBING FOR TYPE "C" CB**



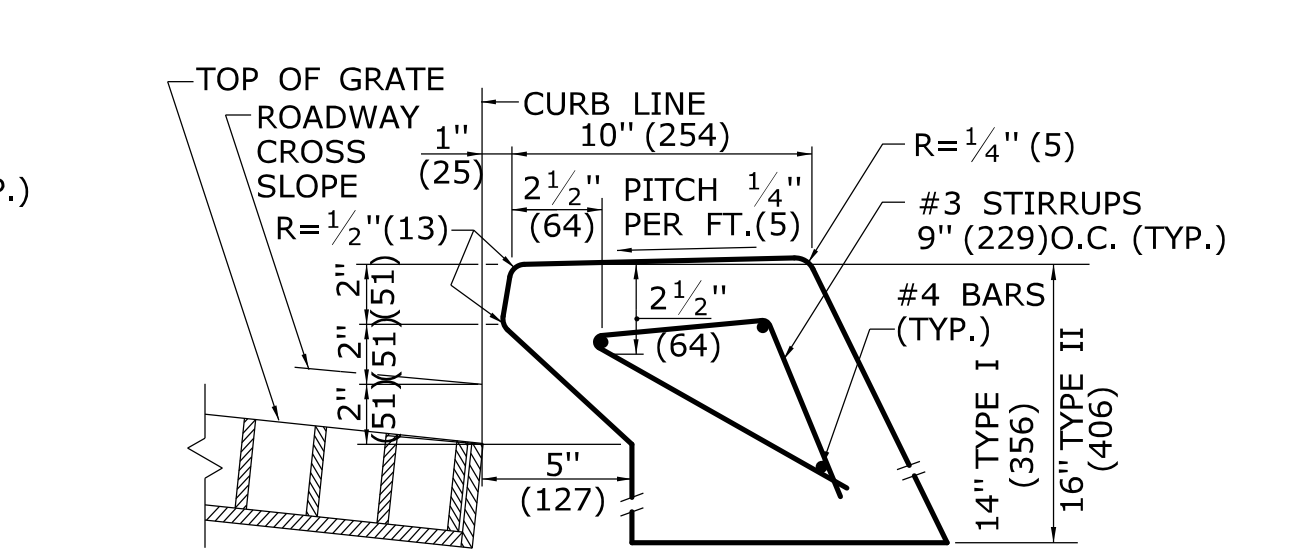
**INLET WITH 4" (102) CONCRETE PARK CURBING FOR TYPE "C" CB**



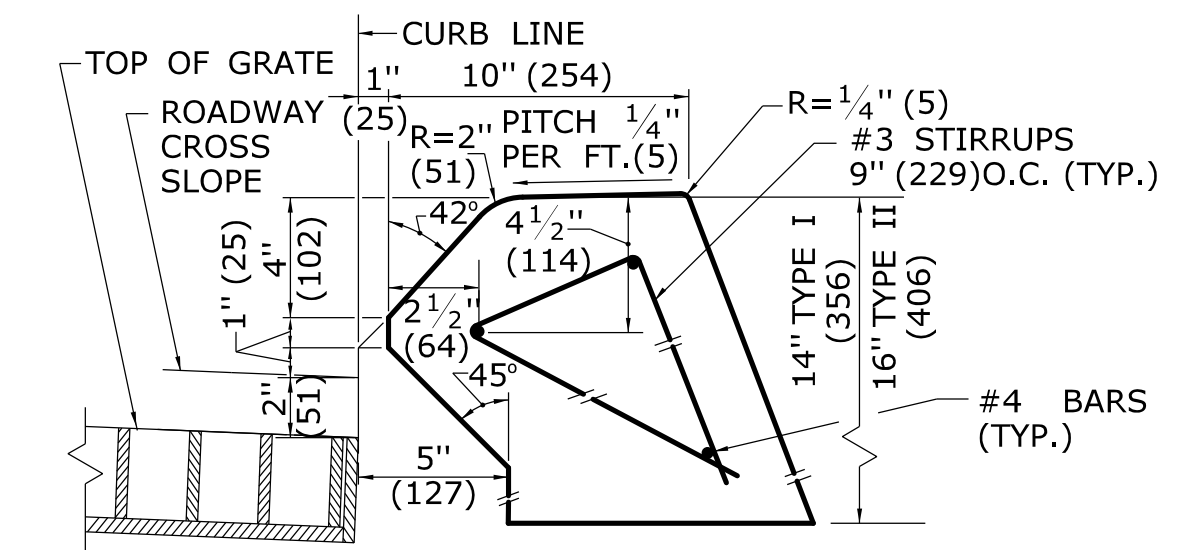
**INLET WITH GRANITE SLOPE CURB FOR TYPE "C" CB**



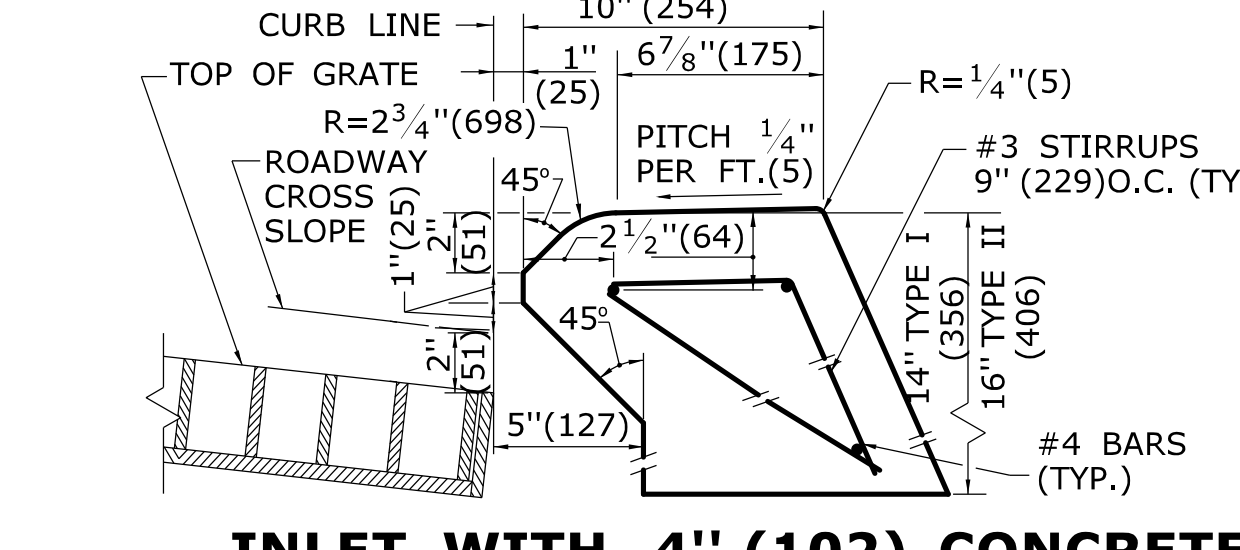
**INLET WITH 6" (152) CONCRETE OR STONE CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II**



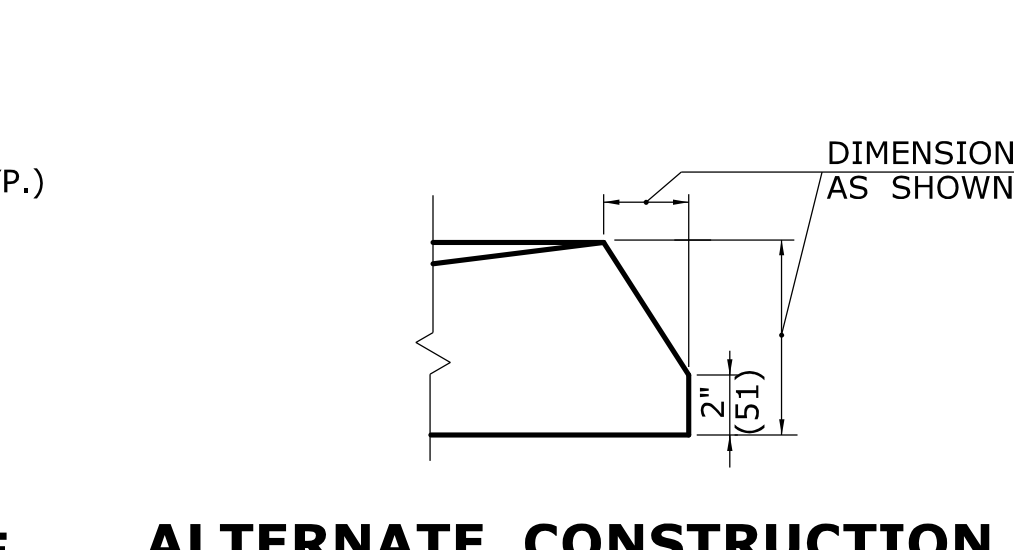
**INLET WITH NO CURBING (PLAIN TYPE) FOR TYPE "C" CB DOUBLE GRATE TYPE I & II**



**INLET WITH 6" (152) BITUMINIOUS CONCRETE LIP CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II**



**INLET WITH 4" (102) CONCRETE PARK CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II**



**ALTERNATE CONSTRUCTION OF TYPE II TOP**

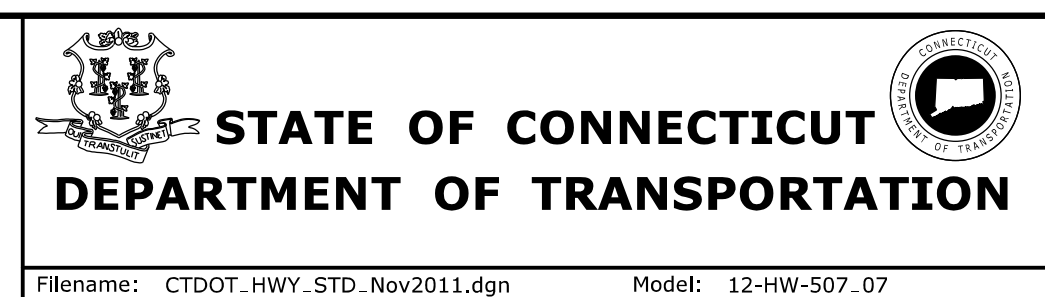
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
2	7/28/11	REMOVE MIN. DROP NOTE
1	6/01/10	REVISE CALL-OUT

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 11/10/2011

NOT TO SCALE



SUBMITTED BY: \_\_\_\_\_ NAME/DATE/TIME: \_\_\_\_\_

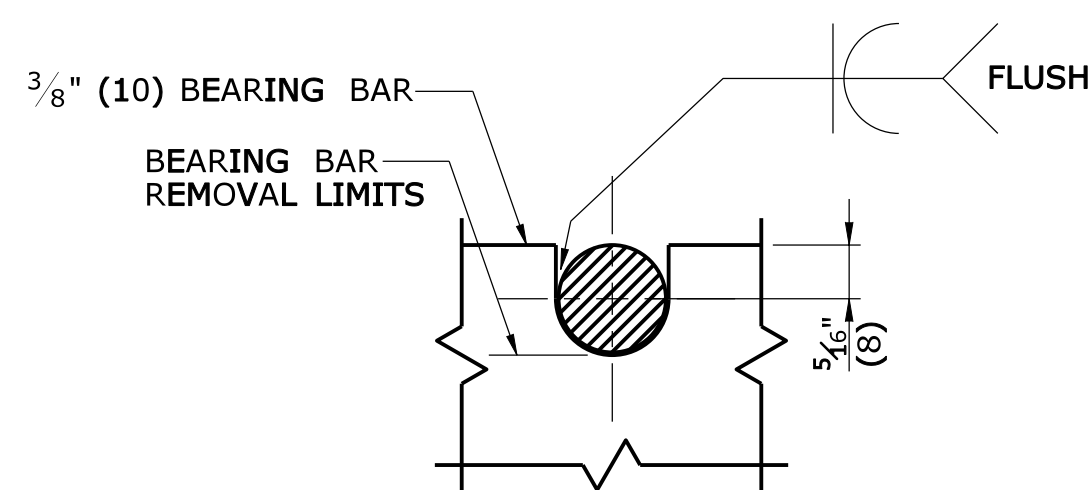
APPROVED BY: \_\_\_\_\_ NAME/DATE/TIME: \_\_\_\_\_

CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:  
**TYPE "C" & "C-L" CATCH BASIN TOPS AND CURBS**

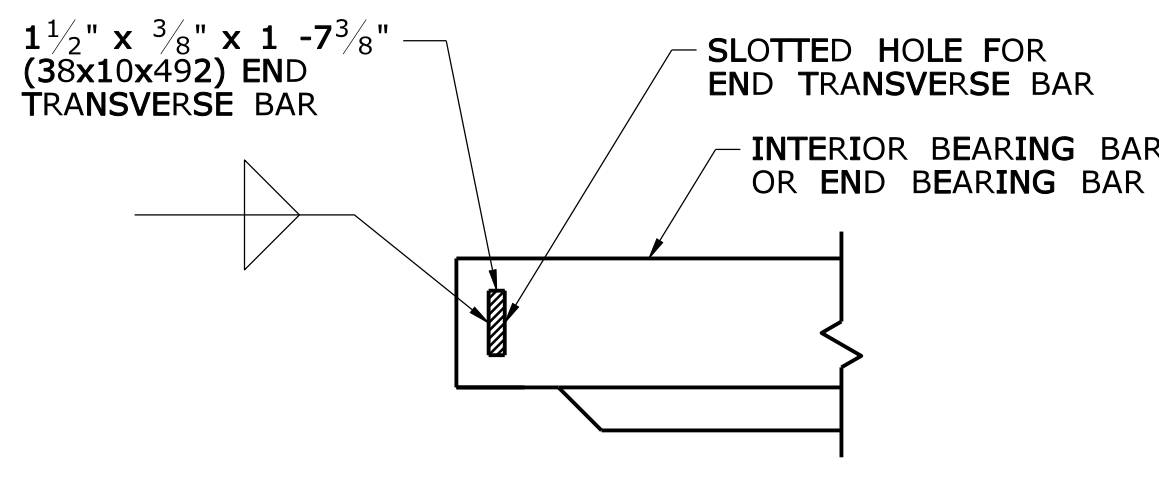
STANDARD SHEET NO.:  
**HW-507\_07**



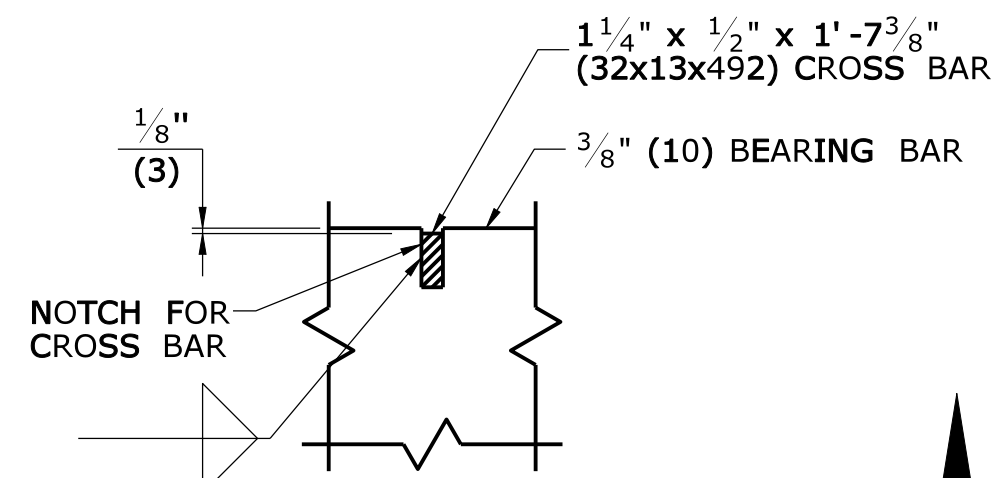


NOTE:  
5/8" (16) DIA. ROUND BAR SHALL CONTACT BEARING BAR AT BOTTOM AND BE FLUSH AT TOP.

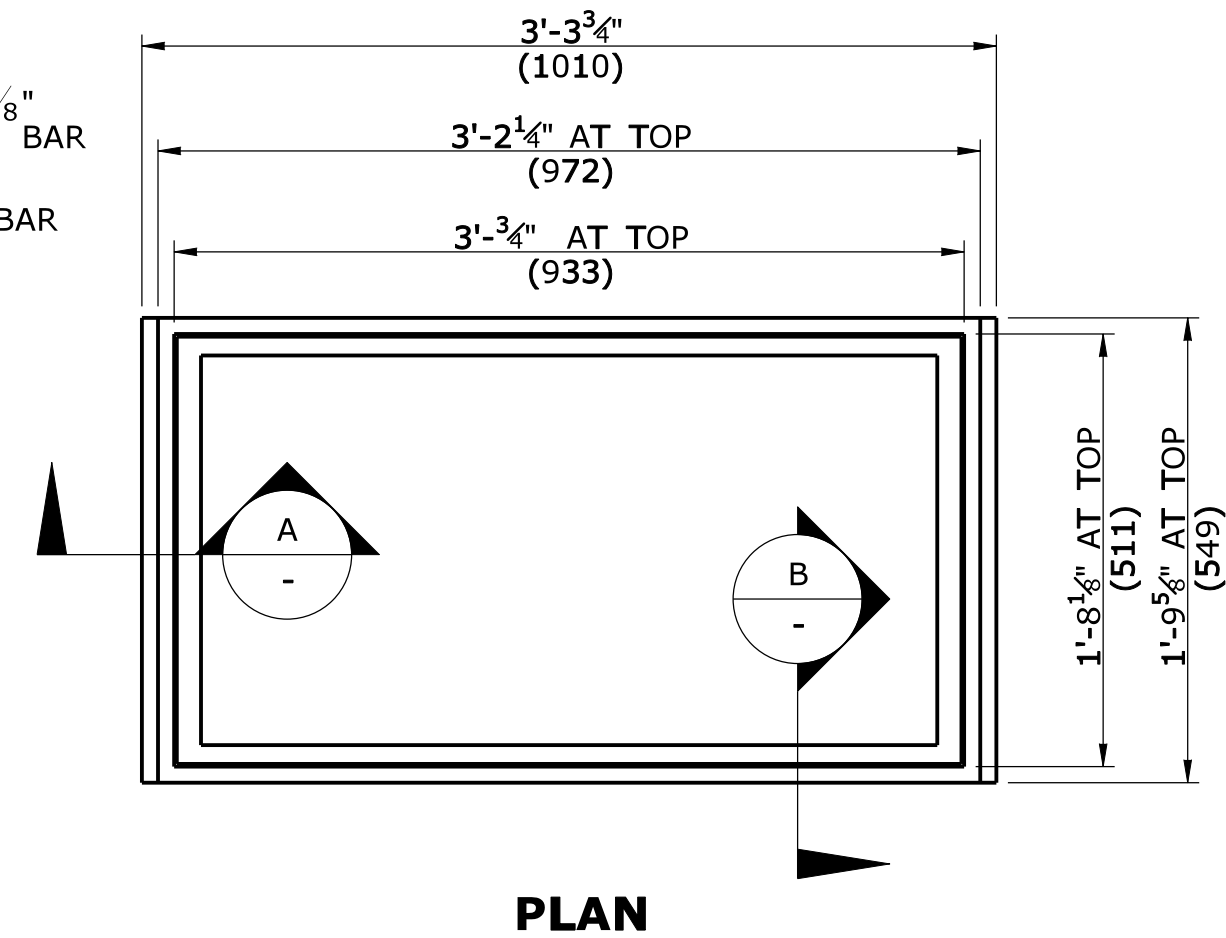
**ROUND BAR ATTACHMENT  
CATCH BASIN GRATE TYPE A**



**END TRANSVERSE BAR ATTACHMENT  
CATCH BASIN GRATE TYPE A & B**



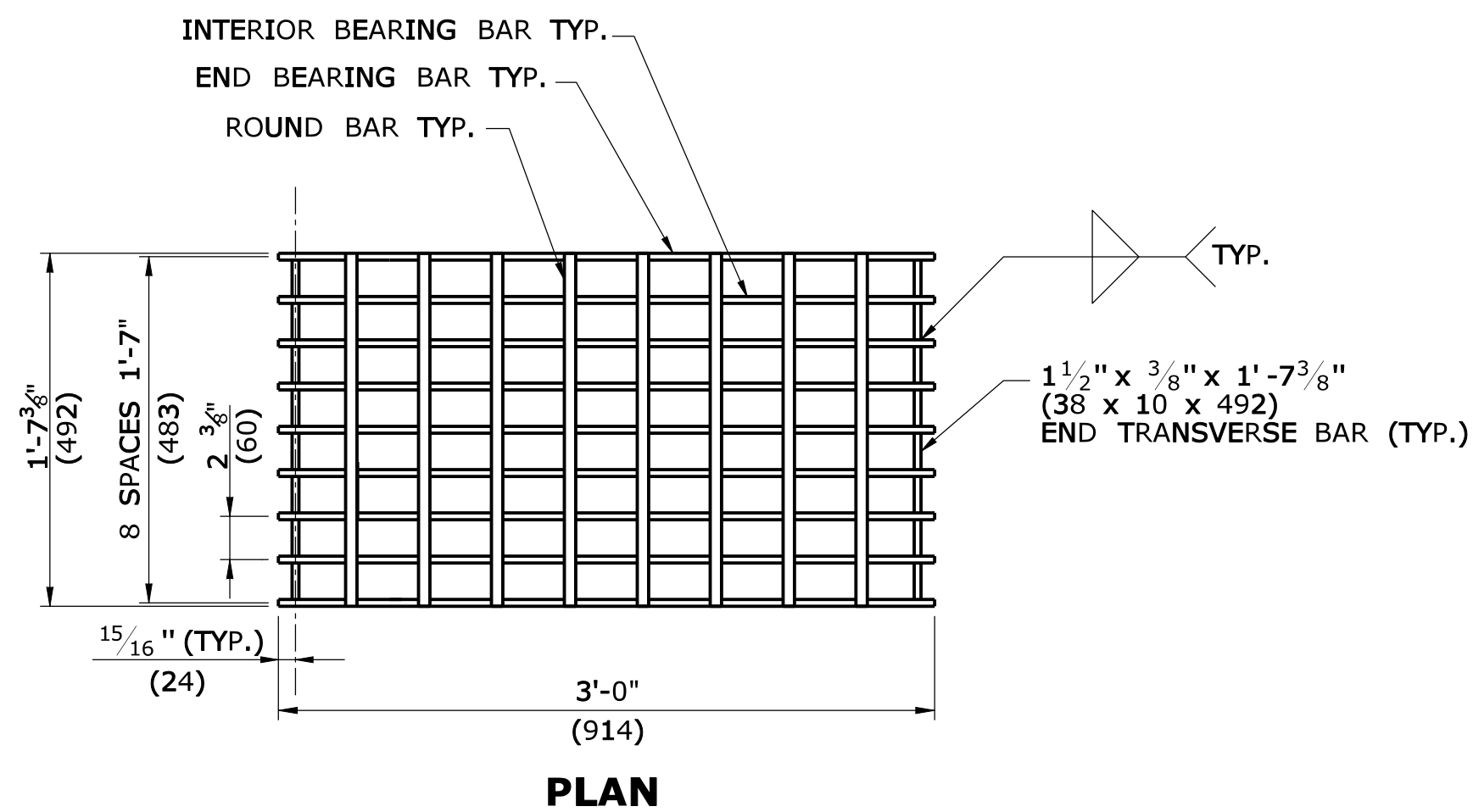
**CROSS BAR ATTACHMENT  
CATCH BASIN GRATE TYPE B**



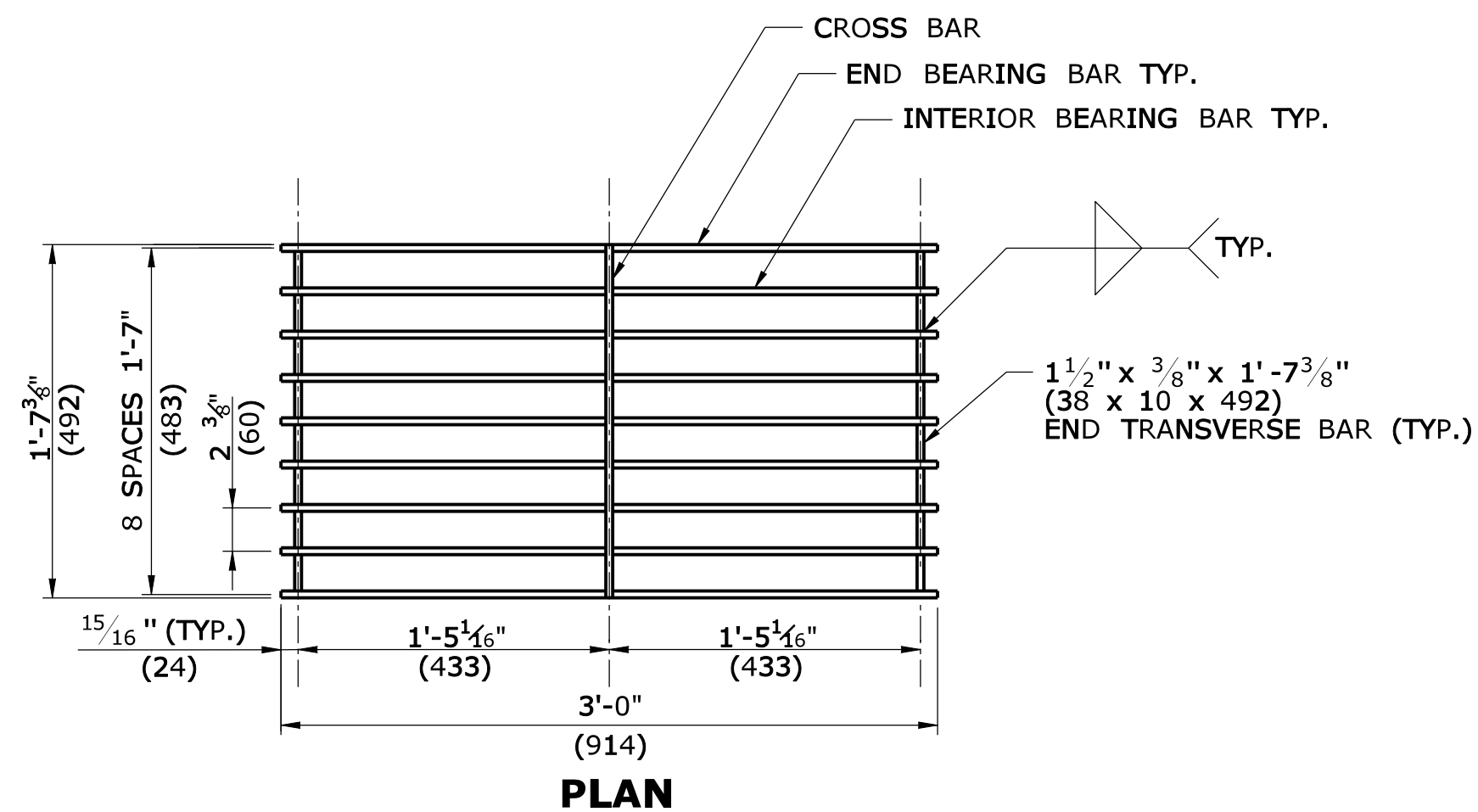
**PLAN**

**GENERAL NOTES:**

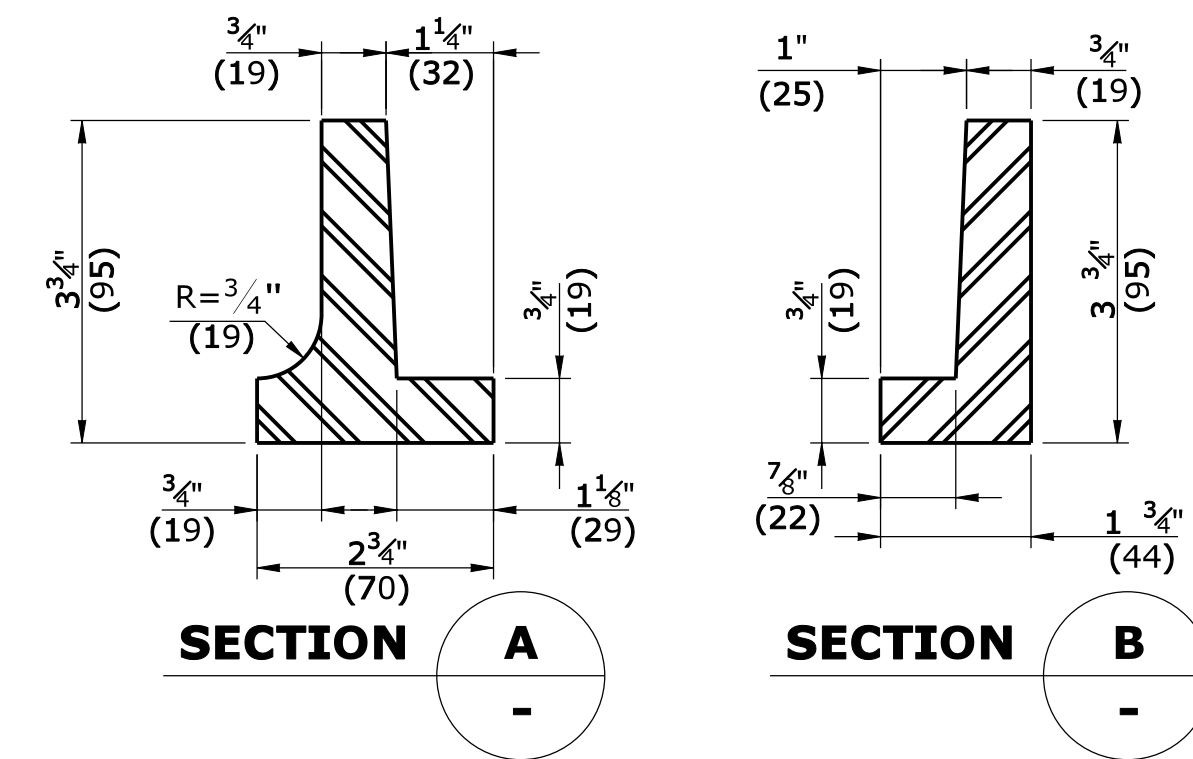
1. STEEL OR CAST IRON SHALL BE USED FOR FRAMES. STEEL SHALL BE USED FOR TYPE "A" & "B" GRATES.
2. TYPE "A" GRATES SHALL BE USED ON ALL ROADWAYS WHERE BICYCLE TRAFFIC IS ALLOWED OR AS DIRECTED BY THE ENGINEER.
3. TYPE "B" GRATES SHALL BE USED ON ALL LIMITED ACCESS HIGHWAYS, RAMPS AND WHERE BICYCLE TRAFFIC IS NOT ALLOWED OR AS DIRECTED BY THE ENGINEER.
4. STEEL FRAMES AND GRATES SHALL BE GALVANIZED IN ACCORDANCE WITH ARTICLE M.06.03.
5. DO NOT GALVANIZE CAST IRON FRAMES.
6. DIMENSIONAL TOLERANCES SHALL BE  $\pm 1/16"$  (1.6)
7. ALL STEEL BARS SHALL BE WELDED AT ALL INTERSECTIONS.
8. ALL WELDING SHALL CONFORM TO THE REQUIREMENTS OF AWS STRUCTURAL WELDING CODE, D1.1.



**PLAN**



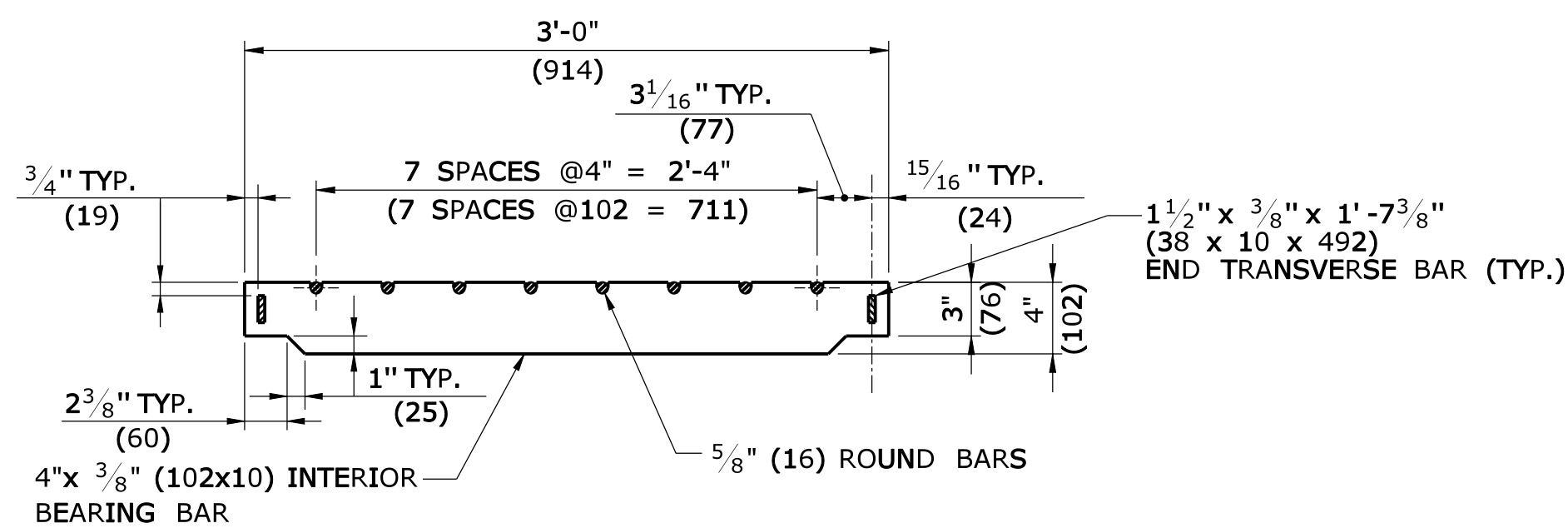
**PLAN**



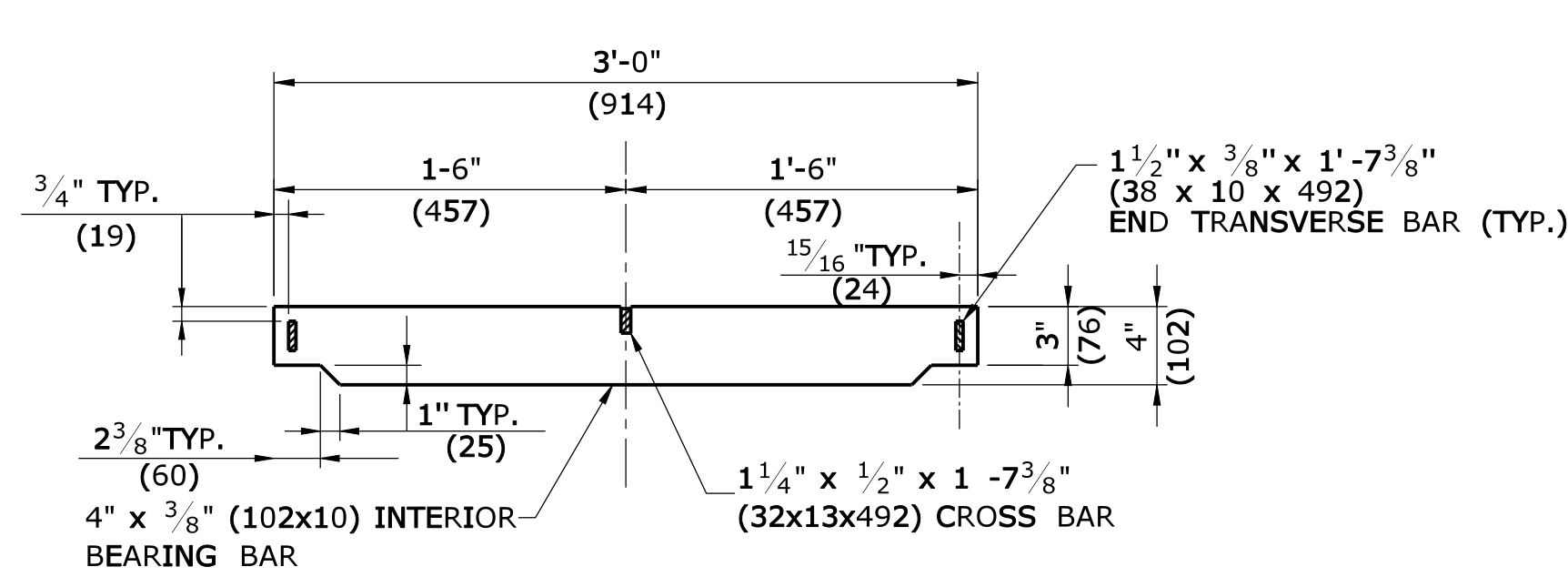
**SECTION A**

**SECTION B**

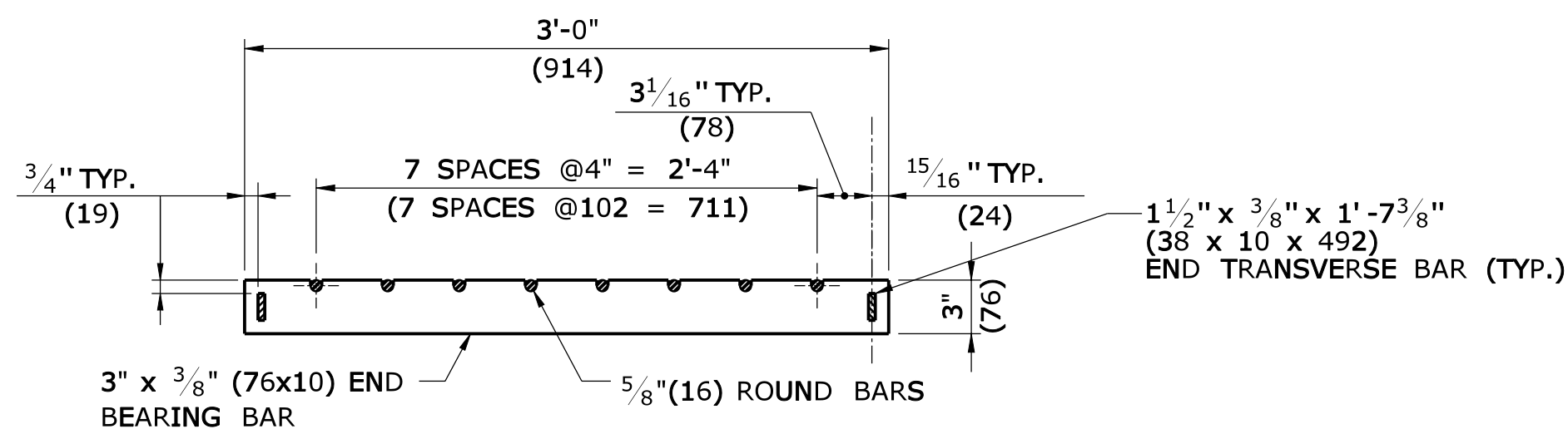
**CAST IRON FRAME ALTERNATE**



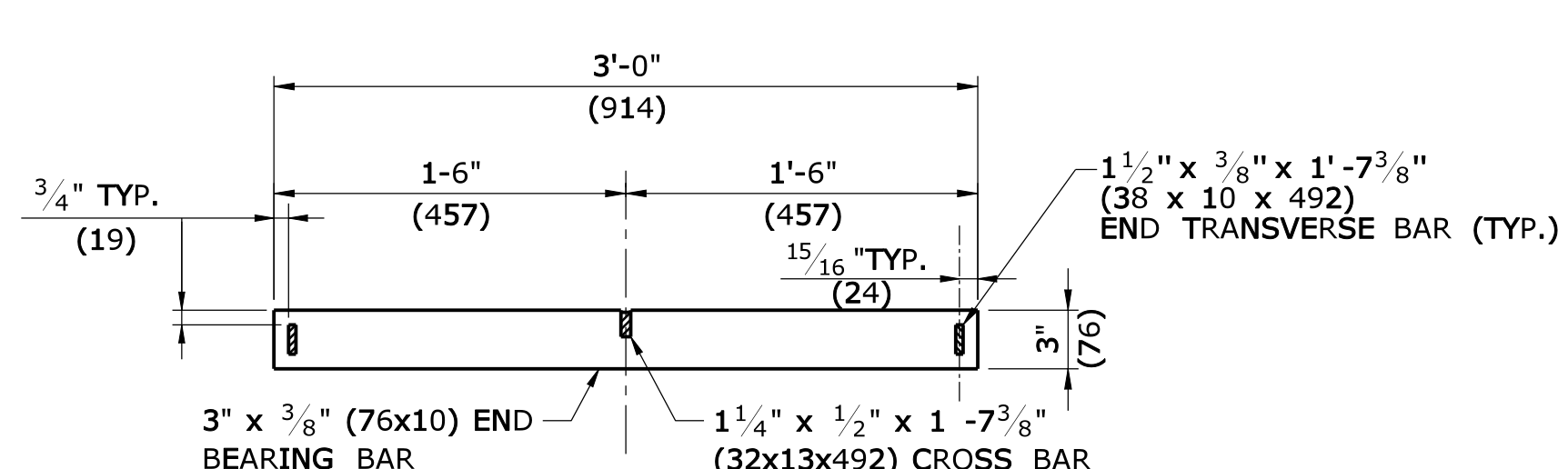
**ELEVATION- INTERIOR BEARING BAR  
CATCH BASIN GRATE TYPE A**



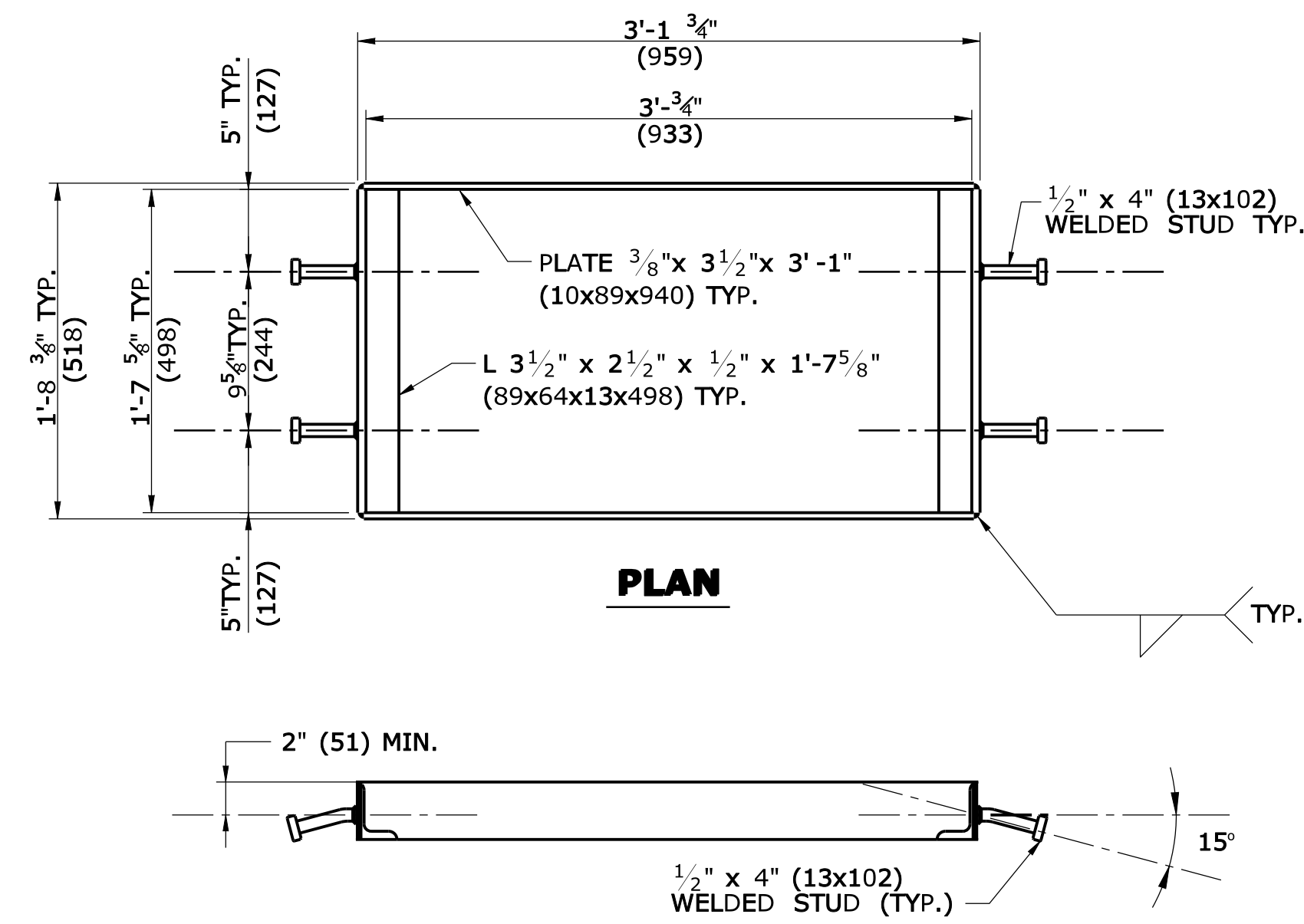
**ELEVATION- INTERIOR BEARING BAR  
CATCH BASIN GRATE TYPE B**



**ELEVATION- END BEARING BAR  
CATCH BASIN GRATE TYPE A**



**ELEVATION- END BEARING BAR  
CATCH BASIN GRATE TYPE B**



**WELDED STUD ANCHOR DETAILS  
STEEL FRAME**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REVISION	DATE	DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

NOT TO SCALE

Plotted Date: 9/11/2009

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

File name: CTDOT\_HIGHWAY\_STD.dgn Model: HW-507\_08

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:  
**CATCH BASIN FRAMES AND GRATES**

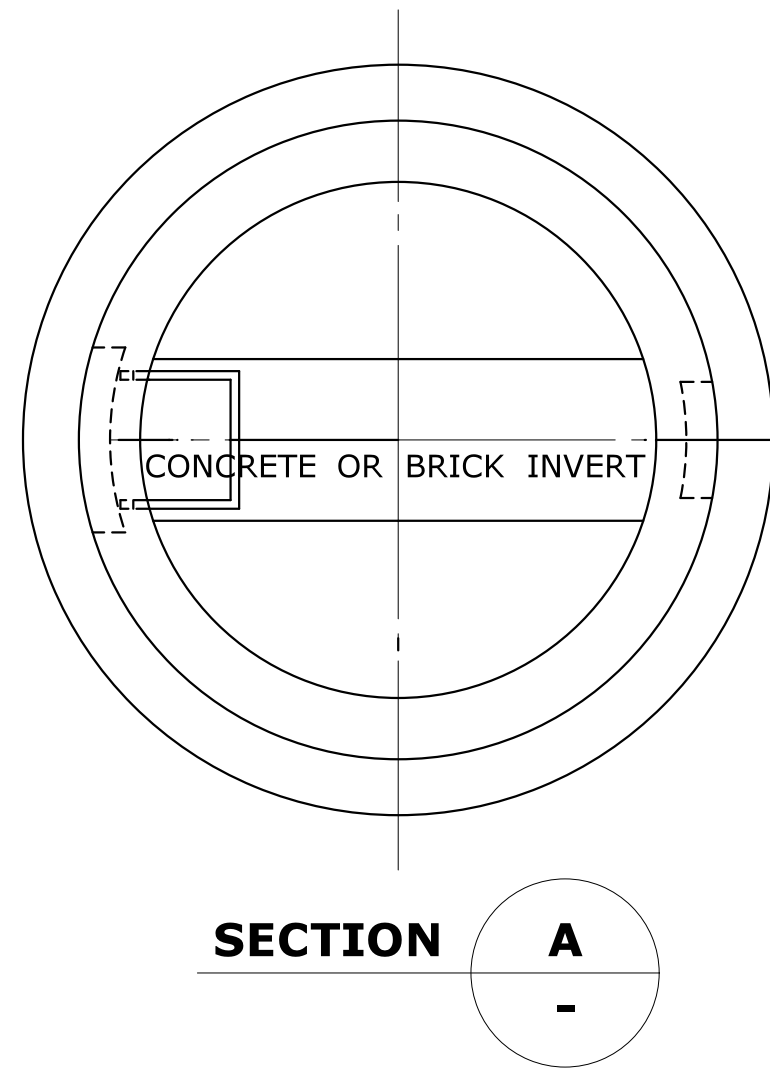
STANDARD SHEET NO.:  
**HW-507\_08**



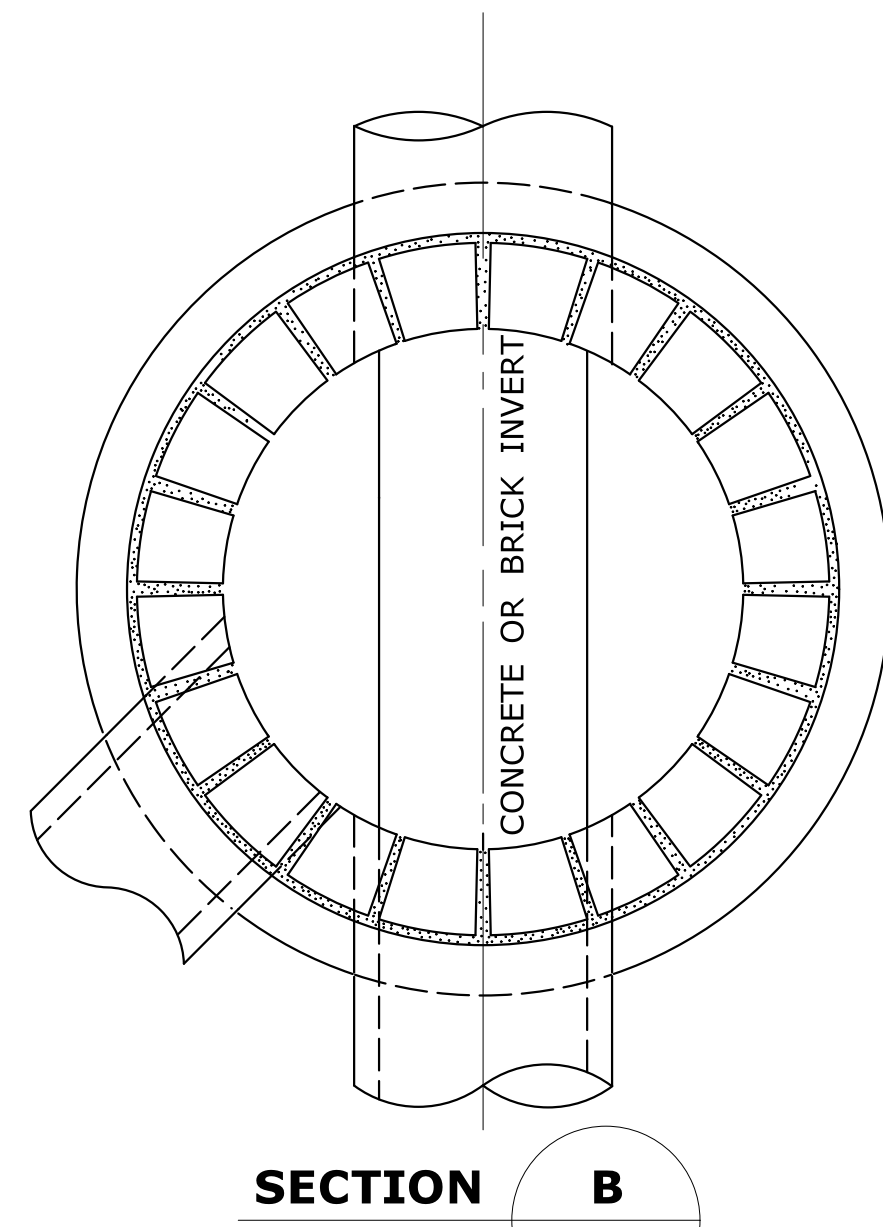
**GENERAL NOTES:**

1. CHANNELS MAY BE SHAPED IN CONCRETE BASE OF MANHOLE OR FORMED USING BRICK OR MASONRY.
2. A FRAME DIAMETER OF 3'-3" (991) WITH 4" (102) FLANGE MUST BE USED WHEN THE TOP DIAMETER OF THE PRECAST CONE IS LESS THAN 3'-6" (1067). ALL OTHER FRAME DIMENSIONS SHALL REMAIN THE SAME.
3. COVER:
 

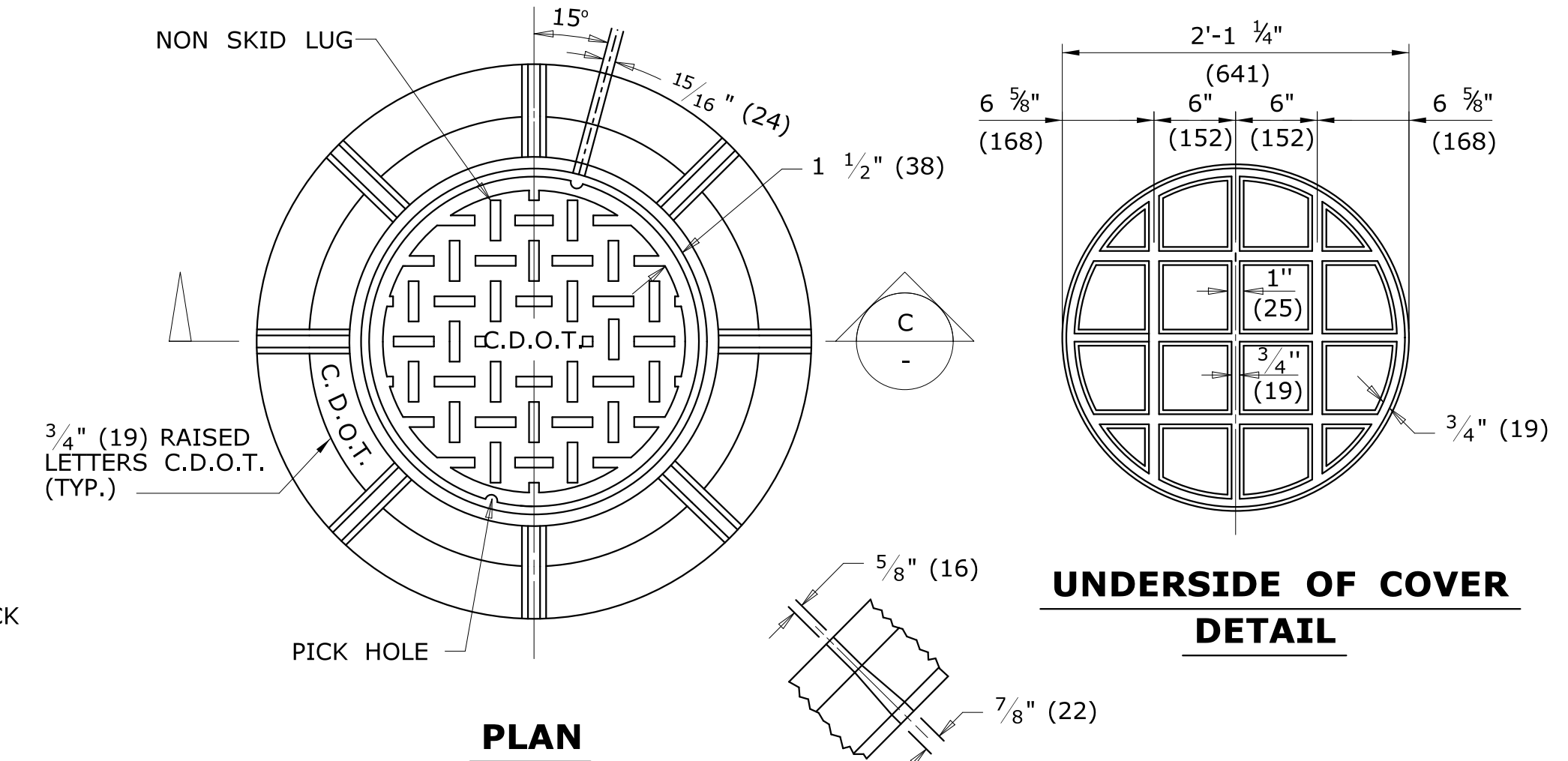
	CAST IRON	STEEL
MIN. COVER WEIGHT	134LB.(61kg)	134LB.(61kg)
4. ALL DIMENSIONS SUBJECT TO MANUFACTURING TOLERANCES.



**SECTION A**

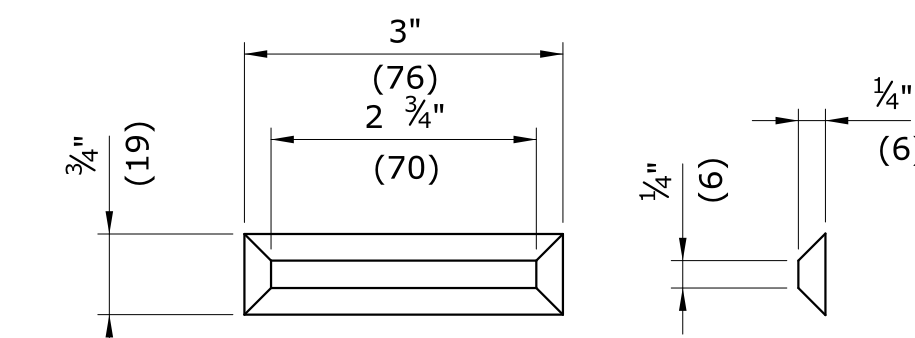


**SECTION B**

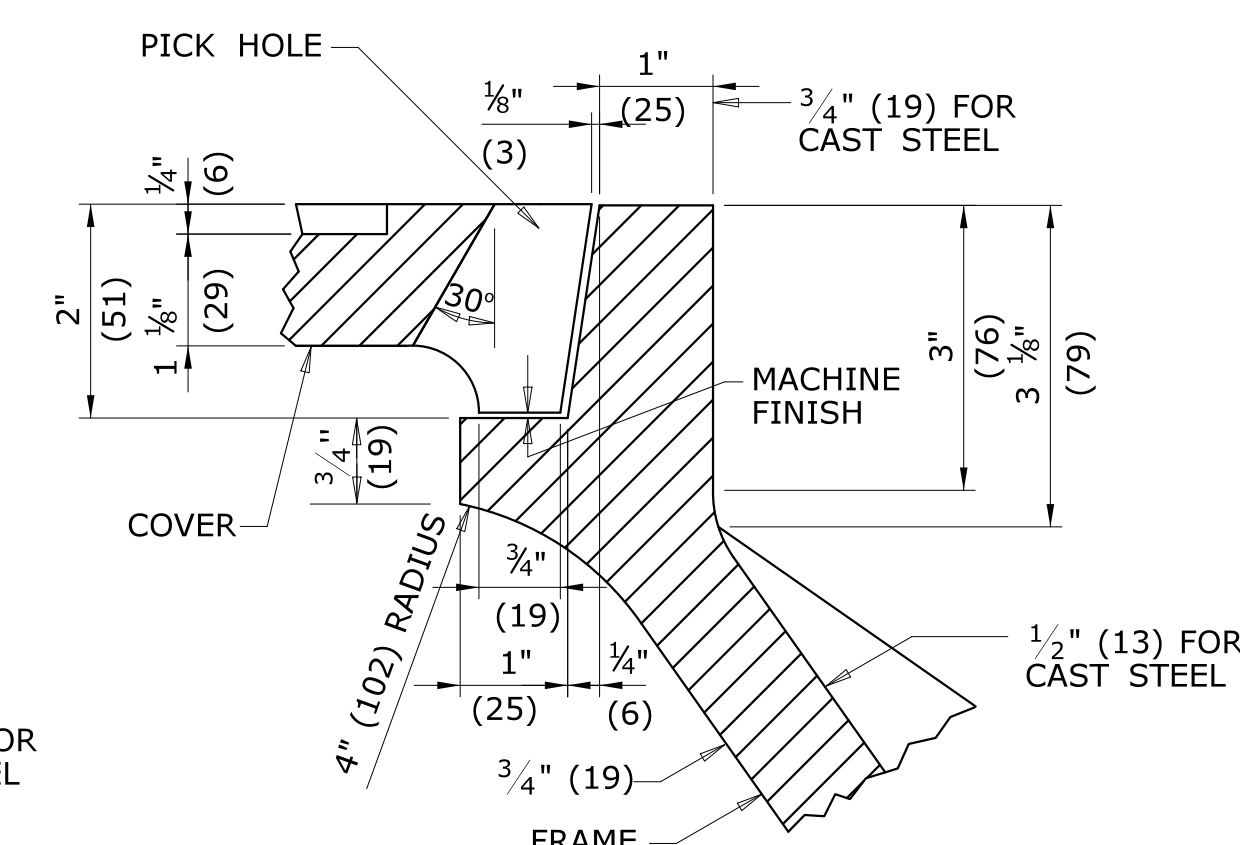


**PLAN**

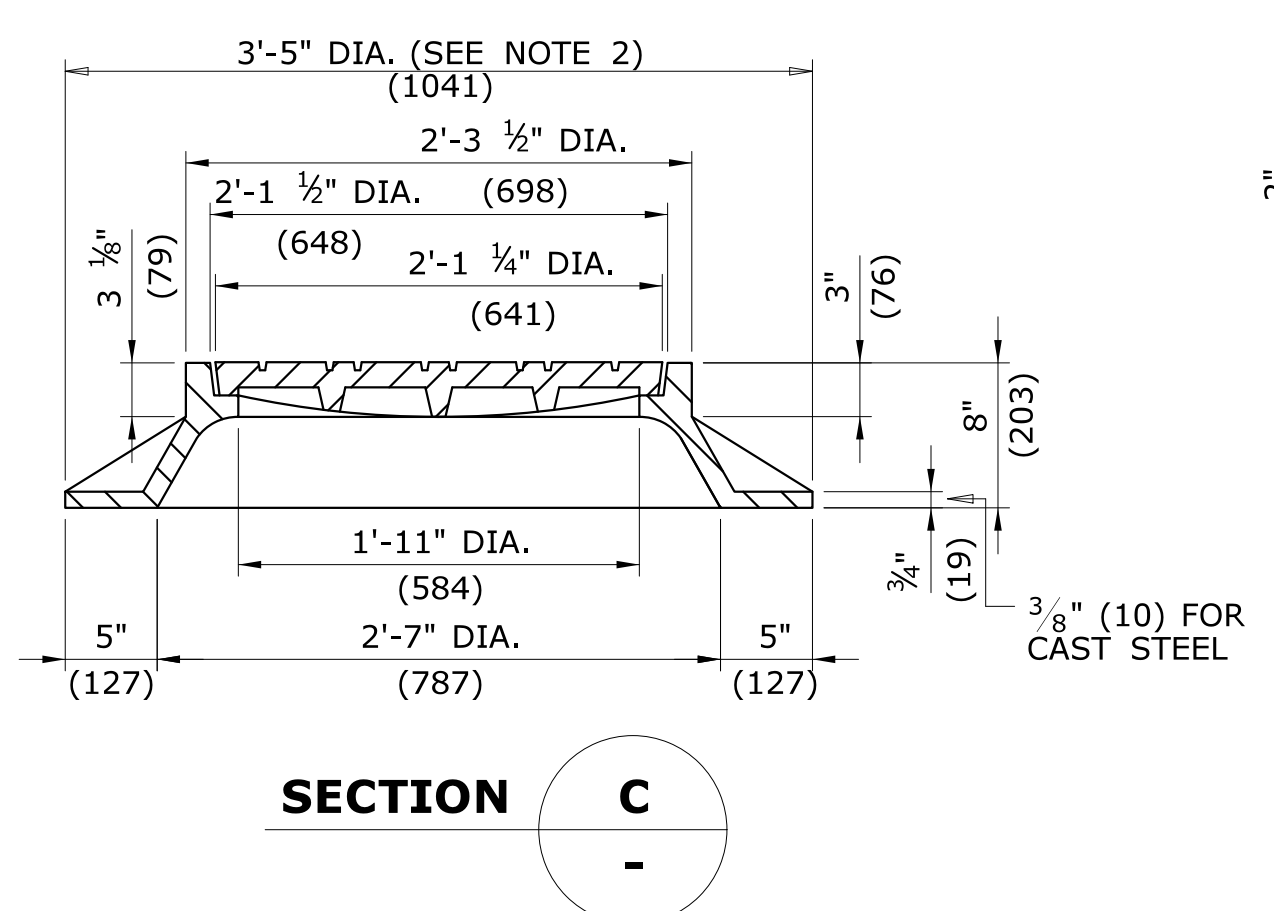
**UNDERSIDE OF COVER DETAIL**



**NON SKID LUG DETAIL**

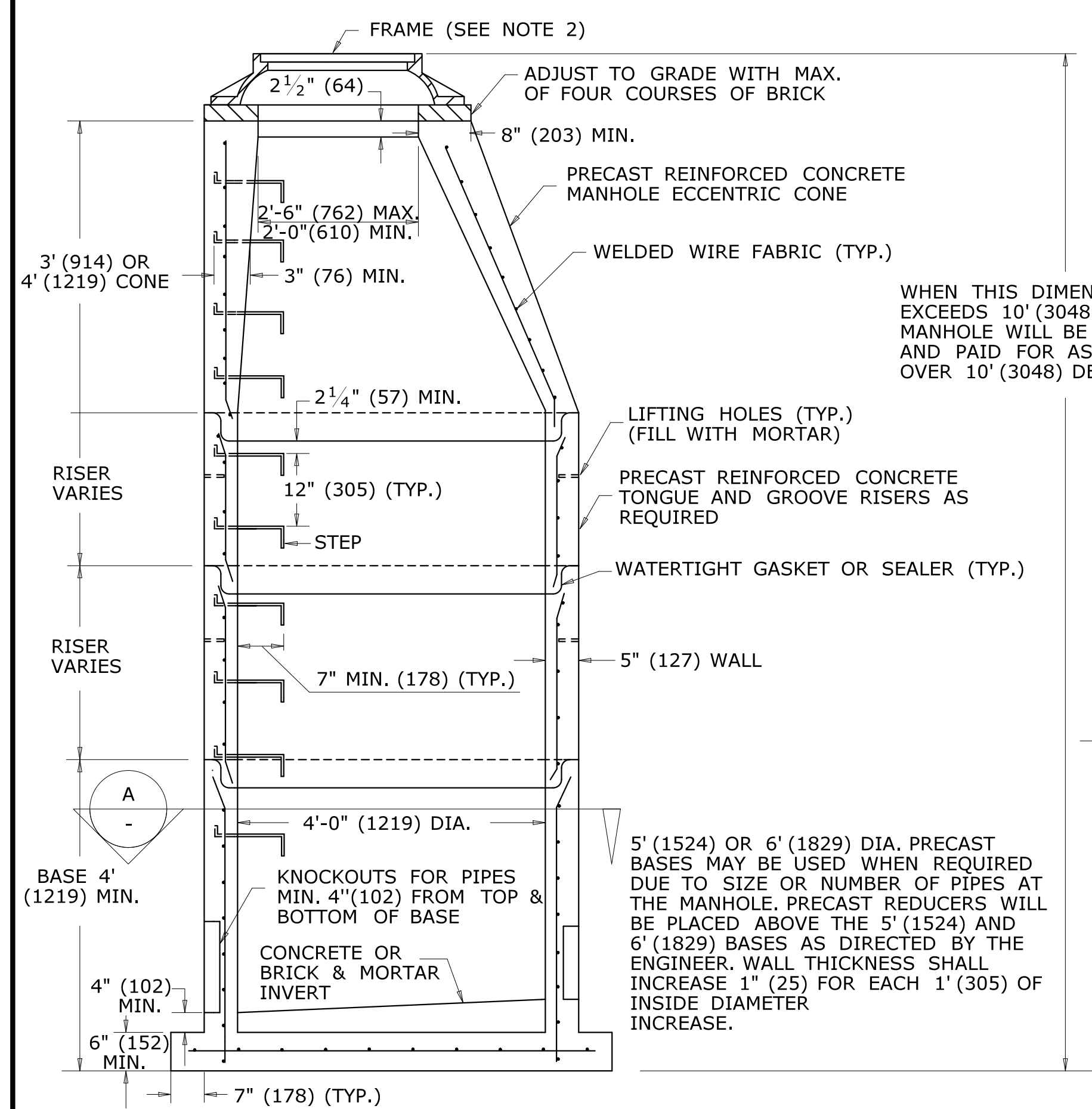


**DETAIL OF SEAT**



**SECTION C**

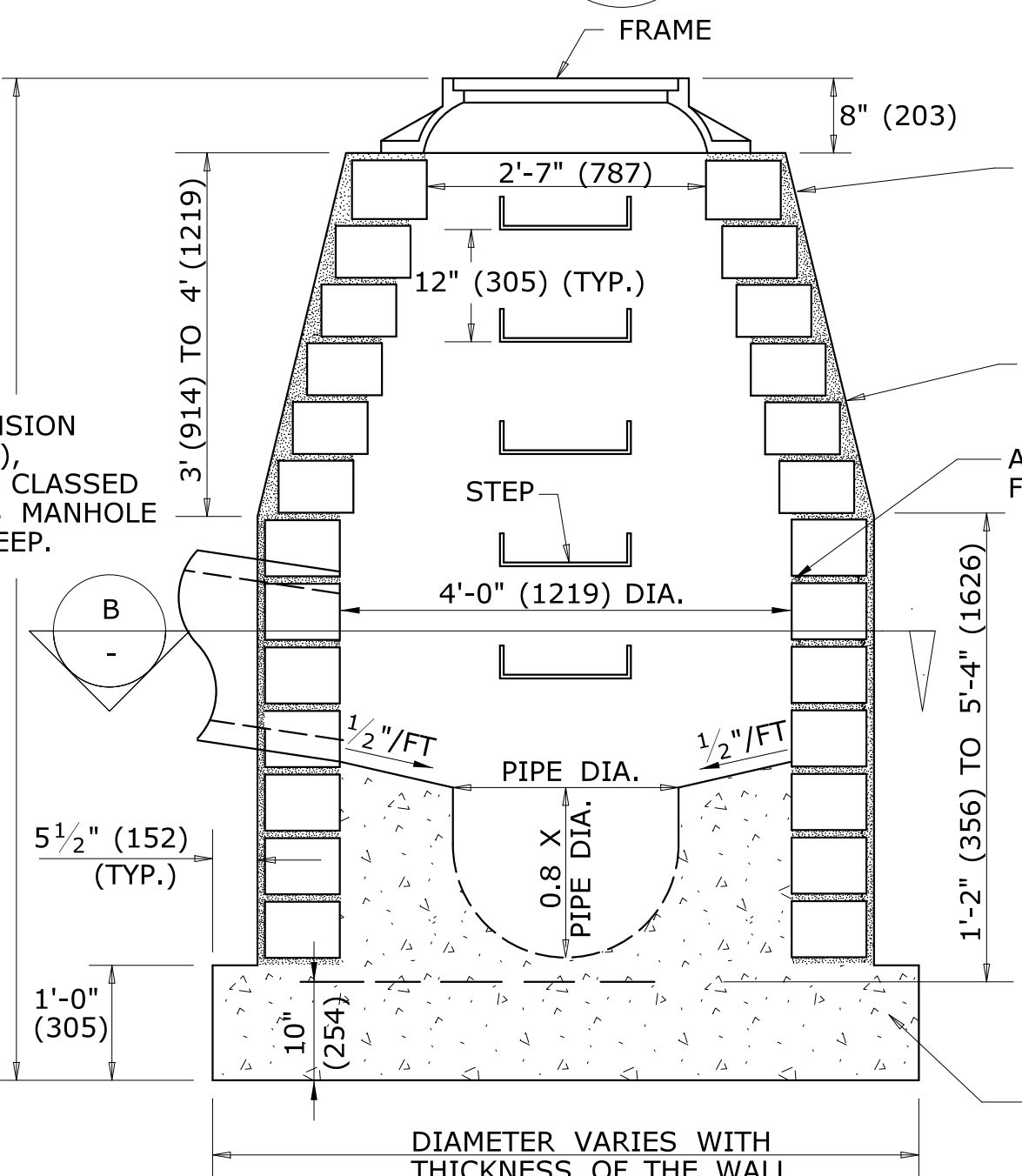
**FRAME AND COVER DETAILS**



**ELEVATION**

**MANHOLE**

REINFORCED PRECAST CONCRETE UNIT



**ELEVATION**

**MANHOLE**

MASONRY CONCRETE UNIT OR CLASS "A" CONCRETE

WALL SHALL BE A MIN. OF 6"(152) WITH MASONRY CONCRETE UNITS, CLASS "A" CONCRETE WALL SHALL BE 12"(300) THICK WHEN DEPTH OF MANHOLE IS GREATER THAN 10'(3048) DEEP.

MASONRY WALLS SHALL BE PLASTERED OUTSIDE WITH 2:1 CEMENT MORTAR 1/2"(13) THICK. MASONRY MUST BE WET WHEN MORTAR IS APPLIED.

ALL JOINTS SHALL BE POINTED FLUSH AND FULL

WALLS SHALL BE BUILT OF MASONRY CONCRETE UNITS OR CLASS "A" CONCRETE AT THE OPTION OF THE CONTRACTOR.

MASONRY CONCRETE UNITS SHALL BE LAID IN CEMENT SAND MORTAR 1:2 MIX, JOINTS SHALL NOT BE OVER 1/2"(13) ON INSIDE FACE

WHEN THIS DIMENSION EXCEEDS 10' (3048), MANHOLE WILL BE CLASSED AND PAID FOR AS MANHOLE OVER 10' (3048) DEEP.

5' (1524) OR 6' (1829) DIA. PRECAST BASES MAY BE USED WHEN REQUIRED DUE TO SIZE OR NUMBER OF PIPES AT THE MANHOLE. PRECAST REDUCERS WILL BE PLACED ABOVE THE 5' (1524) AND 6' (1829) BASES AS DIRECTED BY THE ENGINEER. WALL THICKNESS SHALL INCREASE 1" (25) FOR EACH 1' (305) OF INSIDE DIAMETER INCREASE.

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

1	6/11	REVISE STEP WIDTH PER OSHA
2	7/13	REVISE COVER FRAME WEIGHT
-	-	-
-	-	-
-	-	-
-	-	-
REV.	DATE	REVISION DESCRIPTION

NOT TO SCALE

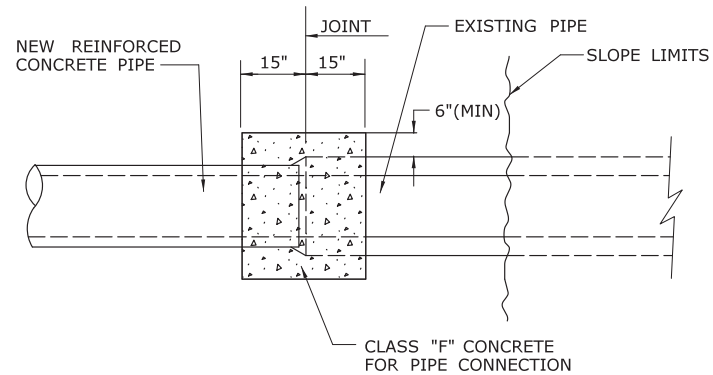
**STATE OF CONNECTICUT**  
**DEPARTMENT OF TRANSPORTATION**

SUBMITTED BY: \_\_\_\_\_ NAME/DATE/TIME: \_\_\_\_\_  
 APPROVED BY: \_\_\_\_\_ NAME/DATE/TIME: \_\_\_\_\_

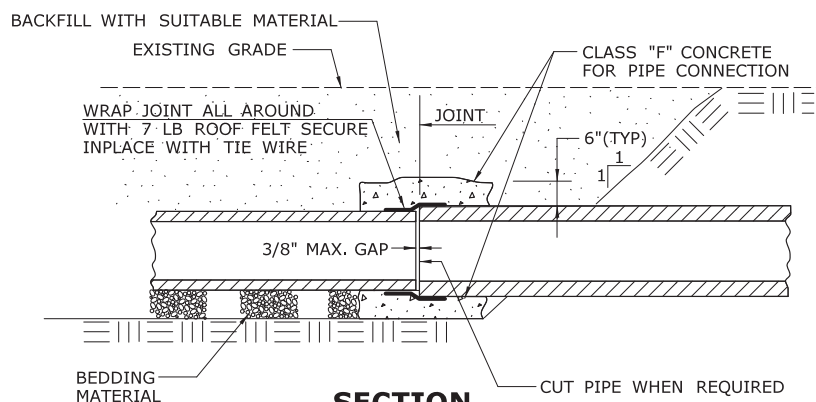
**CTDOT**  
**STANDARD SHEET**  
**OFFICE OF ENGINEERING**

STANDARD SHEET TITLE:  
**MANHOLE - FRAME & COVER**

STANDARD SHEET NO.:  
**HW-507\_10**



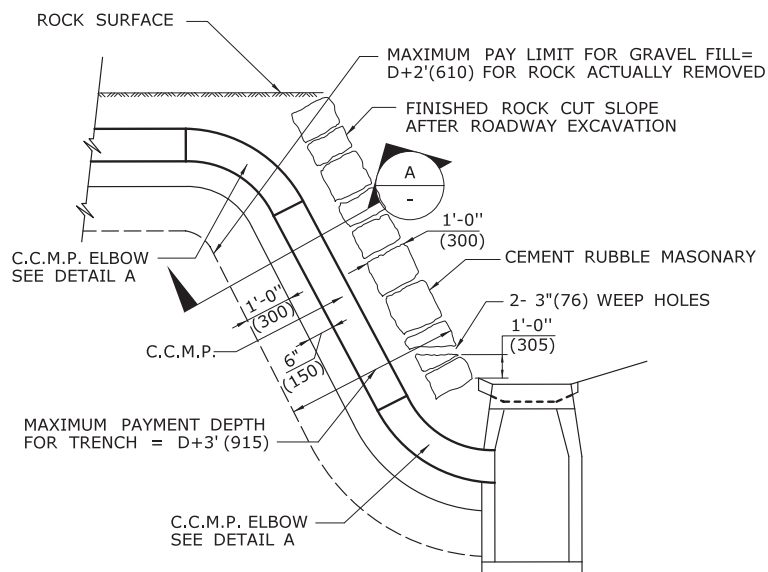
**PLAN**



**SECTION  
CONCRETE PIPE CONNECTION**

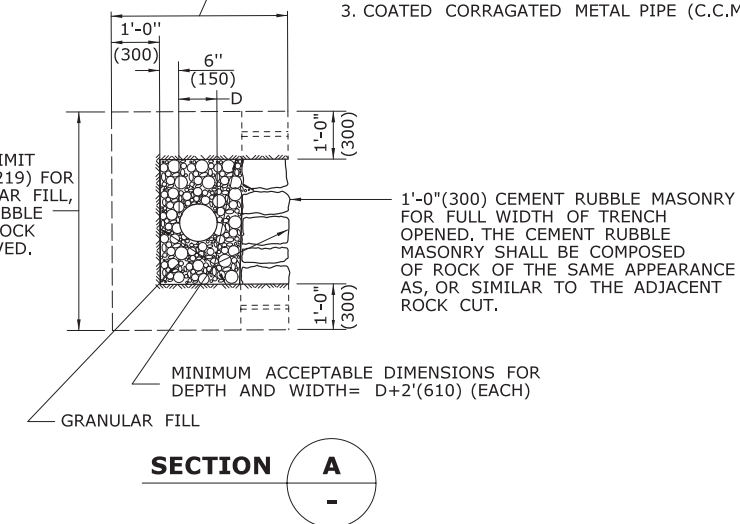
**NOTES:**

1. "CONCRETE PIPE CONNECTION" IS INTENDED FOR USE WHERE A REINFORCED CONCRETE PIPE REPAIR OR MODIFICATION IS NEEDED SOMEWHERE WITHIN A PIPE RUN WHERE A BELL/SPIGOT JOINT CANNOT BE ACHIEVED.
2. MAINTAIN INTERIOR ALIGNMENT OF PIPE AT JOINTS UNTIL CONCRETE IS PROPERLY CURED.
3. BACKFILL OF PIPE REPAIR WITH SUITABLE MATERIAL MAY NOT TAKE PLACE UNTIL CONCRETE IS PROPERLY CURED.
4. CONTRACTOR SHALL MAINTAIN LINE AND GRADE OF PIPE REPAIR OR MODIFICATION BY METHODS APPROVED BY THE ENGINEER.
5. HOLES OR GAPS AT JOINT LARGER THAN 3/8" SHALL BE FILLED OR WRAPPED TO PREVENT CONCRETE FROM ENTERING PIPE.
6. TRENCH EXCAVATION SHALL BE TO THE MAXIMUM EXTENT NEEDED TO PERFORM WORK.



MAXIMUM PAY LIMIT DEPTH FOR TRENCH =  $D+3'(915)$ . MAXIMUM PAY LIMIT FOR GRAVEL FILL =  $D+2'(610)$  FOR ROCK ACTUALLY REMOVED

MAXIMUM PAY LIMIT WIDTH =  $D+4'(1219)$  FOR TRENCH, GRANULAR FILL, AND CEMENT RUBBLE MASONRY FOR ROCK ACTUALLY REMOVED.



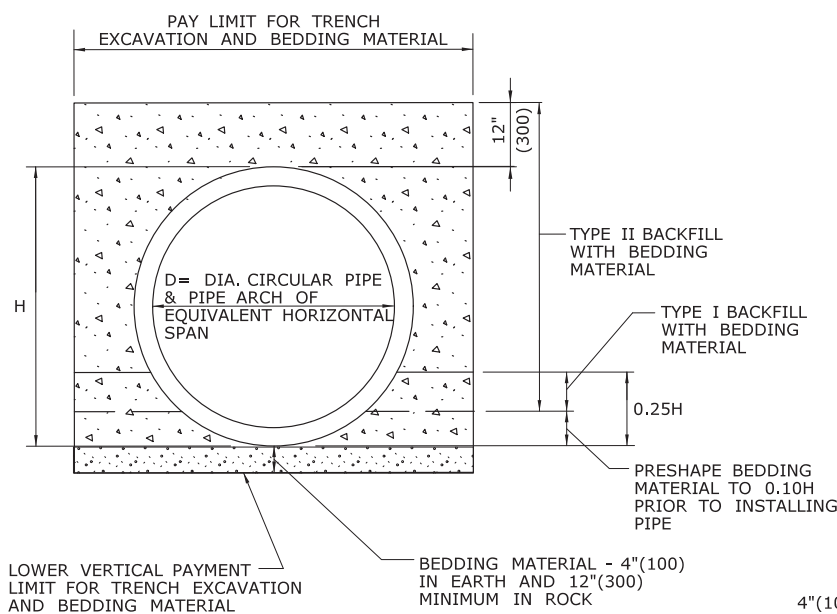
**SECTION A**

**TYPICAL INSTALLATION OF C.C.M.P. IN ROCK SLOPE**

**GENERAL NOTES:**

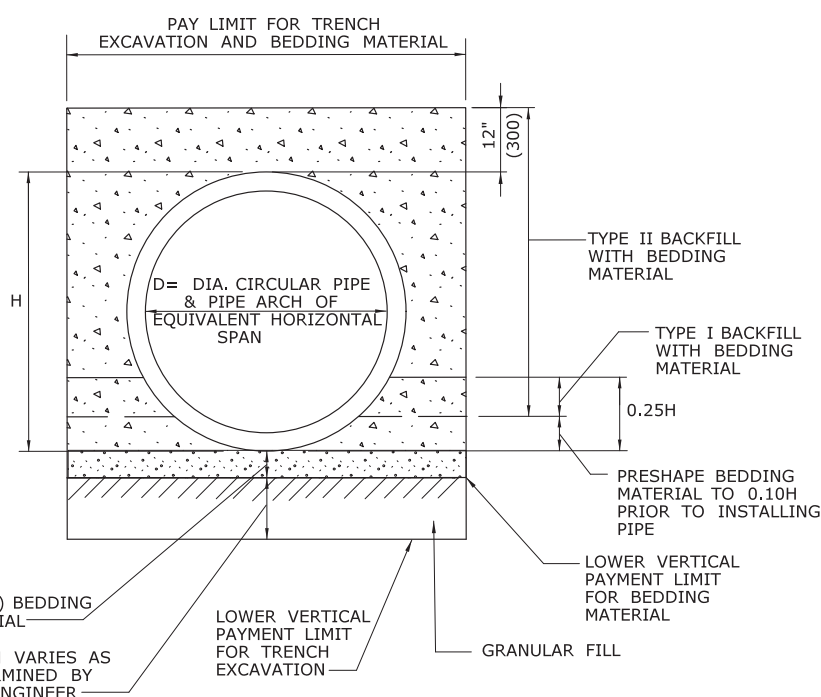
1. ROCK REMOVED BEYOND THE MAXIMUM PAY LIMIT SHOWN SHALL BE REPLACED WITH CEMENT RUBBLE MASONRY AND GRANULAR FILL.
2. FILL, AS REQUIRED TO CLOSE THE OPENING AS SHOWN ON THE PLANS, WILL BE AT THE CONTRACTOR'S EXPENSE. HOWEVER, THE PAY LIMIT LINES MAY BE MODIFIED TO COINCIDE WITH NATURAL FAULTS OR FISSURES OF ROCK AS THE ENGINEER MAY DETERMINE.
3. COATED CORRUGATED METAL PIPE (C.C.M.P.)

1'-0" (300) CEMENT RUBBLE MASONRY FOR FULL WIDTH OF TRENCH OPENED, THE CEMENT RUBBLE MASONRY SHALL BE COMPOSED OF ROCK OF THE SAME APPEARANCE AS, OR SIMILAR TO THE ADJACENT ROCK CUT.



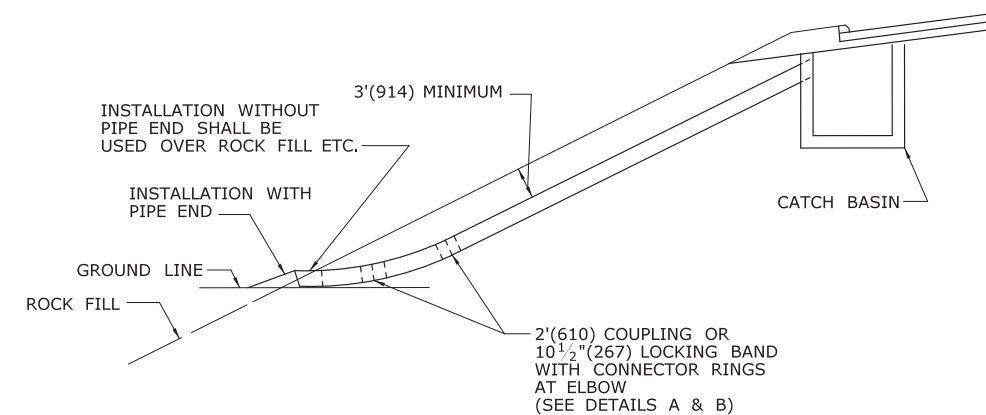
**PIPE TRENCH DETAIL**

**WHERE GRANULAR FILL IS NOT USED**



**PIPE TRENCH DETAIL**

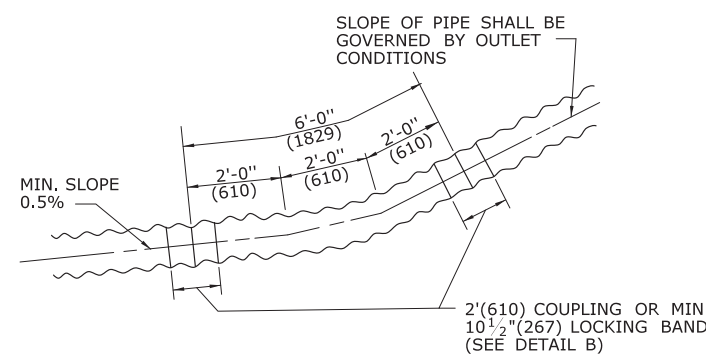
**WHERE GRANULAR FILL IS USED AS BEDDING**



**TYPICAL INSTALLATION OF C.C.M.P. ELBOW IN FILL SLOPE**

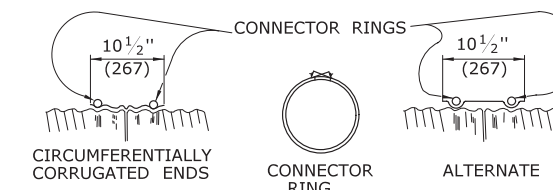
**TABLE C  
CONNECTOR RINGS**

PIPE DIAMETER	LENGTH OF RING
12" (300)	52" (1321)
15" (381)	61" (1549)
18" (457)	71" (1803)
21" (533)	80" (2032)
24" (610)	90" (2286)
30" (762)	108" (2743)
36" (915)	128" (3251)
42" (1066)	147" (3734)
48" (1219)	166" (4216)



**DETAIL A**

**C.C.M.P. ELBOW AND COUPLING**



**DETAIL B**

**ELBOW DIMENSIONS**

RODS: 7/16" (11) DIAMETER ELECTRO-GALVANIZED WITH 6" (152) LENGTH OF 1/2" (13) ROLLED THREADS EACH END, FURNISHED CURVED, TO FIT PIPE. SEE TABLE C

LUGS: DOUBLE TAKE UP, CAST IRON, ELECTRO-GALVANIZED.

NOTE: THE COUPLER FASTENING DEVICE SHALL NOT INTERFERE WITH INSTALLATION OF CONNECTOR RINGS.

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
3	7/13	ADD CONCRETE PIPE CONNECTION DETAIL
2	6/01/10	REVISE TITLE TO SAY TRENCH DETAIL
1	5/01/10	REMOVE GRAVEL, REPLACE W/ GRANULAR

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/13/2013

NOT TO SCALE



Filename: CTDOT\_HIGHWAY\_STD2013.dgn Model: 17-HW-651\_01

SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

**CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING**

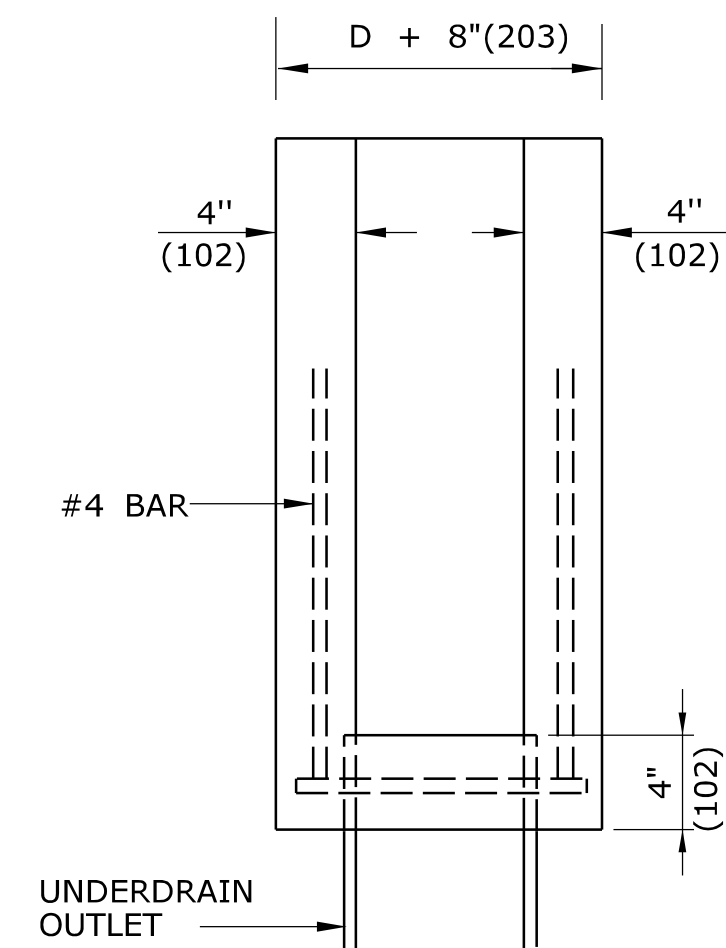
STANDARD SHEET TITLE:  
**C.C.M.P.IE INSTALLATIONS IN FILL & ROCK SLOPES & PIPE TRENCH DETAIL**

STANDARD SHEET NO.:  
**HW-651\_01**

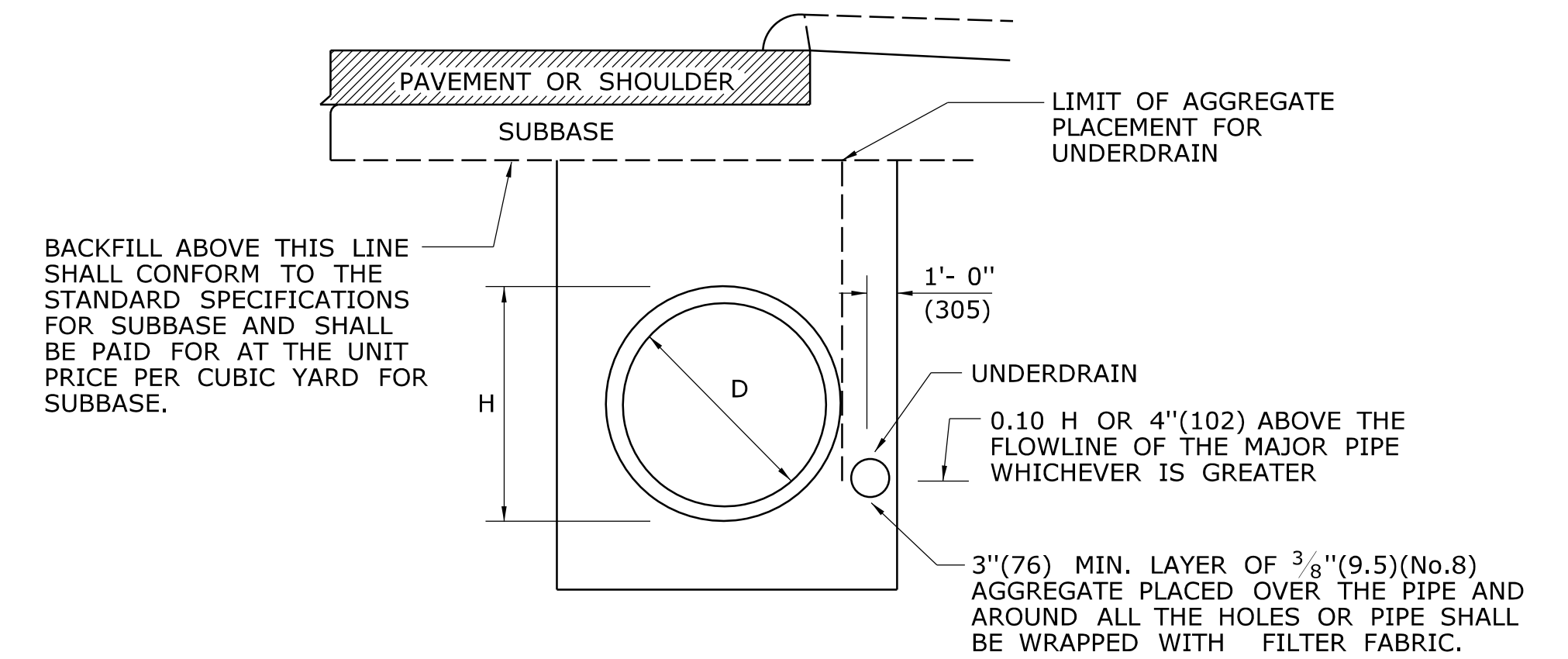


**GENERAL NOTES:**

1. THE OUTLET ENDWALL CAN BE EITHER PRECAST OR CAST IN PLACE. CONCRETE SHALL BE CLASS "C" CONCRETE.
2. IF PRECAST CONCRETE ENDWALL IS USED, THE OUTLET SHALL BE GROUTED AND SEALED TO ENDWALL OPENING WITH NON-SHRINK GROUT.

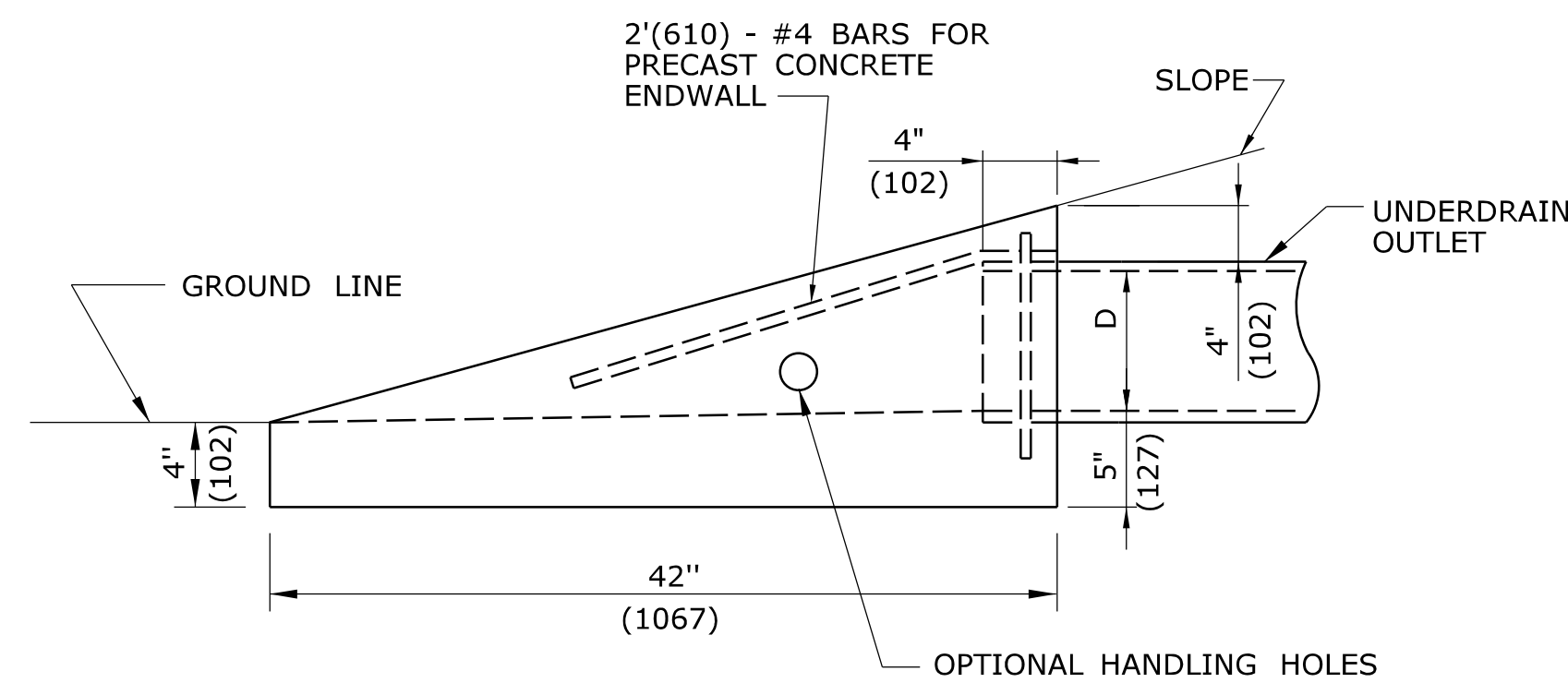


**PLAN VIEW**

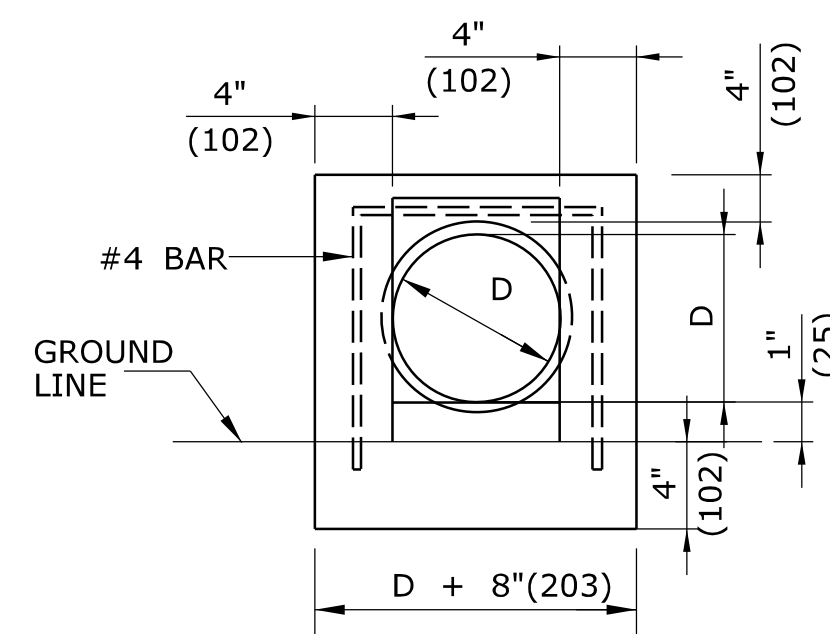


NOTE:  
PAY LIMIT FOR TRENCH EXCAVATION AND BEDDING SHALL BE THE SAME AS FOR THE CULVERT ALONE. MATERIAL ABOVE THE BEDDING SHALL BE SUITABLE MATERIAL OR GRANULAR FILL.

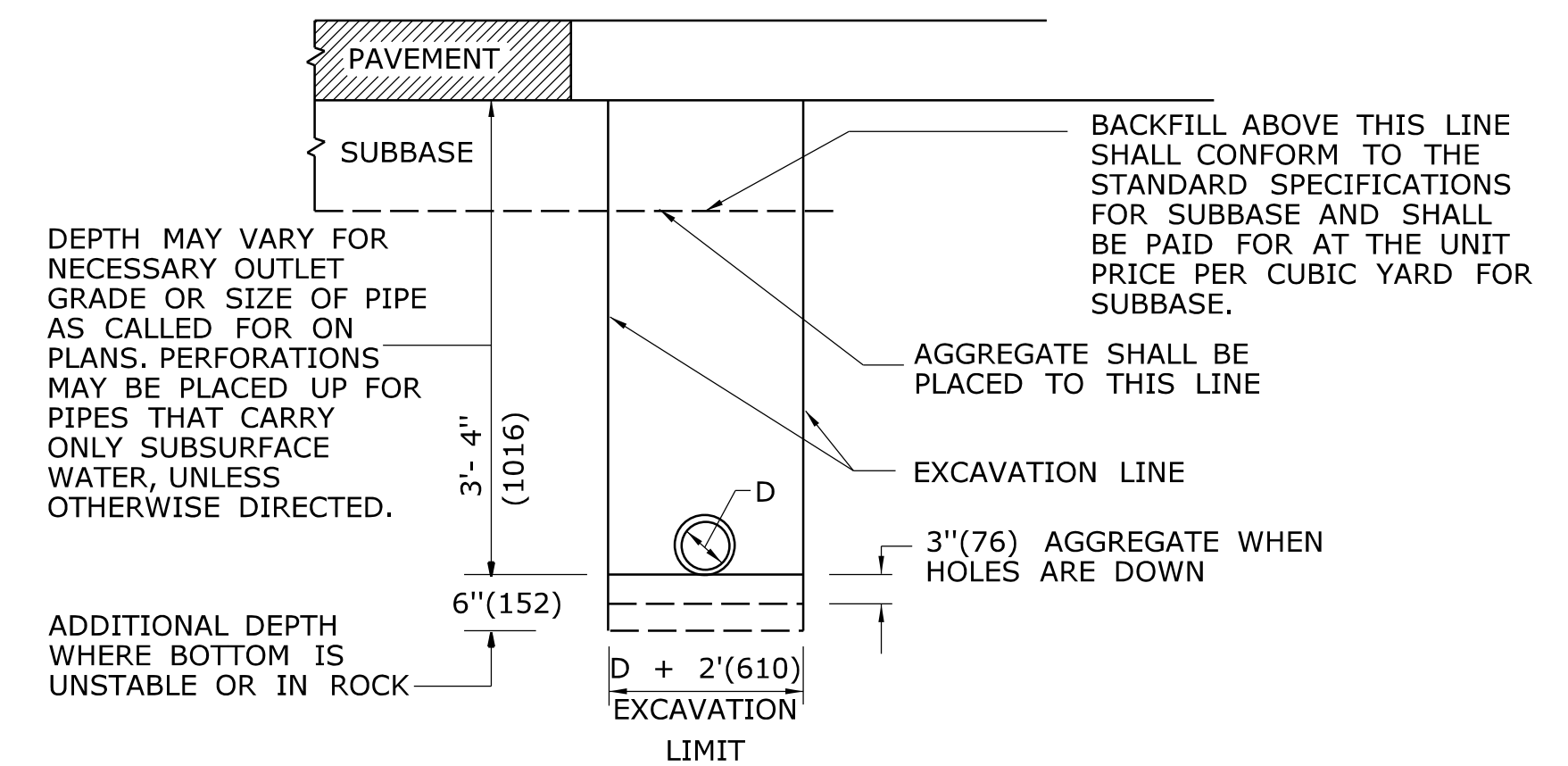
**CULVERT AND UNDERDRAIN  
IN THE SAME TRENCH**



**ENDWALL**



**ELEVATION**



**UNDERDRAIN**

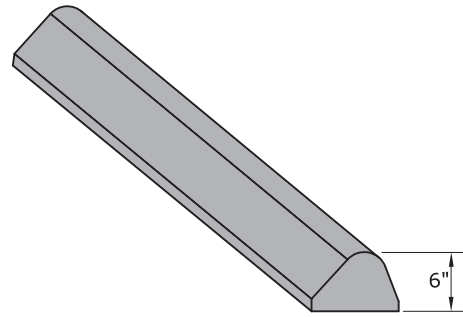
**UNDERDRAIN OUTLET DETAILS**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

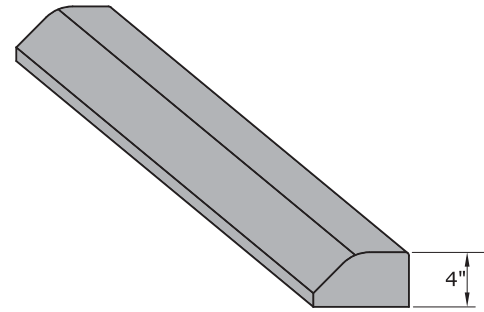
1	6/01/10	REMOVED RODENT SCREEN DETAILS AND REVISED TITLES	<p>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</p>	<p>NOT TO SCALE</p>	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	<p>SUBMITTED BY: _____ NAME/DATE/TIME: _____</p>	<p>CTDOT STANDARD SHEET</p>	<p>STANDARD SHEET TITLE: <b>UNDERDRAINS AND UNDERDRAIN OUTLETS</b></p>	<p>STANDARD SHEET NO.: <b>HW-751_01</b></p>
-	-	-				<p>APPROVED BY: _____ NAME/DATE/TIME: _____</p>			
-	-	-							
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-	-	-							
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REV.	DATE	REVISION DESCRIPTION	Plotted Date: 6/23/2011		Filename: CTDOT_HIGHWAY STD_JUNE2011.dgn Model: 20 - HW-751_01		OFFICE OF ENGINEERING		



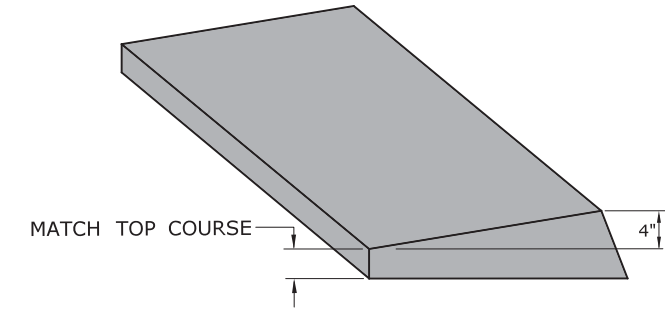




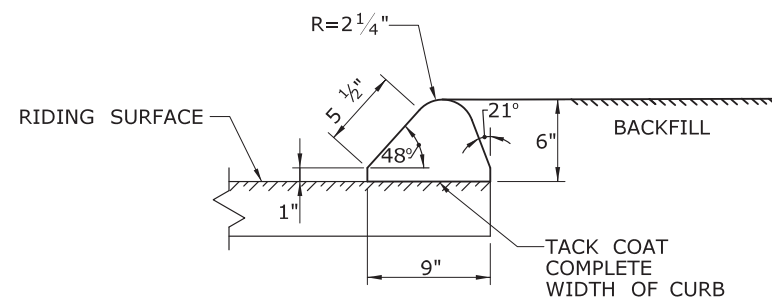
**BITUMINOUS CONCRETE LIP CURBING  
(6" HIGH)**



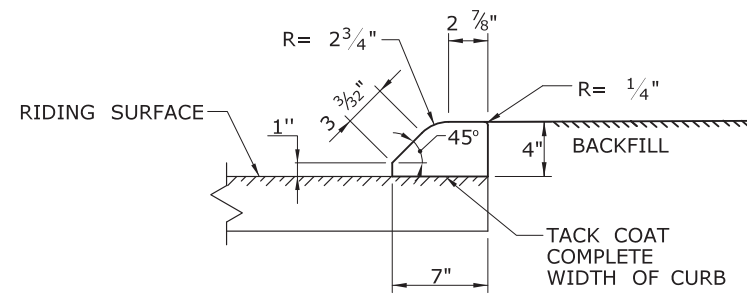
**BITUMINOUS CONCRETE PARK CURBING  
(4" HIGH)**



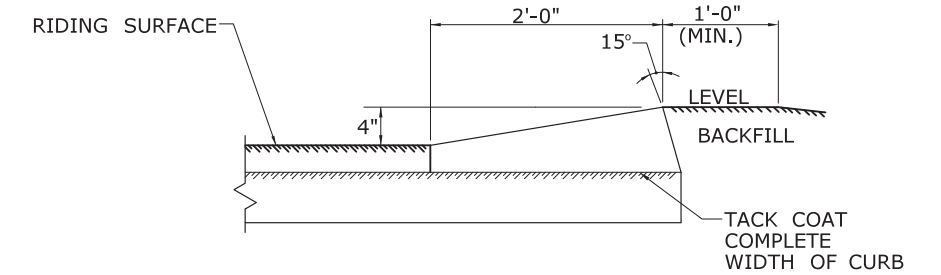
**BITUMINOUS CONCRETE BERM CURBING  
(4" HIGH)**



**SECTION**



**SECTION**



**SECTION**

1 6/17 NEW SHEET

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

NOT TO SCALE



SUBMITTED BY: NAME/DATE/TIME:

Leo Fontaine, P.E.  
2017.06.07 07:33:51-04'00'

APPROVED BY: NAME/DATE/TIME:

Gregory M. Dorosh, P.E.  
2017.06.07 10:44:27-04'00'

**CTDOT  
STANDARD SHEET**

**OFFICE OF ENGINEERING**

STANDARD SHEET TITLE:

**BITUMINOUS CONCRETE CURBING**

STANDARD SHEET NO.:

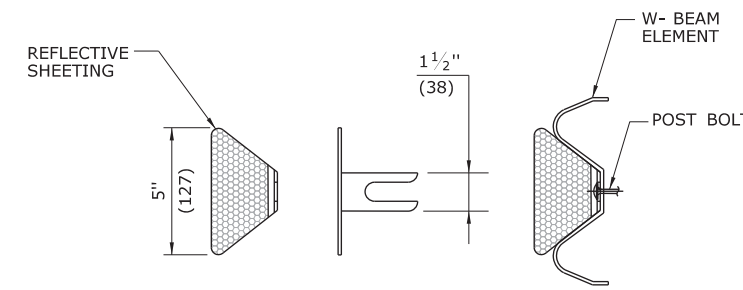
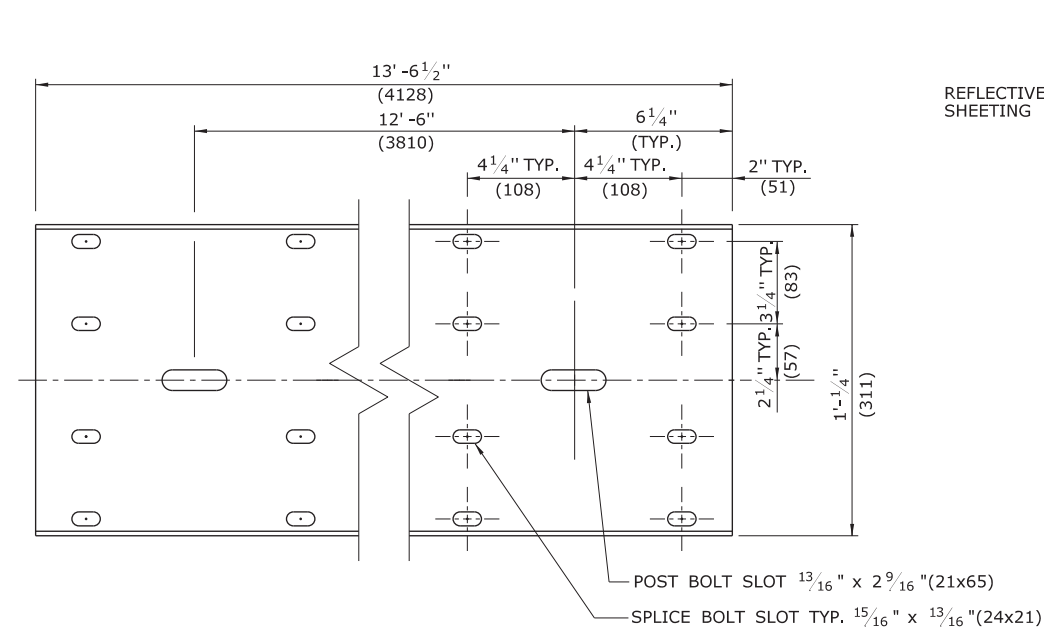
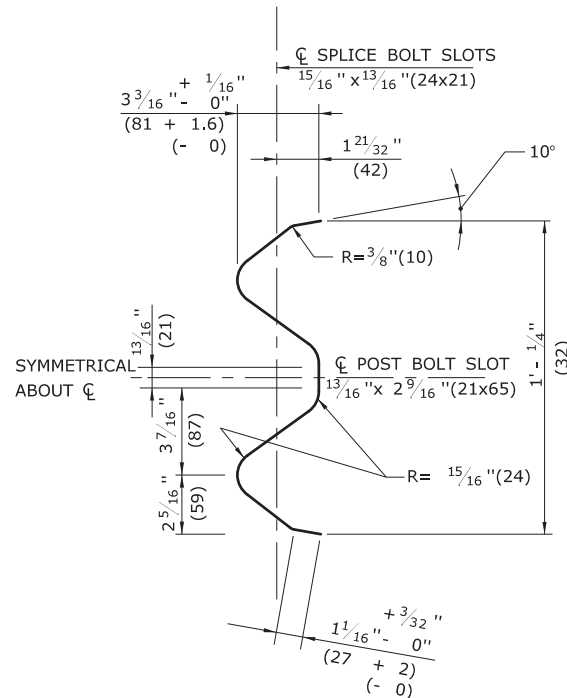
**HW-815\_01**

REV. DATE REVISION DESCRIPTION

Plotted Date: 6/6/2017

Filename: HW-815\_01.dgn

Model: CT-Civil-2D-Sheet



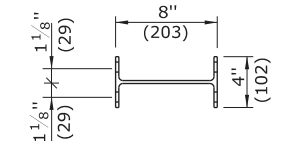
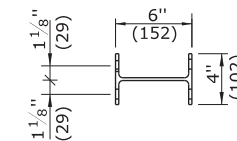
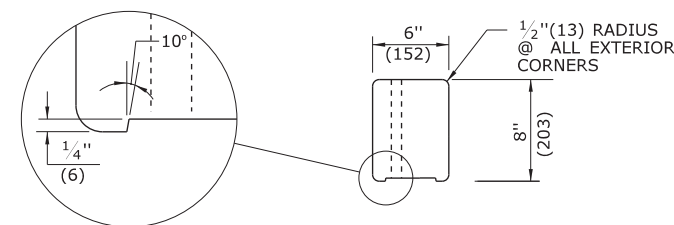
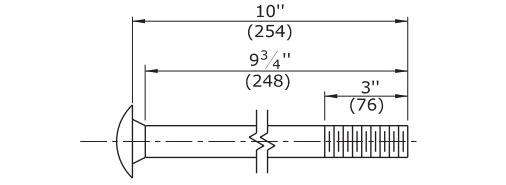
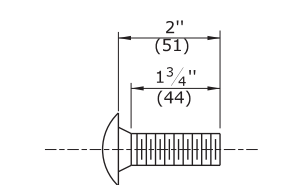
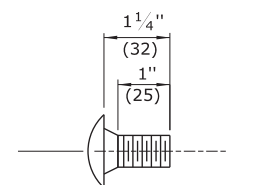
**GENERAL NOTES:**

- NEW R-B 350 GUIDERAIL INCLUDING SYSTEMS, ANCHORS AND TRANSITIONS INSTALLED ON EXPRESSWAYS AND RAMPS SHALL USE CLASS B TYPE II (10 GAUGE) W-BEAM RAIL ELEMENTS.
- W6x9 (W150x14) POSTS MAY BE USED IN PLACE OF W6 x 8.5(W150x13) POSTS.
- W8x13 (W200x19) POSTS, 7'-6"(2286) LONG, ARE USED WITH TRANSITIONS TO VERTICAL OR SAFETY SHAPE PARAPETS (POSTS 1 AND 2) AND SYSTEM 6.
- W6x8.5 (W150x13) POSTS, 6'-0"(1829) LONG, ARE USED WITH TRANSITIONS TO VERTICAL OR SAFETY SHAPE PARAPETS (POSTS 3 THROUGH 6), MD-B 350, SYSTEM 5 & 5A, AND STANDARD R-B 350 GUIDERAIL.

**DELINEATOR NOTES:**

- DELINEATORS SHALL BE FORMED OF .080 POLY-CARBONATE OR .080 SHEET ALUMINUM IN ACCORDANCE WITH M.18.13.
- REFLECTIVE SHEETING SHALL CONFORM TO M.18.09.2.
- DELINEATORS SHALL BE INSTALLED ON THE POST CLOSEST TO THE DESIGNATED SPACING.
- REFLECTIVE SHEETING SHALL BE WHITE EXCEPT ON THE LEFT SIDE OF DIVIDED STREETS, HIGHWAYS, RAMPS, AND ONE WAY ROADS IN THE DIRECTION OF TRAVEL WHERE IT SHALL BE YELLOW.
- INSTALL DELINEATORS ON RAIL THAT IS PARALLEL TO AND NOT GREATER THAN 6'(1829) FROM THE EDGE OF THE ROADWAY. A MINIMUM OF THREE DELINEATORS MUST BE INSTALLED ON ANY RUN OF RAIL.

DELINEATOR SPACING:  
RADIUS ≥ 300'(91440) - SPACE EVERY 50'(15,24m)  
RADIUS < 300'(91440) - SPACE EVERY 25'(7,62m)

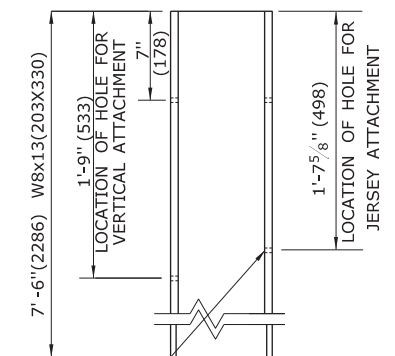
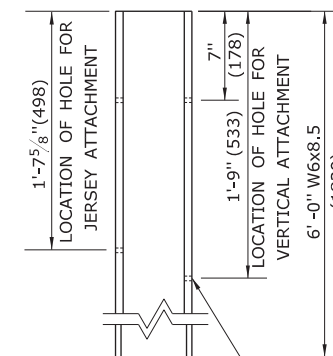
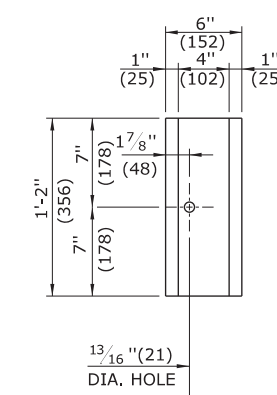
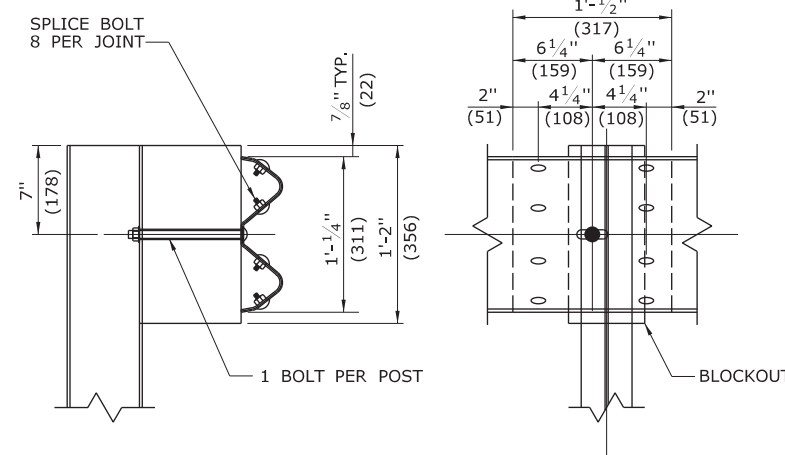
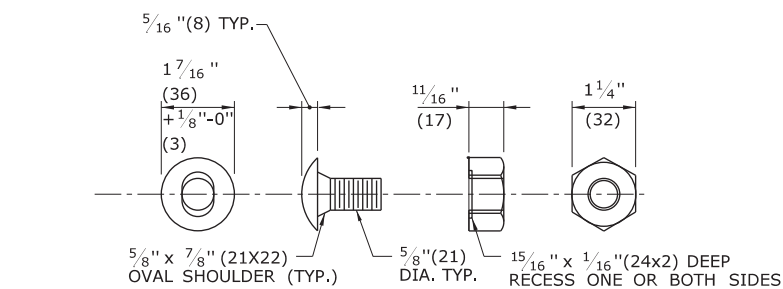


**W-BEAM SPLICE BOLT DETAIL**

**POST BOLT DETAIL FOR R-B 350 SYSTEM 6 RUBRAIL**

**POST BOLT DETAIL FOR R-B 350 AND MD-B 350 GUIDERAIL**

**PLAN**



**W6x8.5 POST (W150x13) 6'-0"(1829) LONG**

**W8x13 POST (W200x19) 7'-6"(2286) LONG**

**BOLT HOLE LAYOUT FOR W8x13(W200x19) AND W6x8.5 (W150x13) UNIFORM POST**  
(REFER TO GENERAL NOTES)

1	6/11	REMOVE WEATHERING STEEL NOTES	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
REV.	DATE	REVISION DESCRIPTION	

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 5/10/2011

NOT TO SCALE

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT-HIGHWAY-STD-JUNE2011.dgn Model: 36-HW-910-01

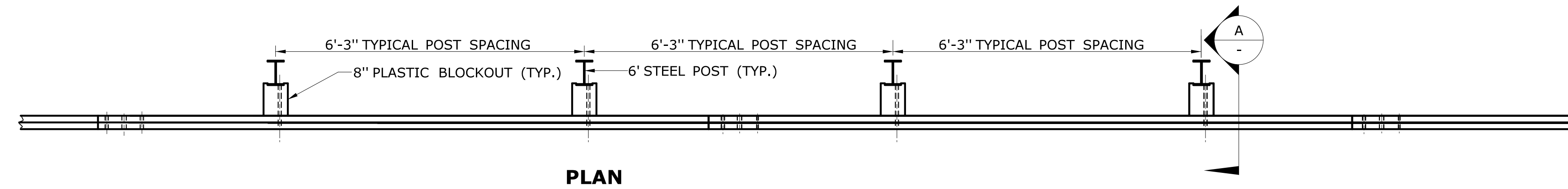
SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:
	James H. Norman 2011.06.09 15:12:16 -04'00'

CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING

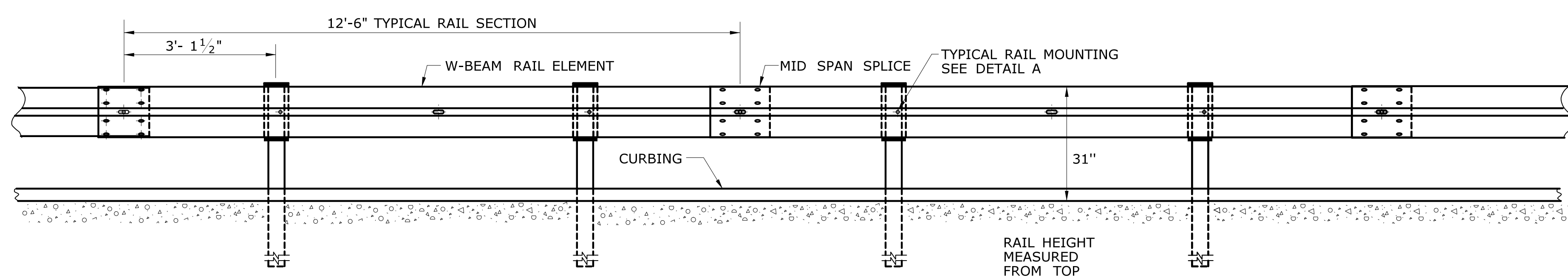
STANDARD SHEET TITLE:	STANDARD SHEET NO.:
W-BEAM METAL BEAM RAIL HARDWARE	HW-910_01

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.



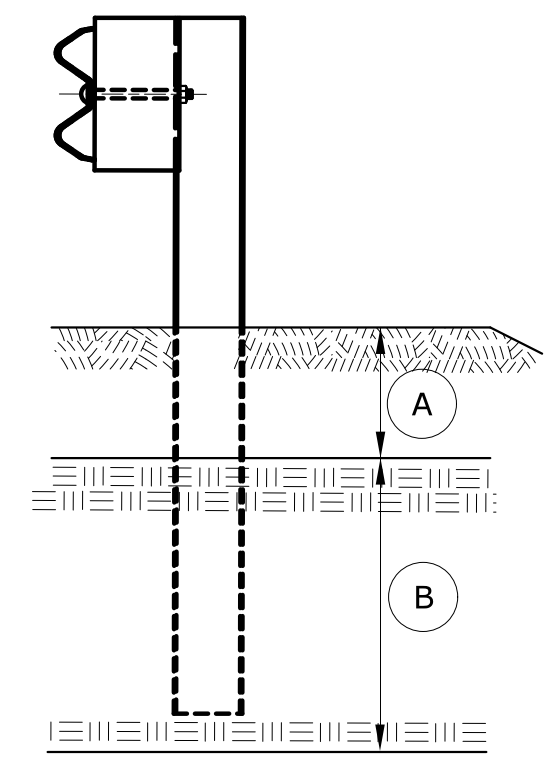


PLAN

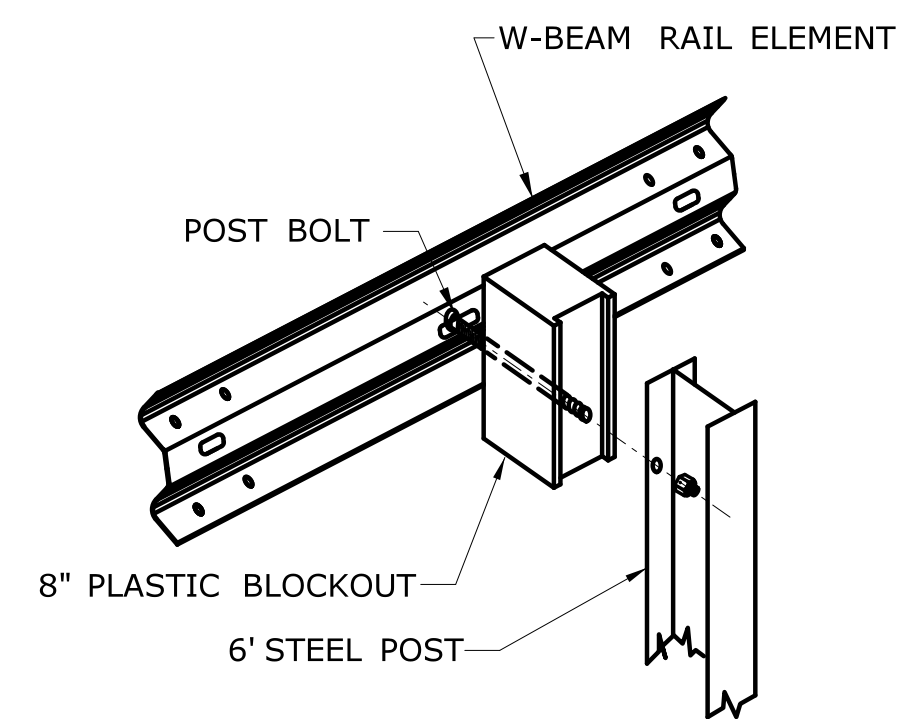


ELEVATION  
METAL BEAM RAIL (R-B MASH) GUIDERAIL

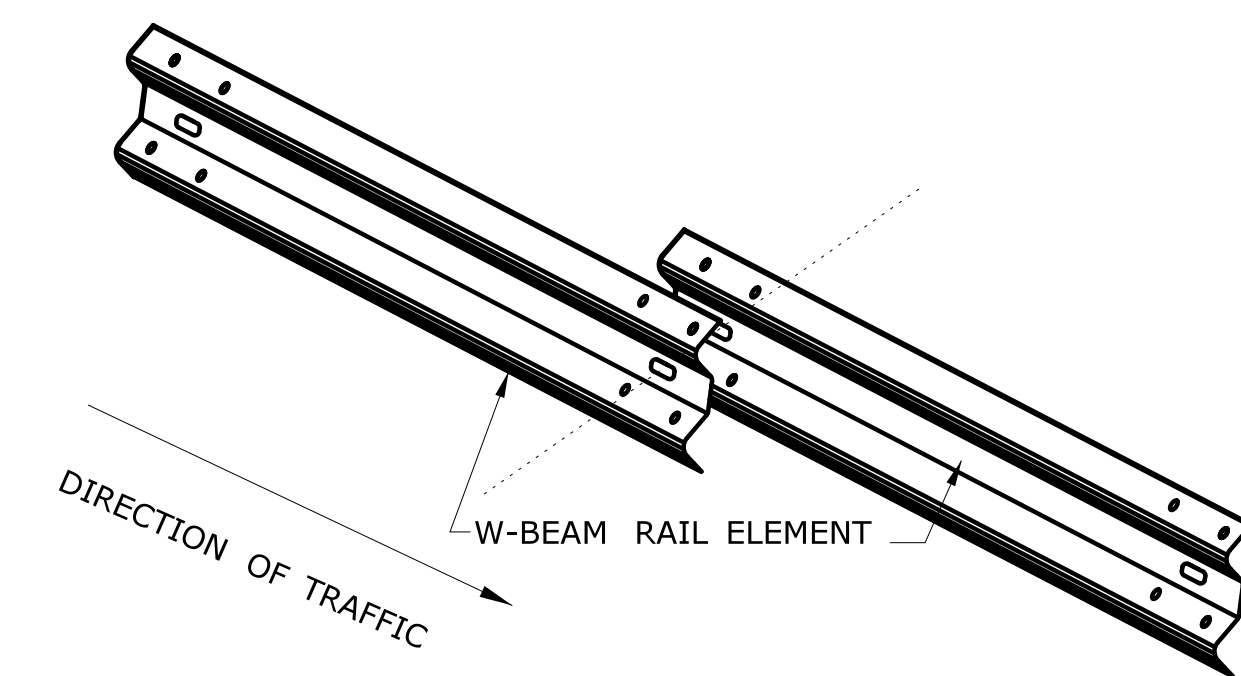
- GENERAL NOTES:**
- SEE SHEET HW-910\_20 FOR MASH W-BEAM HARDWARE AND W-BEAM DELINEATOR DETAILS.
  - THREE BLOCKOUTS MAY BE USED FOR ONE POST ONLY. TWO BLOCKOUTS MAY BE USED FOR A SERIES OF POSTS. THE COST OF ADDITIONAL BLOCKOUTS AND LONGER BOLTS SHALL BE INCLUDED IN THE PRICE PER FOOT OF GUIDERAIL. EXTRA BLOCKOUTS AT TRANSITIONS TO BRIDGE PARAPETS SHOULD BE AVOIDED. DO NOT USE ADDITIONAL BLOCKS IF IT CAUSES THE POST TO BE DRIVEN BEYOND AN EMBANKMENT HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.
  - IF BLOCKOUTS DO NOT AVOID POST FROM OBSTRUCTION, ONE POST MAY BE OMITTED IF 50 FEET OF GUIDERAIL EXISTS ON BOTH SIDES OF LOCATION. USE METAL BEAM RAIL SPAN SECTION TYPE II OR III FOR MORE THAN ONE CONSECUTIVE OMITTED POST, SEE SHEET HW-910\_24.
  - W-BEAM GUIDERAIL MAY BE PLACED 1' OR MORE FROM THE EDGE OF PAVEMENT ONLY ON SLOPES 10:1 OR FLATTER AND WITHOUT CURBING.
  - IF THE RAIL IS INSTALLED WITHIN 2' OF THE EDGE OF PAVEMENT, THE RAIL HEIGHT IS MEASURED FROM THE SHOULDER SLOPE EXTENDED TO THE RAIL. IF THE RAIL IS INSTALLED BEYOND 2' FROM THE EDGE OF PAVEMENT, THE RAIL HEIGHT IS MEASURED FROM THE GROUND DIRECTLY BELOW THE RAIL.
  - RAIL HEIGHT CONSTRUCTION TOLERANCE IS +/- 1 INCH.



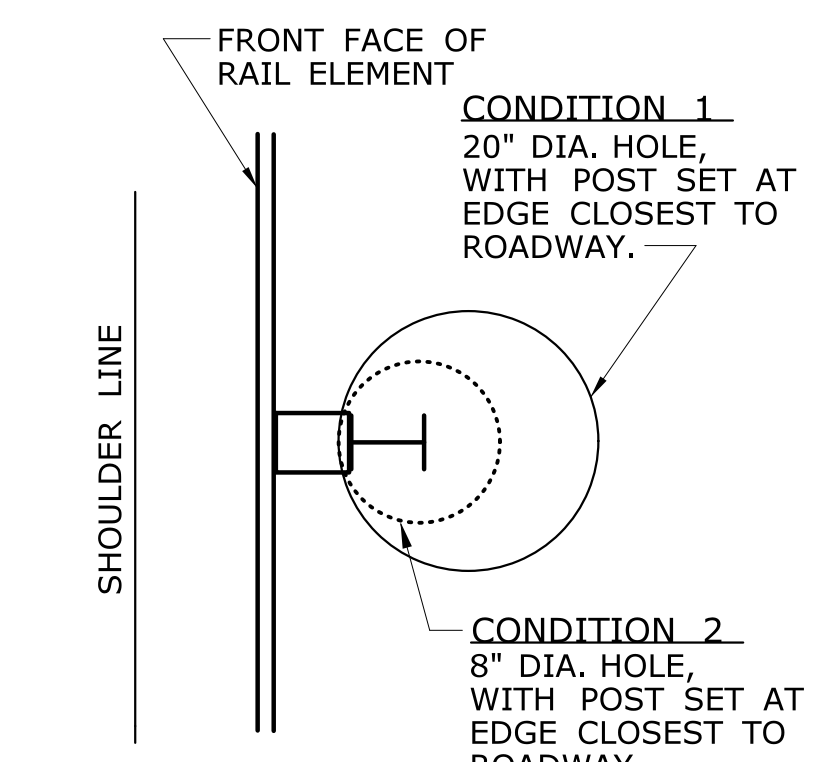
ELEVATION



DETAIL A  
RAIL MOUNTING

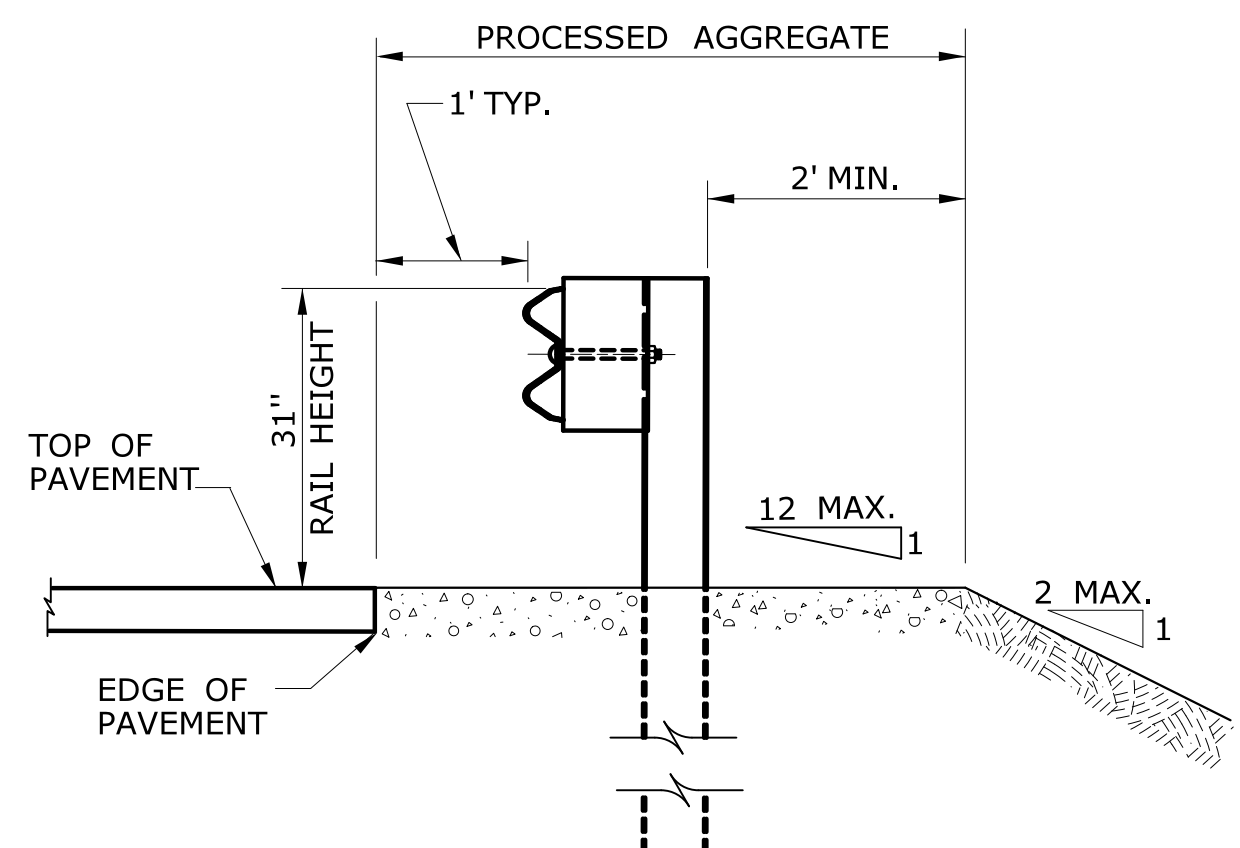


LAP W-BEAM RAIL SECTIONS  
NOTE: EIGHT (8) SPLICE BOLTS PER JOINT



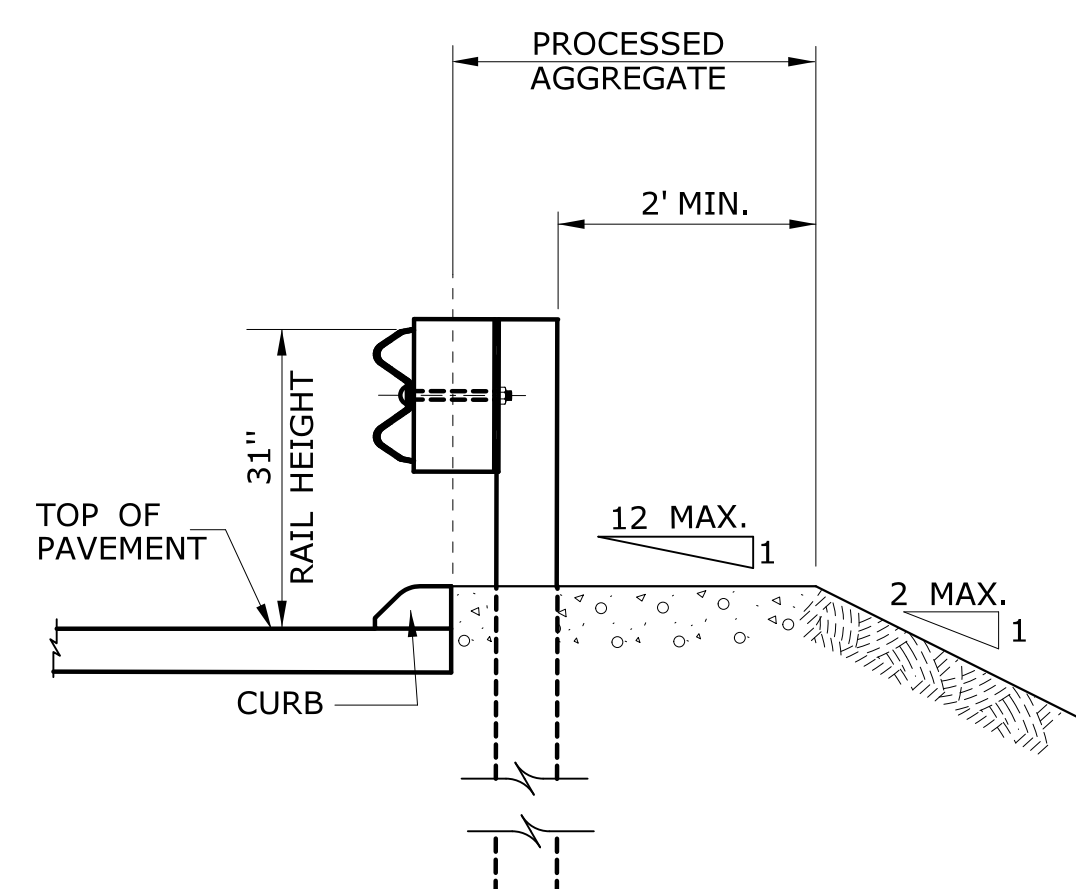
PLAN

GUIDERAIL POSTS IN ROCK



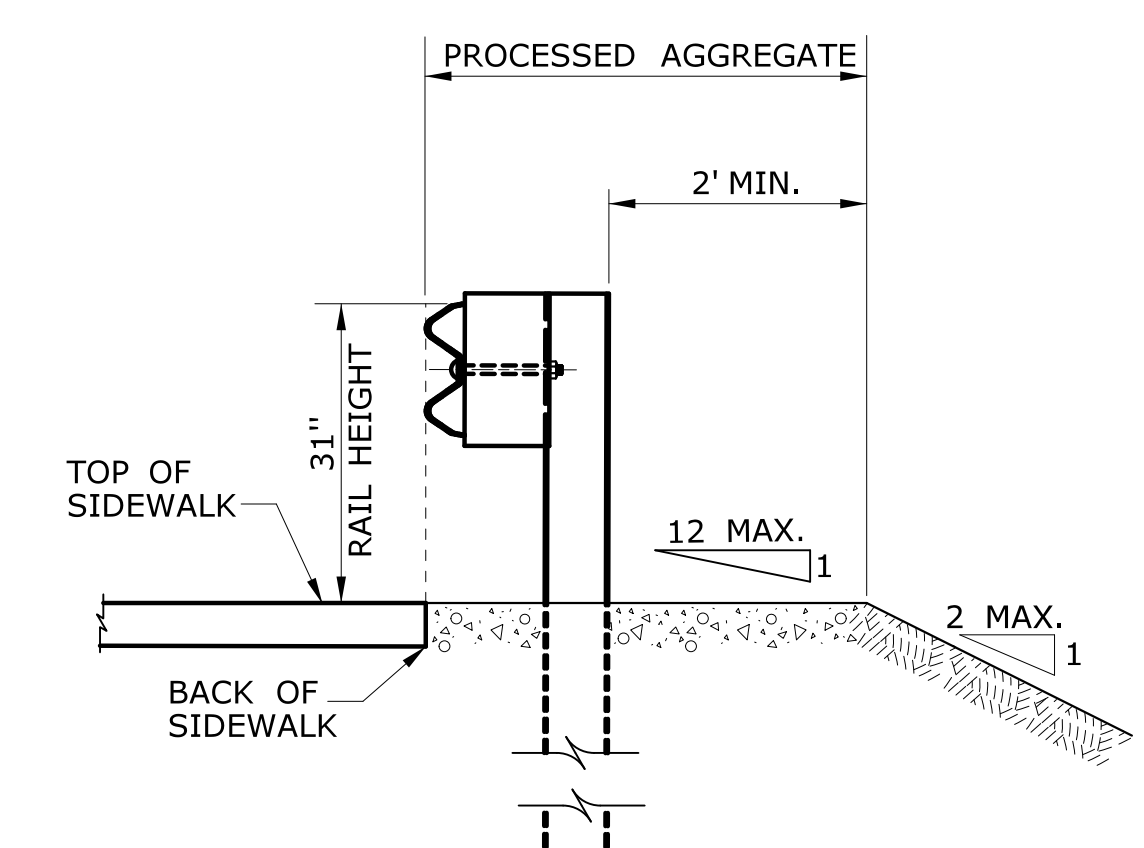
SECTION A

NO CURB APPLICATION



SECTION A

CURB APPLICATION



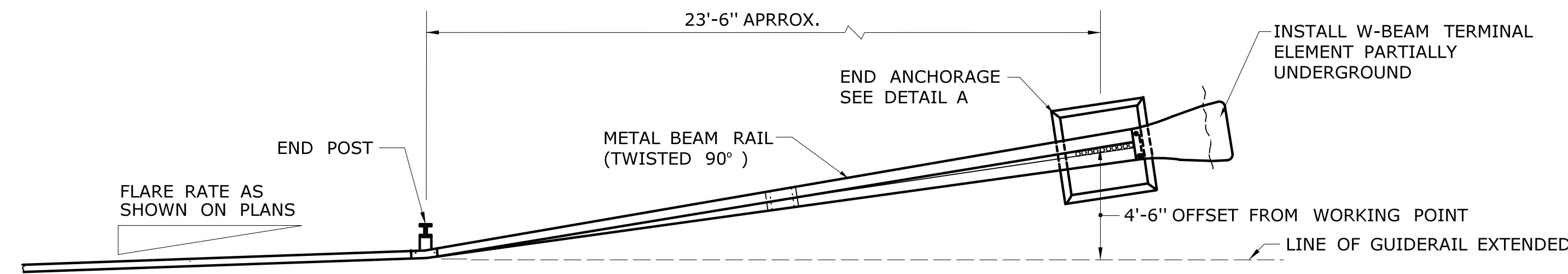
SECTION A

SIDEWALK APPLICATION

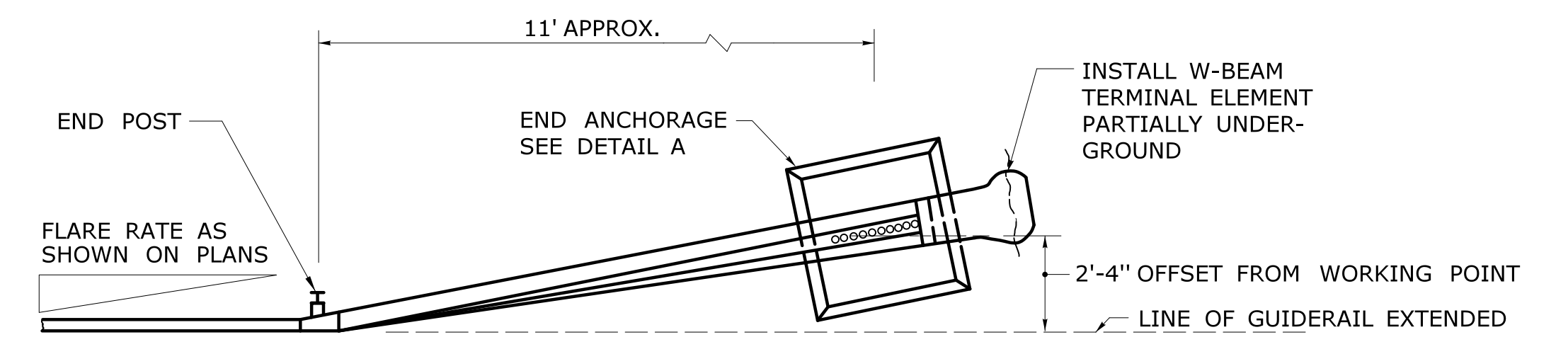
<p>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</p>		<p>NOT TO SCALE</p>	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	<p>SUBMITTED BY: _____ NAME/DATE/TIME: _____</p> <p>APPROVED BY: _____ NAME/DATE/TIME: _____</p>	<p>CTDOT STANDARD SHEET OFFICE OF ENGINEERING</p>	<p>STANDARD SHEET TITLE: METAL BEAM RAIL (R-B MASH) GUIDERAIL</p>	<p>STANDARD SHEET NO.: HW-910_21</p>
<p>REV. DATE</p>	<p>REVISION DESCRIPTION</p>	<p>Plotted Date: 1/3/2018</p>	<p>Filename: CTDOT_HIGHWAY_STD.dgn Model: 274 - HW-910_21</p>				

**GENERAL NOTES:**

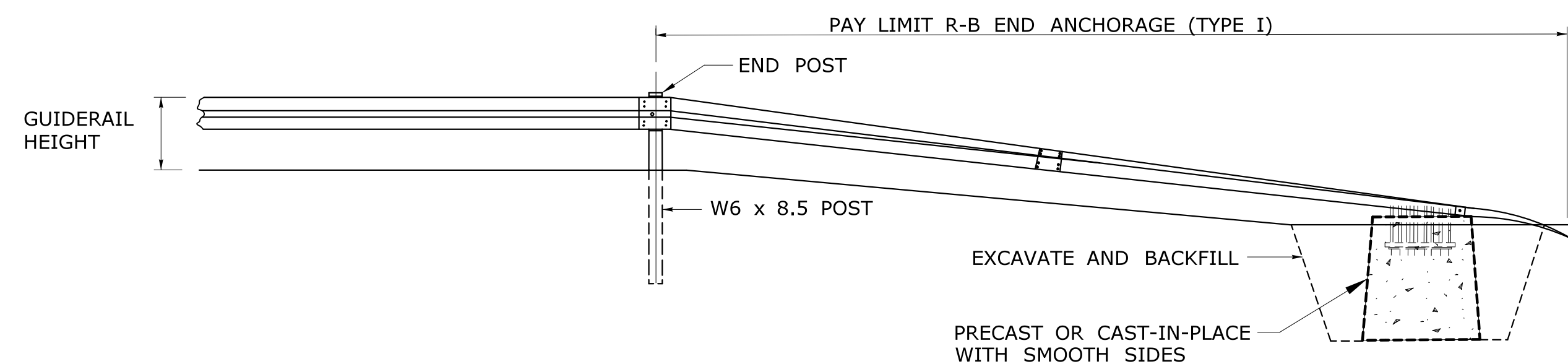
1. SEE SHEET HW-911\_04 FOR TYPICAL END ANCHOR GRADING PLAN.
2. J-HOOK BOLTS MAY BE SUBSTITUTED FOR BOTTOM PLATE ANCHORAGE IN CONCRETE END ANCHORS USING THE SAME SIZE, STRENGTH, AND LENGTH AS NOTED ON THE PLANS.
3. INSTALLATION OF RADII DIFFERENT THAN WHAT IS SHOWN IN DETAIL "C" FOR R-B END ANCHORAGE TYPE II MUST BE APPROVED BY THE ENGINEER.



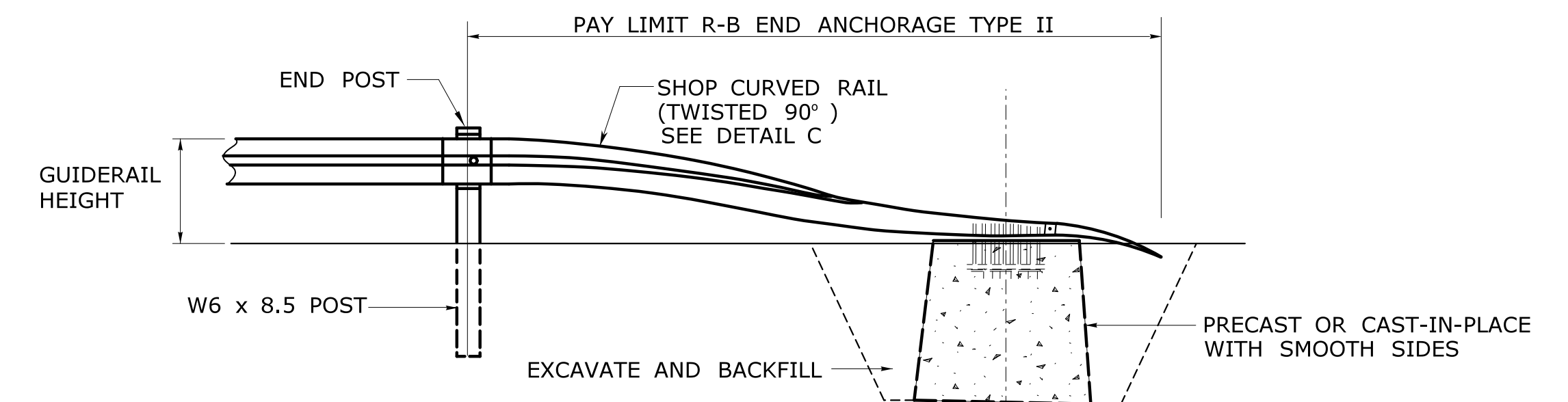
**PLAN**



**PLAN**



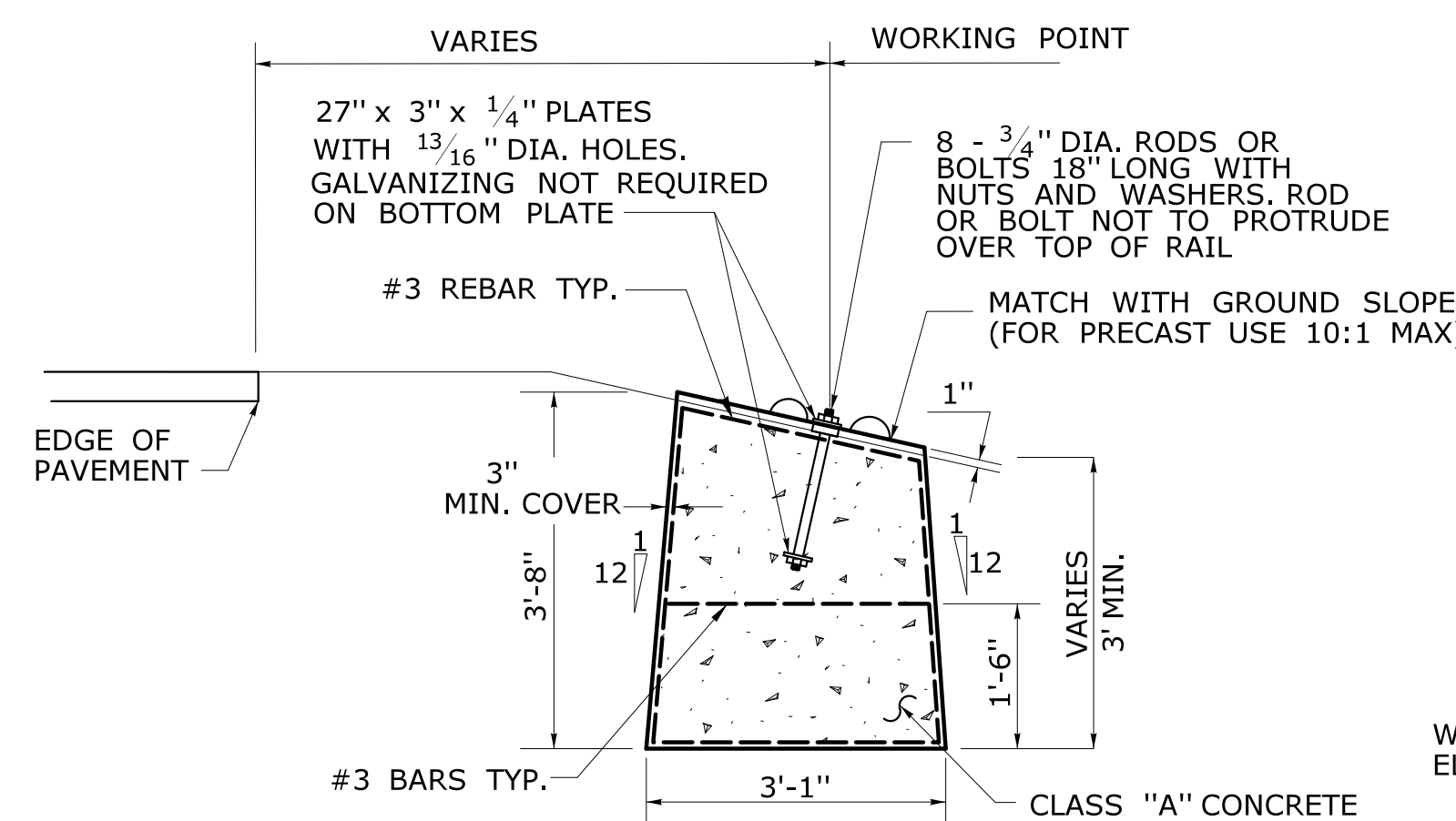
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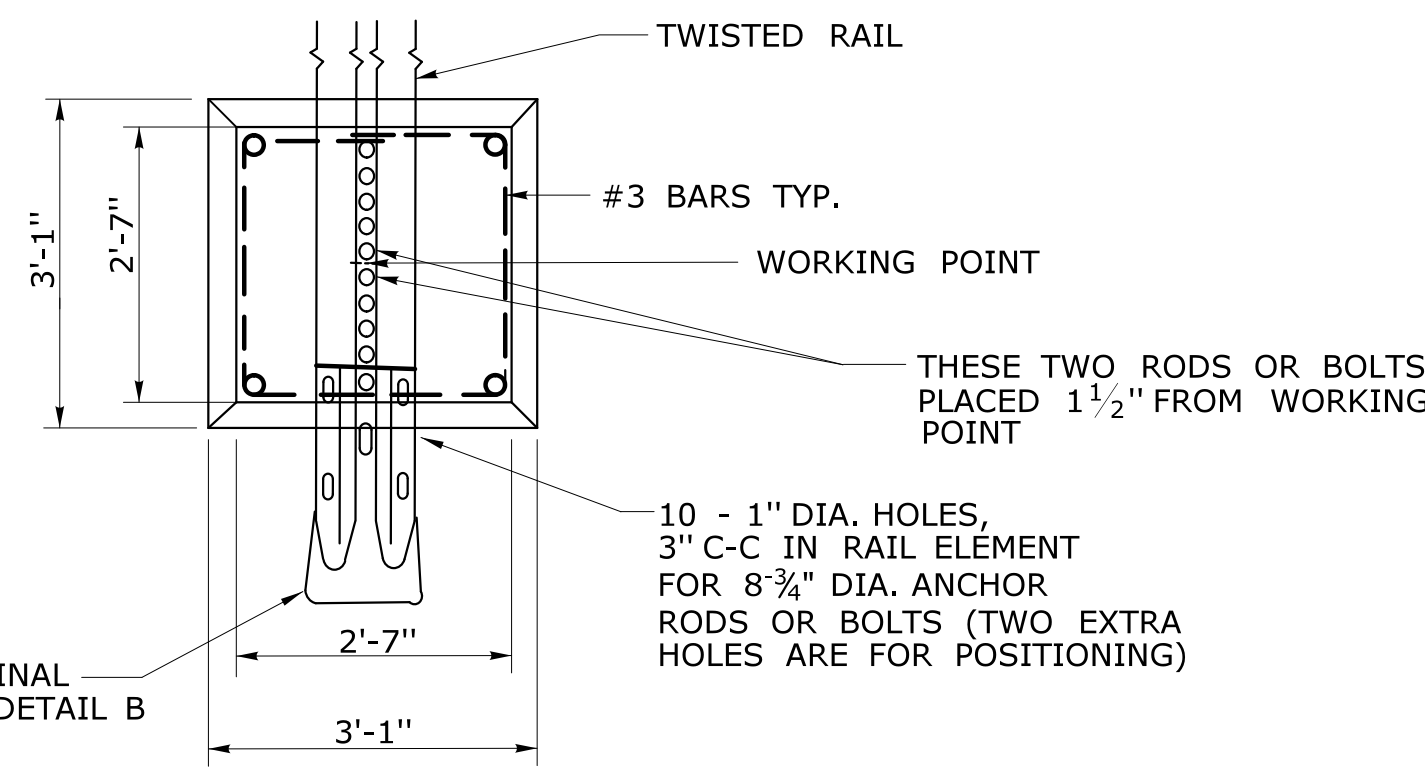
**ELEVATION**

**R-B END ANCHORAGE TYPE I**

**R-B END ANCHORAGE TYPE II**



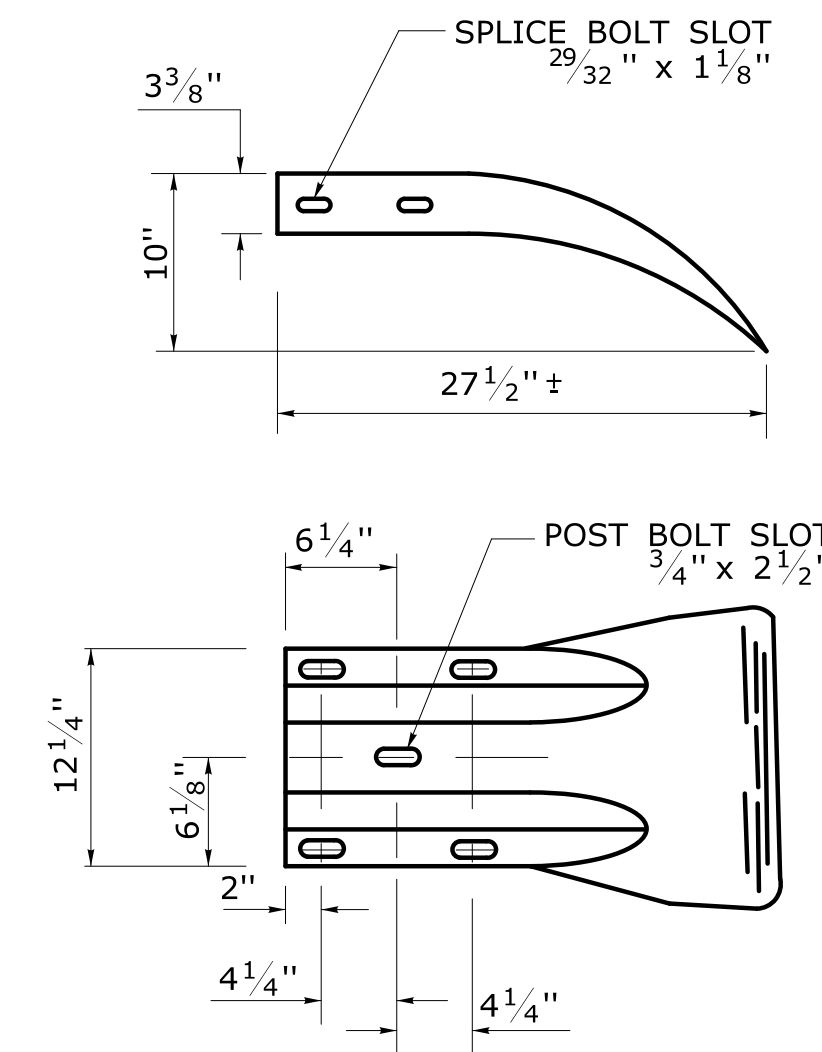
**ELEVATION**



**PLAN**

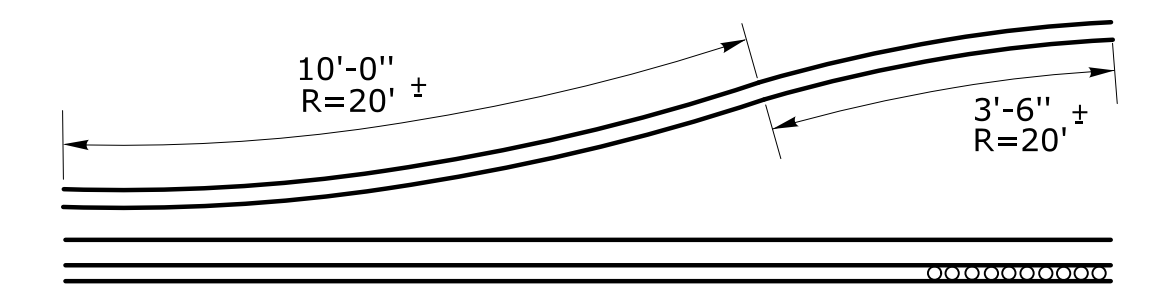
**DETAIL A  
ROADSIDE CONCRETE END ANCHOR**

SEE NOTE 2



**DETAIL B**

**W-BEAM TERMINAL ELEMENT**



**DETAIL C**

**SHOP CURVED RAIL**

SEE NOTE 3

1	6/11	REVISED TYPE I AND II ANCHOR FOR CLEAR ZONE PLACEMENT
2	7/13	ADD POST OFFSET DISTANCE
3	9/17	REVISED TYPE I AND II FOR R-B MASH OR R-B 350 RAIL
REV.	DATE	REVISION DESCRIPTION

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Plotted Date: 1/3/2018

NOT TO SCALE



Filename: CTDOT\_HIGHWAY\_STD.dgn Model: 282 - HW-911\_01

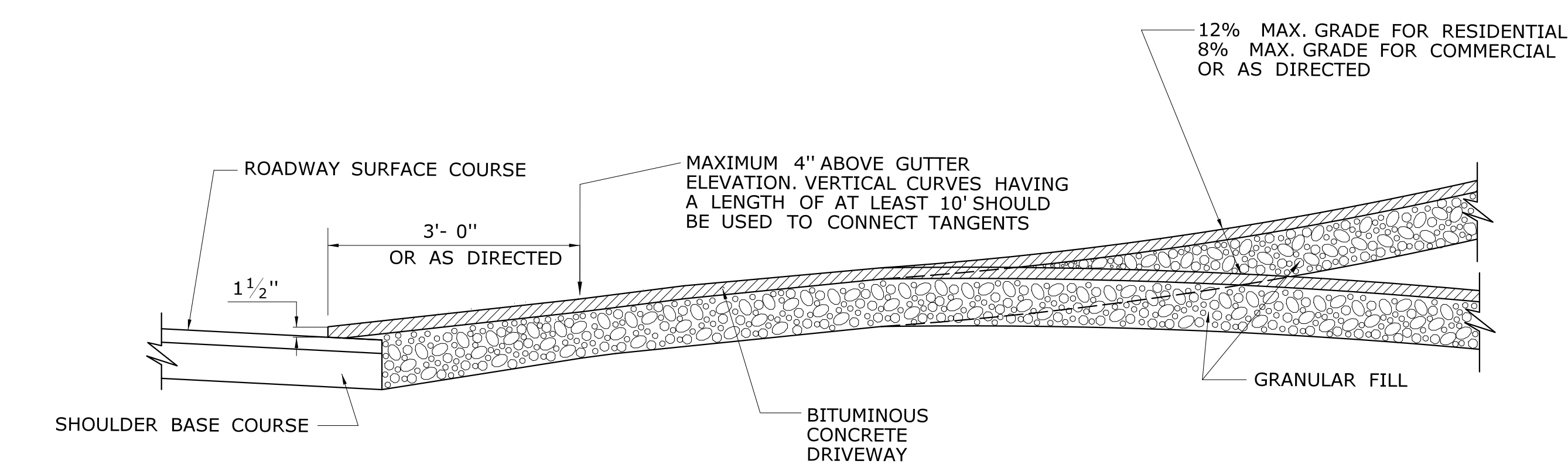
SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

**CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING**

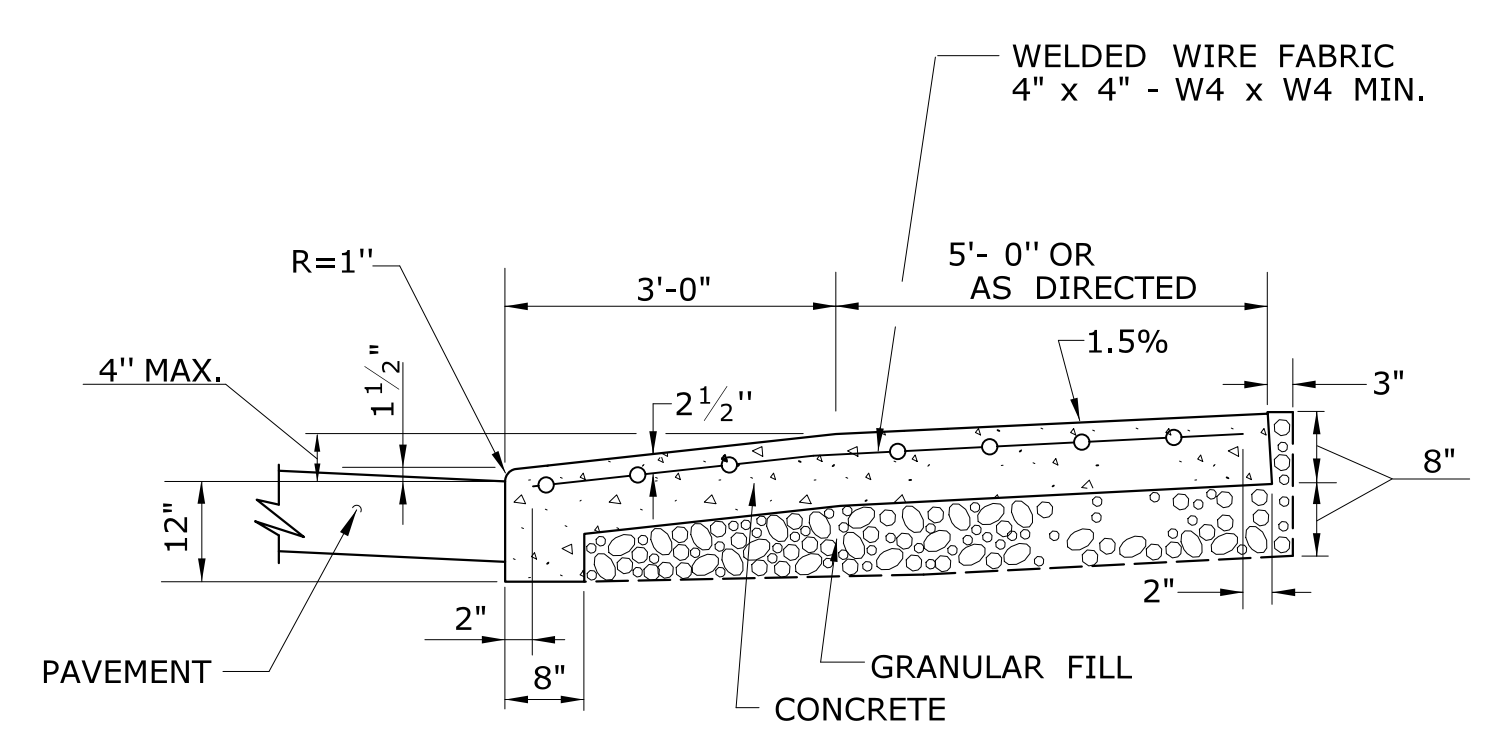
STANDARD SHEET TITLE:	STANDARD SHEET NO.:
<b>R-B END ANCHORAGE TYPE I AND II</b>	<b>HW-911_01</b>



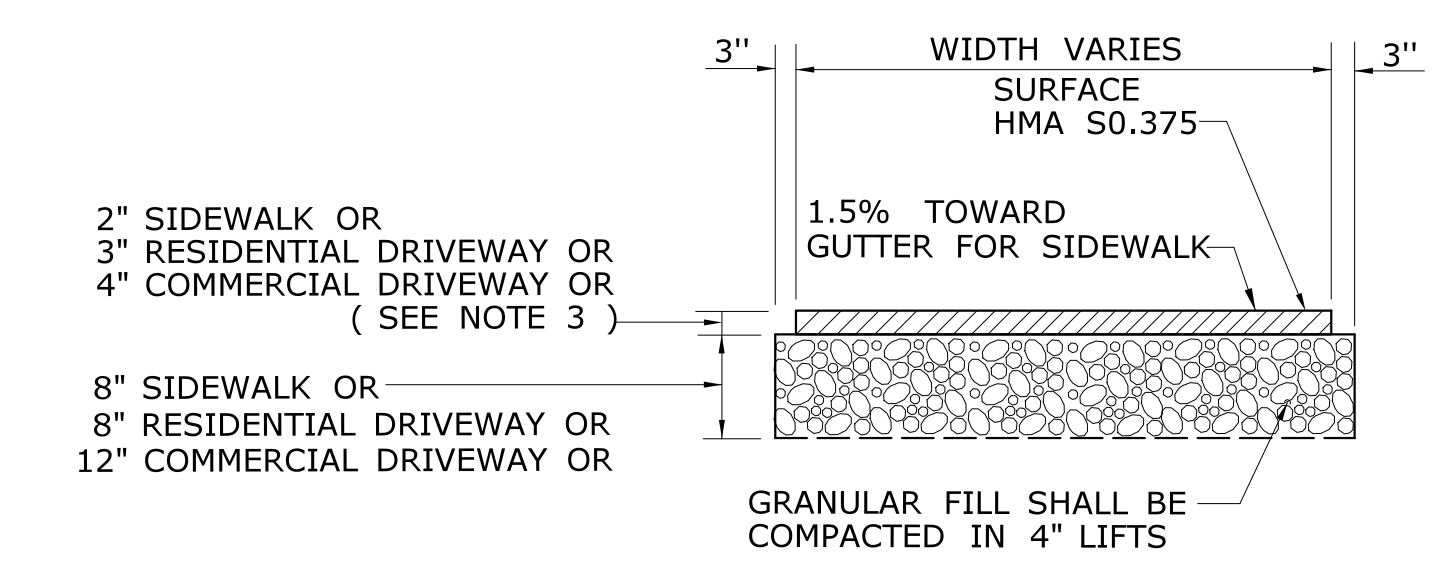
- GENERAL NOTES:**
1. DRIVEWAY ENTRANCE SHALL BE A MINIMUM OF 12' WIDE, EXCLUDING CURBING WHEN PRESENT.
  2. WELDED WIRE FABRIC MATS WITH REINFORCING AT CLOSER SPACING MAY BE USED.
  3. SURFACE HMA S0.375 TO BE PLACED IN TWO EQUAL LIFTS FOR BOTH RESIDENTIAL AND COMMERCIAL DRIVEWAYS.



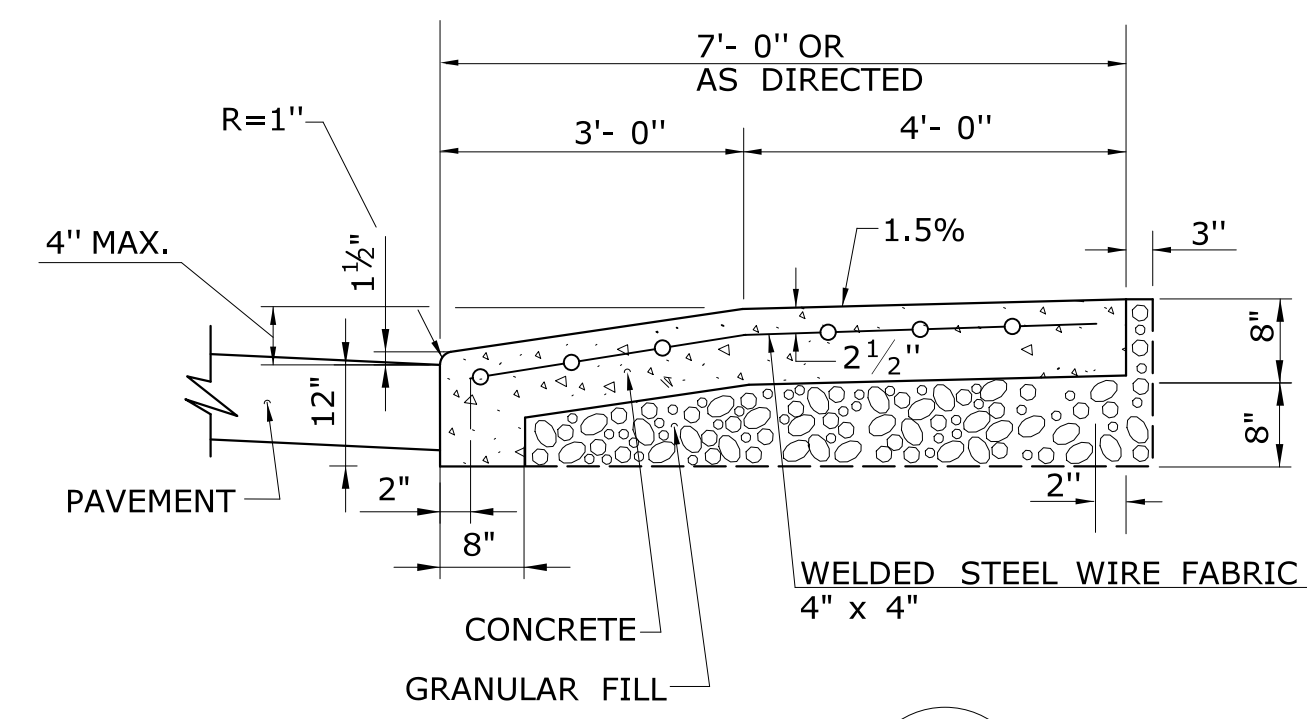
**SECTION A**



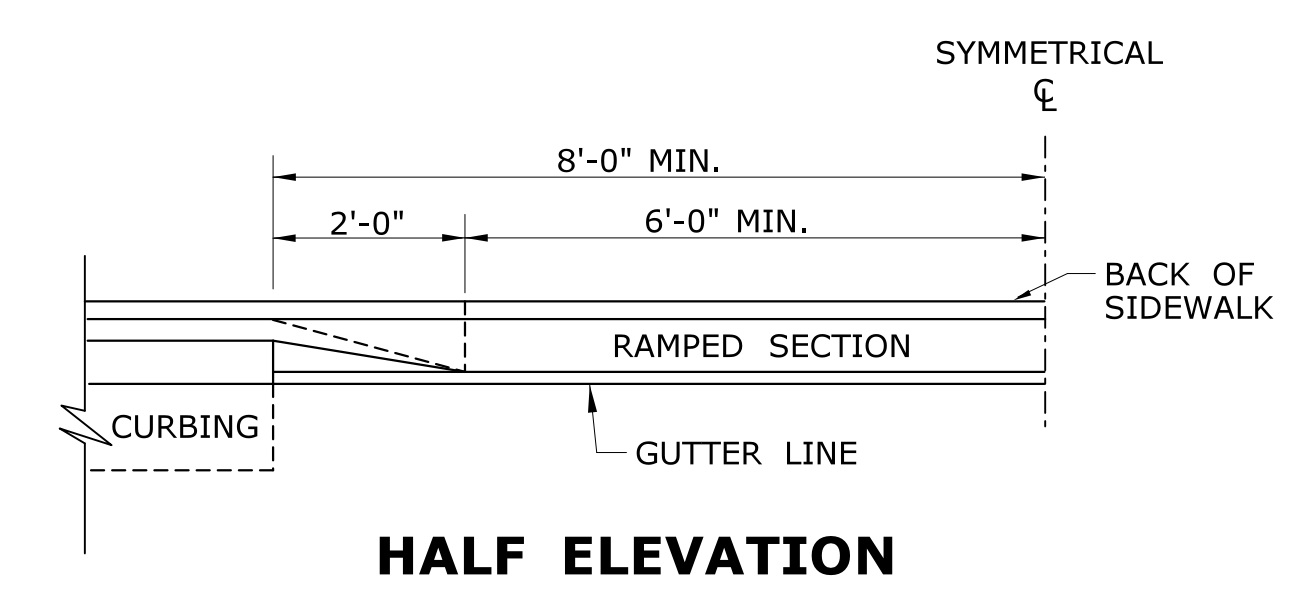
**SECTION C**



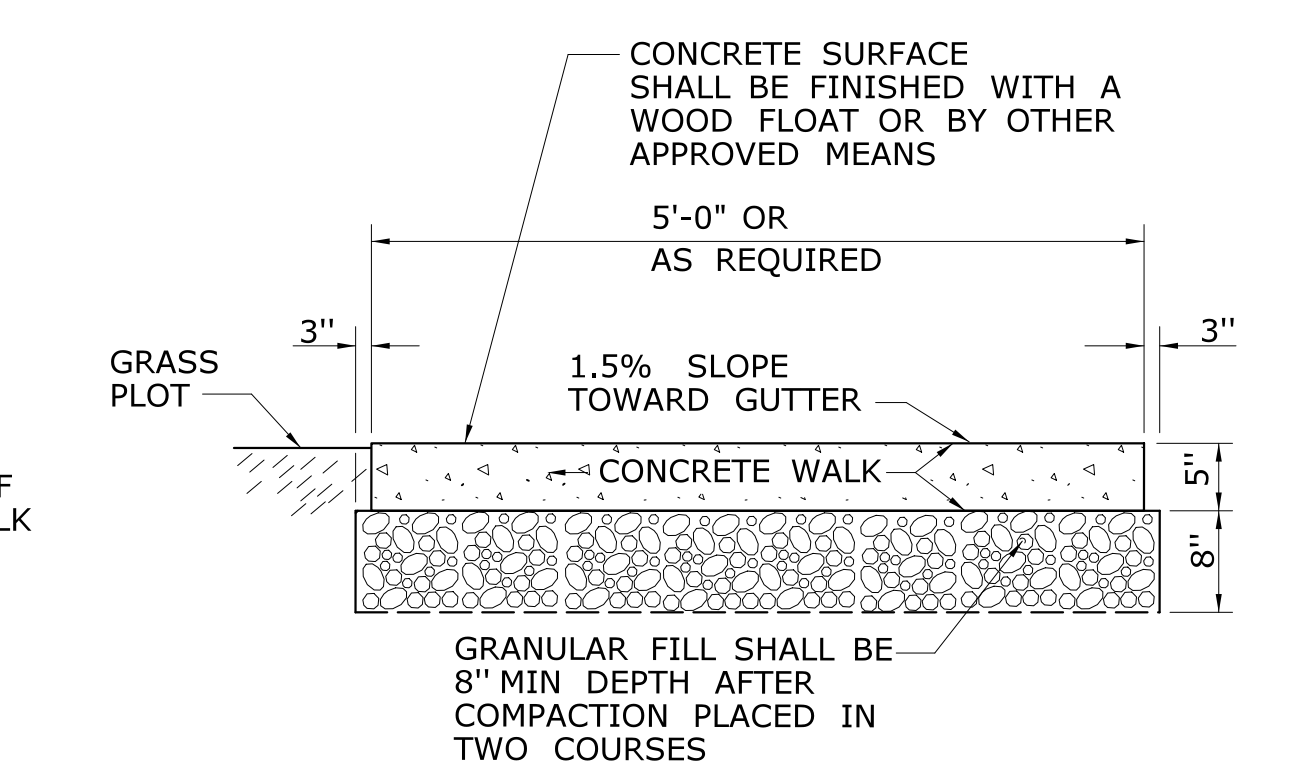
**TYPICAL SECTION BITUMINOUS CONCRETE SIDEWALK AND DRIVEWAY**



**SECTION B**

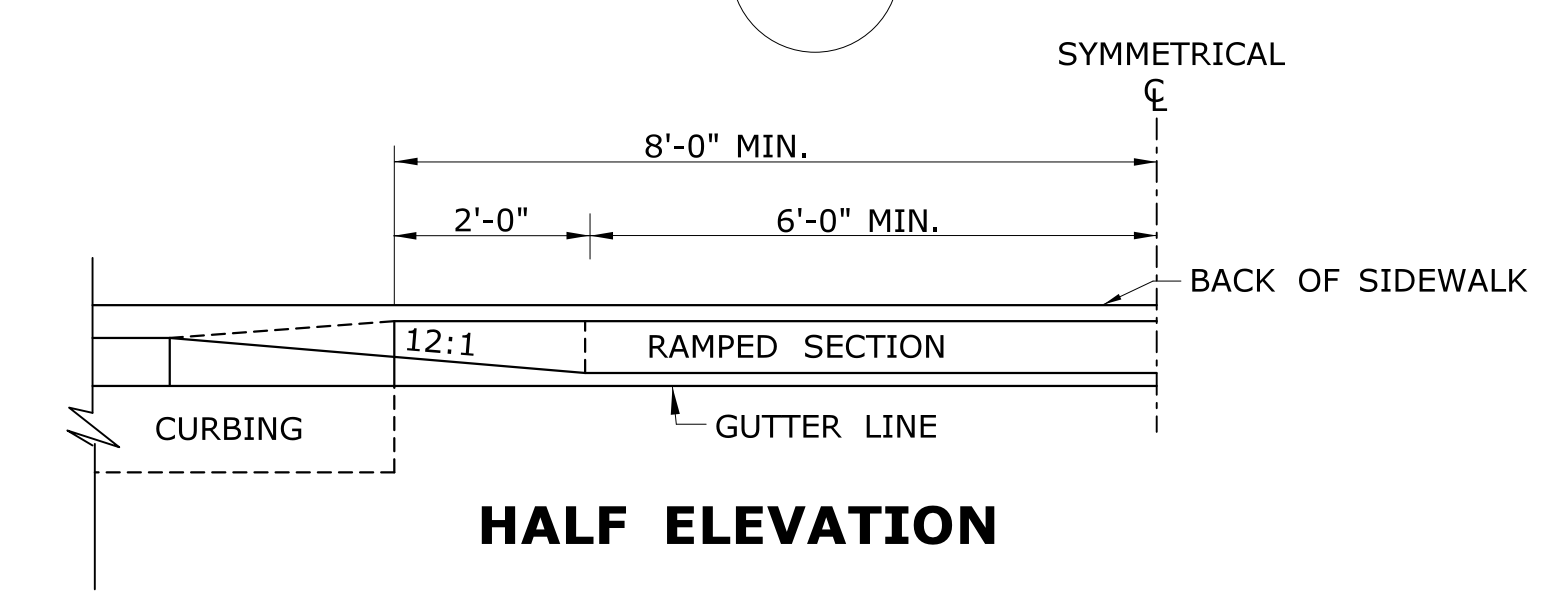


**HALF ELEVATION**

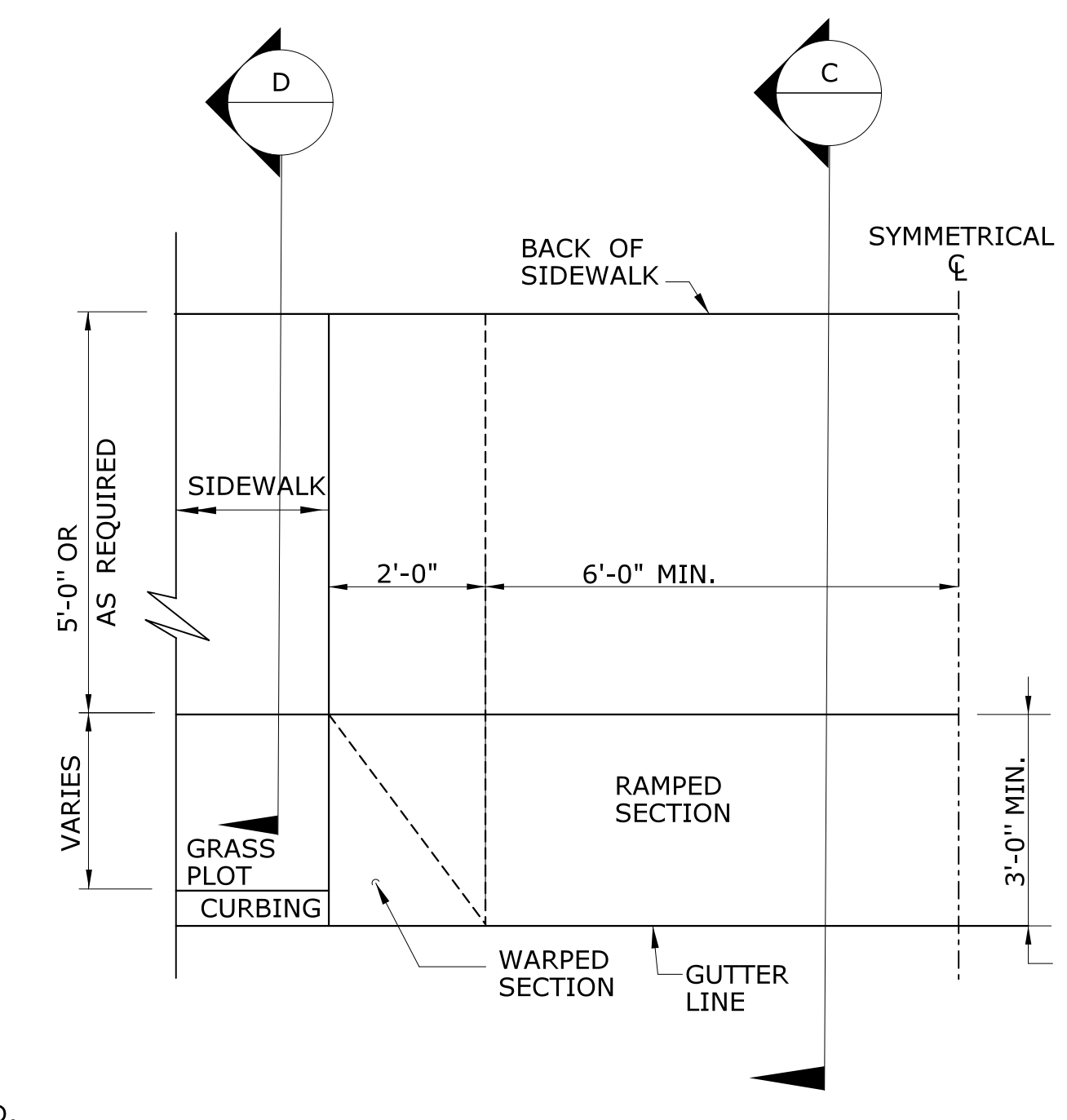


**SECTION D**

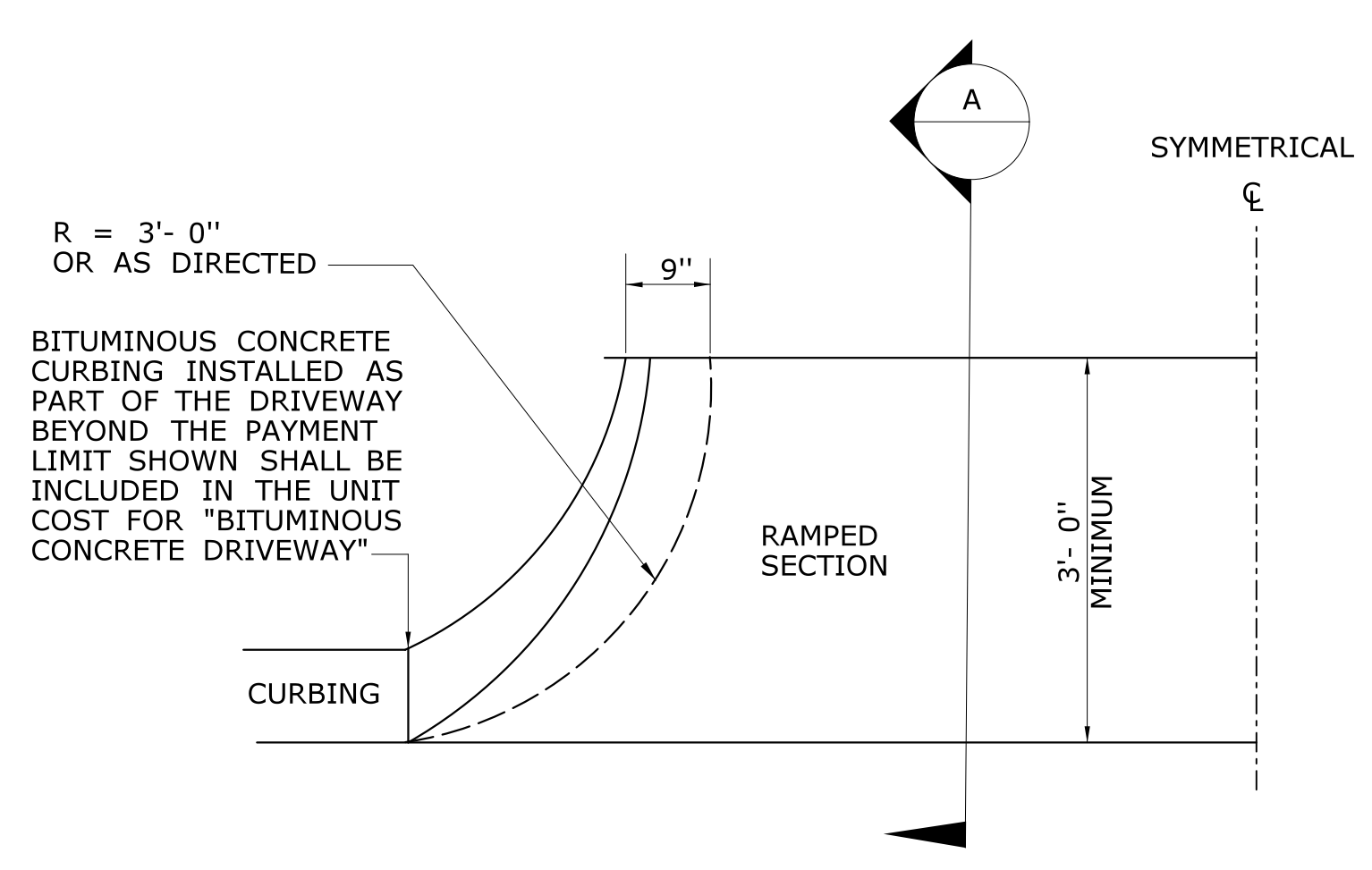
**5' WIDE CONCRETE SIDEWALK WITH GRASS PLOT**



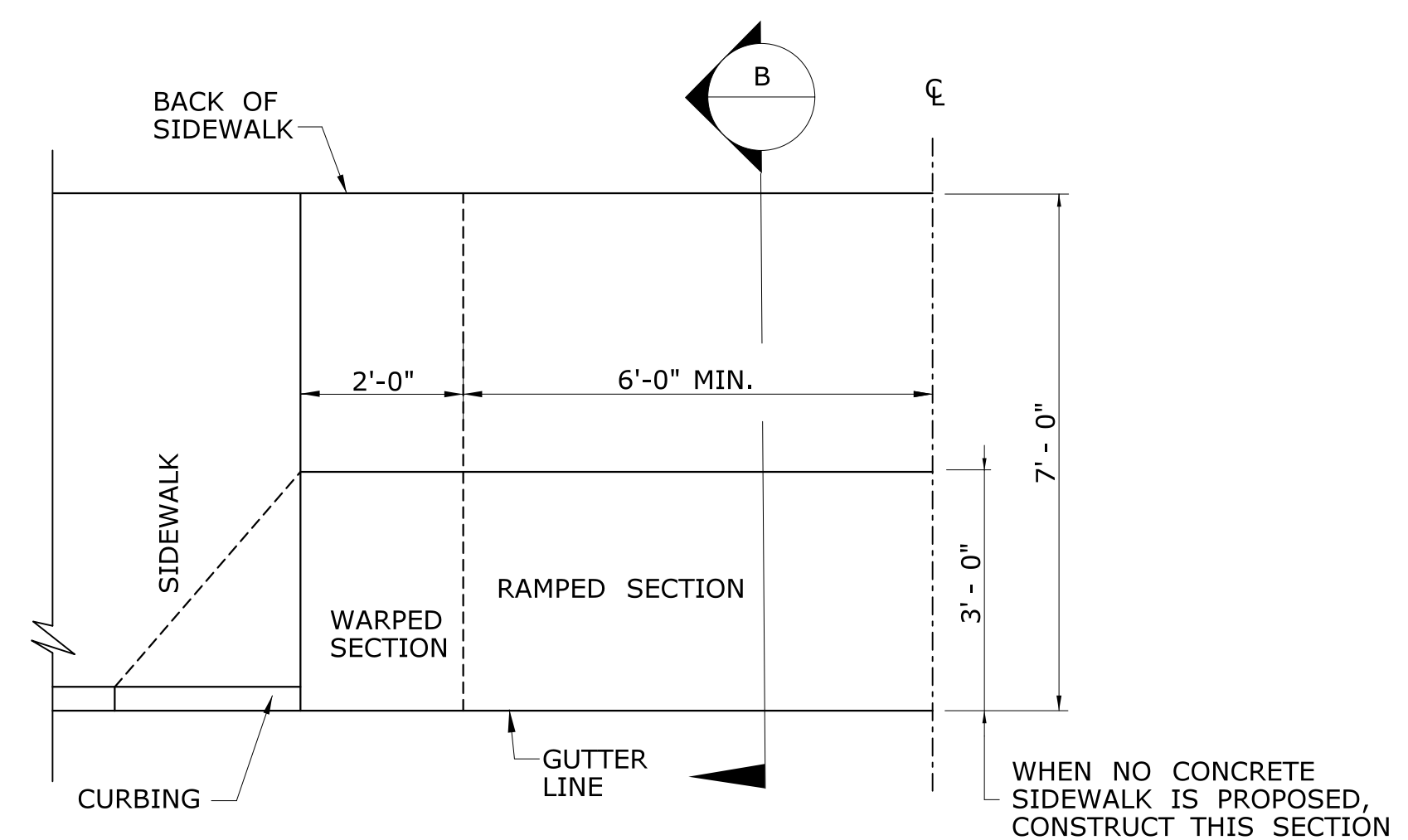
**HALF ELEVATION**



**HALF PLAN OF CONCRETE DRIVEWAY RAMP WHERE CURB IS SEPARATED FROM SIDEWALK BY GRASS PLOT**



**HALF BITUMINOUS CONCRETE DRIVEWAY PLAN**



**HALF PLAN OF CONCRETE DRIVEWAY RAMP WHERE SIDEWALK ADJOINS CURBING**

1	6/01/10	REVISED BORDER TITLE	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		SUBMITTED BY: _____ NAME/DATE/TIME: _____	<b>CTDOT STANDARD SHEET</b>  <b>OFFICE OF ENGINEERING</b>	STANDARD SHEET TITLE:	STANDARD SHEET NO.:
2	6/01/10	REVISED HALF ELEVATION DETAILS			APPROVED BY: _____ NAME/DATE/TIME: _____		<b>DRIVEWAY RAMPS AND SIDEWALKS</b>	<b>HW-921_01</b>
3	1/12	REVISE 2% MAX. SLOPE NOTE						
4	6/17	REVISED SLOPES & MATERIAL COMPOSITIONS						
-	-	INCREASED WALKING WIDTH OF CONCRETE DRIVEWAY RAMP						
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 6/6/2017	NOT TO SCALE	Filename: HW-921_01.dgn Model: CT-Civil-2D_Sheet			





DOCUMENT ALL LOOP DETECTOR VALUES BOTH CALCULATED AND MEASURED.

**DEFINITIONS:**

LOOP: #14 AWG WIRE IN SAWCUT, TERMINATED IN HANDHOLE, IMSA SPEC 51-7.  
 LEAD-IN: 14/2 SHIELDED TWISTED PAIR CABLE FROM HANDHOLE TO CONTROLLER, IMSA SPEC 50-2.  
 LOOP CIRCUIT: LOOP SAWCUT WIRE SPLICED TO 14/2 LEAD-IN CABLE.  
 AMPLIFIER: ELECTRONIC DEVICE CONNECTED TO LOOP CIRCUIT. SENSES CHANGE IN RESONANT FREQUENCY AND CREATES AN OUTPUT TO THE CONTROLLER.  
 MEGOHMMETER: INSTRUMENT SPECIFICALLY DESIGNED TO TEST THE INSULATION RESISTANCE OF A CIRCUIT. COMMON MANUFACTURERS: AMEC®, AMPROBE®, FLUKE®, MEGGER®.

**1: RESISTANCE:**

1a: INSULATION RESISTANCE: PERFORM A 600 VOLT (MINIMUM) MEGOHMMETER TEST ON LOOP CIRCUIT. THE LOOP AMPLIFIER MUST BE DISCONNECTED FROM THE LOOP CIRCUIT OR THE LOOP AMPLIFIER WILL BE DAMAGED. THE RESISTANCE OF THE LOOP WIRE TO GROUND MUST BE GREATER THAN 100 MEG OHMS.

1b: WIRE RESISTANCE: MEASURE THE DC RESISTANCE OF THE LOOP CIRCUIT. THE LOOP CIRCUIT MUST BE DISCONNECTED FROM THE AMPLIFIER. USING AN OHMMETER CONNECTED ACROSS THE LOOP CIRCUIT, MEASURE THE DC RESISTANCE OF THE CONDUCTORS. THE RESISTANCE SHOULD BE LESS THAN 4 OHMS.

NOTE: ALL TESTS SHALL BE DONE AT THE CONTROLLER ASSEMBLY (CA), HOWEVER IT IS RECOMMENDED TO PERFORM A PRELIMINARY MEGOHMMETER TEST AT THE HANDHOLE PRIOR TO SEALING THE SAWCUT AND SPLICING TO THE LEAD-IN. IF A DEFECTIVE LOOP WIRE IS FOUND, IT MAY BE EASILY REPLACED.

**2: LOOP CIRCUIT INDUCTANCE:**

2a: CALCULATE INDUCTANCE OF LOOP (L<sub>LOOP</sub>) AND LEAD-IN CABLE (L<sub>14/2</sub>).

<b>LOOP INDUCTANCE (ENGLISH)</b>	<b>LOOP INDUCTANCE (METRIC)</b>
$L_{LOOP} = (P/4) (N^2 + N)$	$L_{LOOP} = (3.28P/4) (N^2 + N)$
<b>LEAD-IN INDUCTANCE</b>	<b>LEAD-IN INDUCTANCE</b>
$L_{14/2} = (0.24 \mu h/FT) (D)$	$L_{14/2} = (0.78 \mu h/m) (D)$

WHERE:  
 L<sub>LOOP</sub> = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS IN MICROHENRIES (μh).  
 L<sub>14/2</sub> = INDUCTANCE OF LEAD-IN CABLE.  
 P = PERIMETER OF INDIVIDUAL LOOP SEGMENT, IN FEET OR METERS.  
 N = NUMBER OF TURNS.  
 D = LENGTH OF LEAD-IN CABLE FROM SPLICE IN HANDHOLE TO CONTROLLER, IN FEET OR METERS.  
 $L_T = L_1 + L_2 + L_3$  etc.,  
 (TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN SERIES.)  
 $L_T = 1 / [(1/L_1) + (1/L_2) + (1/L_3) + \text{etc.}]$ ,  
 (TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN PARALLEL.)

WHERE:  
 L<sub>T</sub> = TOTAL INDUCTANCE OF THE SEGMENTED ARRANGEMENT.  
 L<sub>1</sub>, L<sub>2</sub>, L<sub>3</sub> = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS.

**EXAMPLE: (IN ENGLISH)**

6' x 6', 4 TURNS, APPROXIMATELY 300' FROM THE CONTROLLER

$L_{LOOP} = (24/4) (4^2 + 4)$	$L_{14/2} = (0.24 \mu h/FT) (300)$
$L_{LOOP} = (6) (20)$	$L_{14/2} = (0.24) (300)$
$L_{LOOP} = 120 \mu h$	$L_{14/2} = 72 \mu h$

2b: MEASURE INDUCTANCE OF LOOP AND LEAD-IN AT CONTROLLER. USE INSTRUMENT DESIGNED TO MEASURE LOOP CIRCUIT INDUCTANCE.

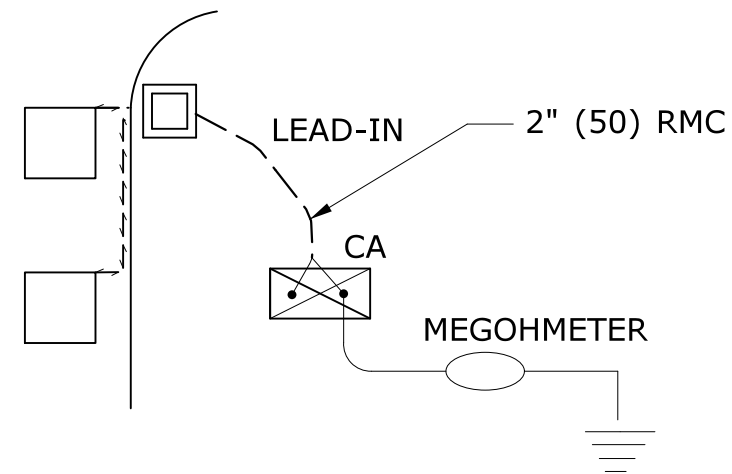
**3: POWER INTERRUPTION:**

AFTER THE AMPLIFIER HAS TUNED AND IS OPERATING, DISCONNECT POWER BY REMOVING FUSE OR HARNESS CONNECTOR. RETURN POWER TO THE AMPLIFIER AND CONFIRM IT RE-TUNES AUTOMATICALLY WITHOUT ANY MANUAL ADJUSTMENTS.

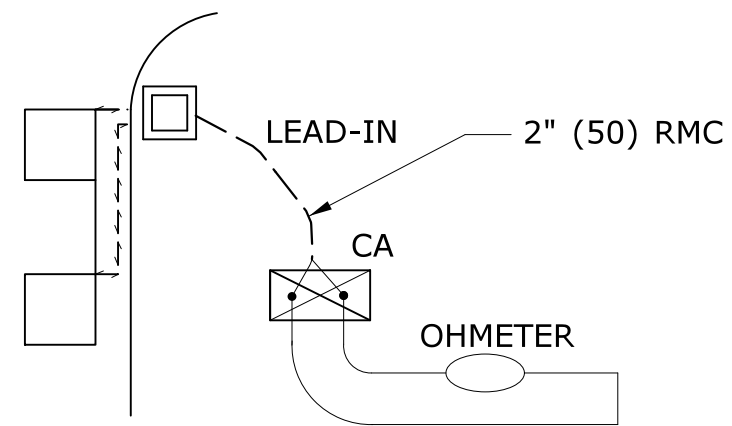
**INDUCTIVE LOOP TEST PROCEDURE**

PIN	COLOR	FUNCTION
A	WHITE	110 VAC Neutral
B	BROWN	Output Relay Common (moving contact)
C	BLACK	110 VAC (Fused)
D	RED	Loop
E	ORANGE	Loop
F	YELLOW	Output Relay Contact (Closes with moving contact when detecting vehicle)
G	BLUE	Output Relay Contact (Opens with moving contact when detecting vehicle)
H	GREEN	Chassis Ground
J	GREY	110 VAC Delay/Extend Override
Shell		Ground (shall be connected to pin H in the connector)

**DETECTOR AMPLIFIER PIN DESIGNATION**



**TEST 1a**



**TEST 1b**

LOOP NUMBER	RESISTANCE OHMS		INDUCTANCE MICROHENRIES (μh)		AMPLIFIER POWER INTERRUPTION PASS/FAIL (3)
	TO GROUND (1a)	LOOP WIRE (1b)	CALCULATED (2a)	MEASURED (2b)	
D1 FRONT					
D1 REAR					
D2A					
D2B					
D4A FRONT					
D4B REAR					
D5					
D6A					
D6B					

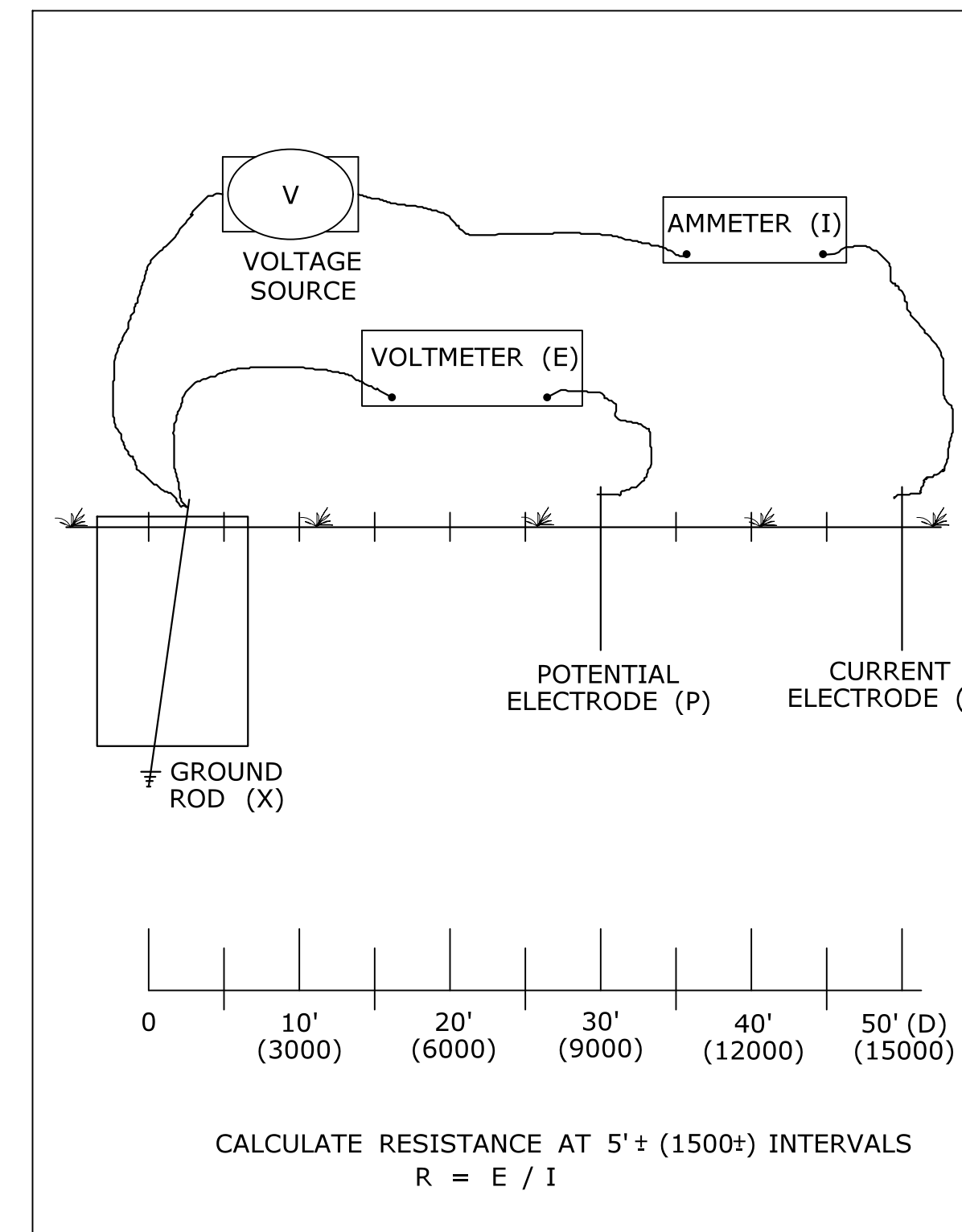
**LOOP CIRCUIT TEST DATA (EXAMPLE)**

**TEST PROCEDURE:**

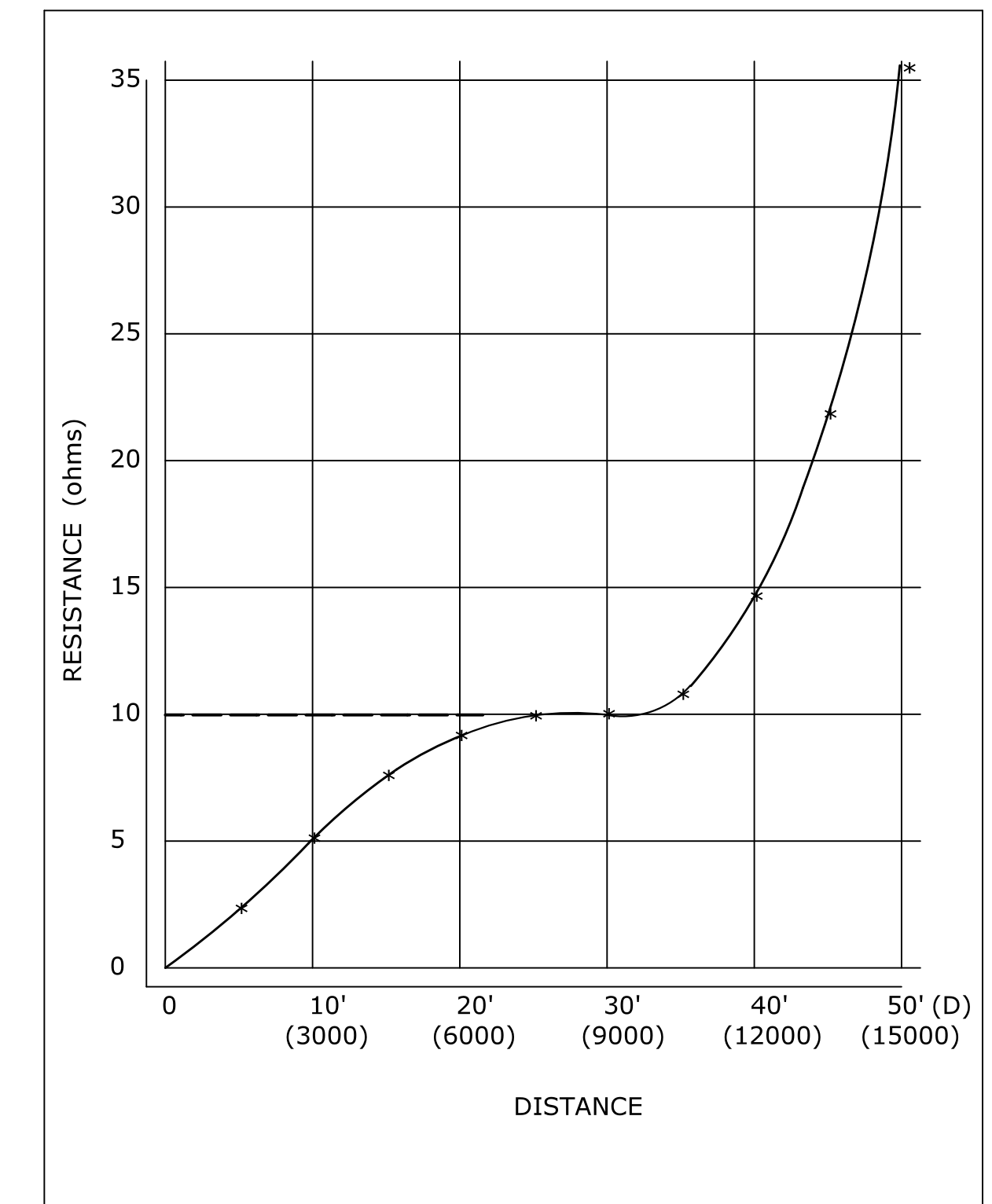
- INSERT ELECTRODE (C) A DISTANCE (D) FROM THE FOUNDATION. RECOMMEND A MINIMUM 50'.
- CONNECT A VOLTAGE SOURCE AND AMMETER BETWEEN THE FOUNDATION GROUND ROD (X) AND C.
- MEASURE THE CURRENT FLOW (I) BETWEEN X AND C.
- INSERT POTENTIAL ELECTRODE (P) AT 5' (1500) INTERVALS IN A STRAIGHT LINE TO ELECTRODE C.
- MEASURE VOLTAGE (E) AT EACH LOCATION OF P.
- CALCULATE RESISTANCE (R) AT EACH LOCATION OF P USING THE FORMULA  $R = E/I$ .
- PLOT THE VALUES ON A RxD GROUND RESISTANCE CHART.
- THE ACTUAL GROUND RESISTANCE IS WHERE THE PLOTTED CURVE IS RELATIVELY FLAT, USUALLY AT 62%± OF D.
- SEE EXAMPLE CHART: CURVE FLATTENS OUT AT 10 OHMS, APPROXIMATELY 30' (9000) FROM FOUNDATION.
- IF GROUND RESISTANCE IS GREATER THAN 10 OHMS, PERFORM CORRECTIVE ACTION AND RE-TEST.

**SUGGESTED CORRECTIVE ACTION:**

- A. INSTALL ADDITIONAL 10' (3000) GROUND ROD(S). REFER TO NESC SECTION 09, RULE 94.B.2. DRIVE ADDITIONAL GROUND RODS NO CLOSER TO FOUNDATION THAN 6' (1800). IF MORE THAN ONE IS NEEDED, SPACE MINIMUM 6' (1800) APART. BONDS TO ADDITIONAL GROUND ROD(S) SHALL BE MADE BY A CLAMP DESIGN FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE. TOP OF ADDITIONAL GROUND ROD(S) SHALL BE 6" (150) BELOW GRADE.
- B. IN AREAS OF SHALLOW BEDROCK, INSTALL A GROUND GRID OR ARRAY CONSISTING OF BURIED WIRE, RODS, STRIPS OR PLATES. REFER TO NESC SECTION 09, RULE 94.B.3. REFER TO NEC SECTION 250. MINIMUM DEPTH OF 18" (450). GRID CONNECTIONS AND BONDS ON GROUND GRID SHALL BE MADE BY CLAMPS DESIGNED FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE.



**3 POINT GROUND RESISTANCE TEST CIRCUIT**



**GROUND RESISTANCE CHART (EXAMPLE)**

**NOTES:**

1. WHEN REQUESTED BY THE ENGINEER, MEASURE RESISTANCE-TO-GROUND OF GROUND ROD AT TRAFFIC CONTROL FOUNDATIONS. SEE FALL-OF-POTENTIAL METHOD. IF LESS THAN 10 ohms, INSTALL SUPPLEMENTAL ELECTRODES AS REQUIRED. NEC ARTICLE 250.
2. DURING THE TEST, THE GROUND ROD SHOULD NOT BE BONDED TO ANY RMC IN THE FOUNDATION.
3. THE VOLTAGE SOURCE, VOLTMETER, AMMETER, ELECTRODES P AND C, AND CONNECTING CABLES ARE AVAILABLE AS A SPECIALIZED TEST INSTRUMENT.
4. REFER TO NATIONAL ELECTRICAL SAFETY CODE (NESC) SECTION 09, GROUNDING METHODS FOR ELECTRIC SUPPLY AND COMMUNICATIONS FACILITIES.
5. REFER TO NATIONAL ELECTRICAL CODE (NEC) CHAPTER 2, ARTICLE 250, GROUNDING.

**3 POINT FALL-OF-POTENTIAL GROUND RESISTANCE TEST**

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

- INDUCTIVE LOOP DETECTOR
- SAW CUT
- RIGID METAL CONDUIT
- HANDHOLE

REV.	DATE	REVISION DESCRIPTION
2	1-2014	REVISED GROUND RESISTANCE NOTES.
1	4-2012	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 1/7/2014

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm. - UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

File name: CTDOT\_TRAFFIC\_STD.DGN Model: TR-1000\_01

SUBMITTED BY: NAME/DATE/TIME:

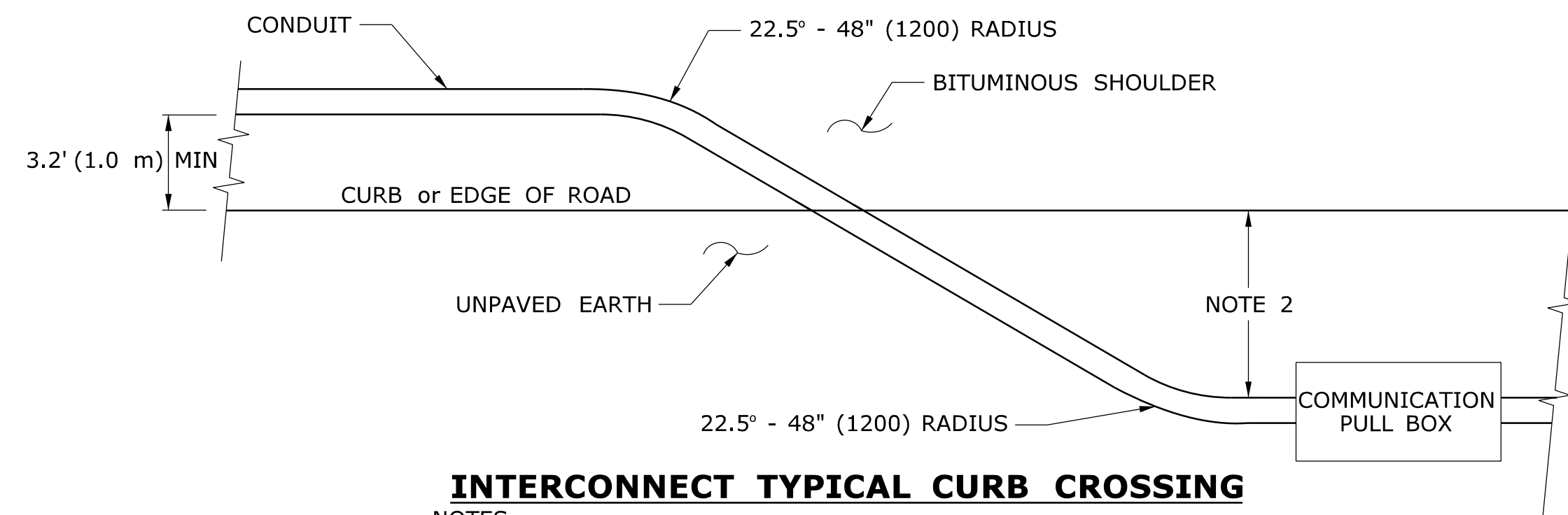
APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE: GENERAL CLAUSES (TEST PROCEDURES)

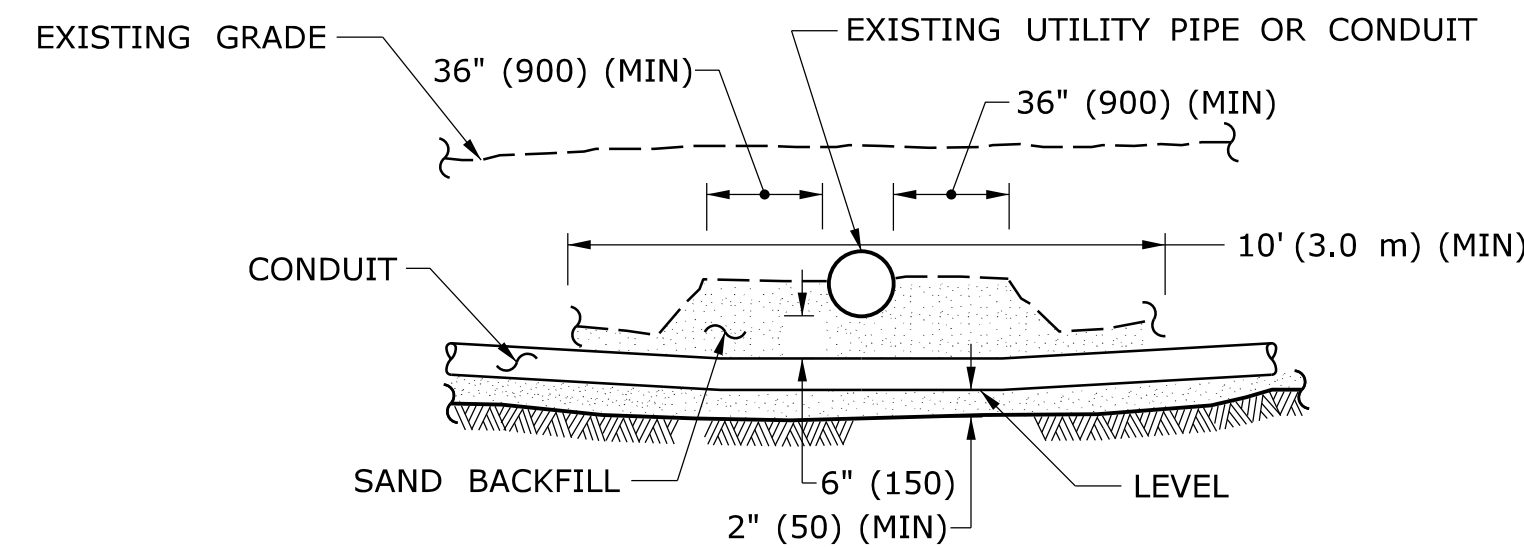
STANDARD SHEET NO.: TR-1000\_01



**INTERCONNECT TYPICAL CURB CROSSING**

NOTES:

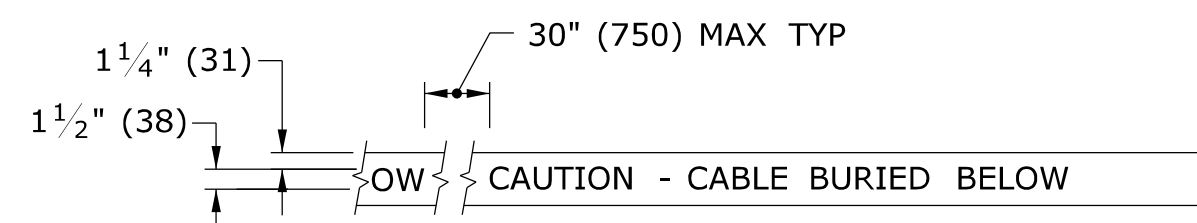
- RESTORE AREAS DISTURBED BY TRENCH TO ORIGINAL CONDITION.
- INSTALL PULL BOX A MINIMUM OF 10' (3.0 m) FROM CURB UNLESS OTHERWISE SHOWN ON PLANS OR DIRECTED BY ENGINEER.



**CROSSING UNDER EXISTING UTILITY**

NOTES:

- WHEN ENCOUNTERED AT APPROXIMATELY THE SAME DEPTH, CROSS BENEATH.
- PROTECT & SUPPORT EXPOSED EXISTING UTILITY.



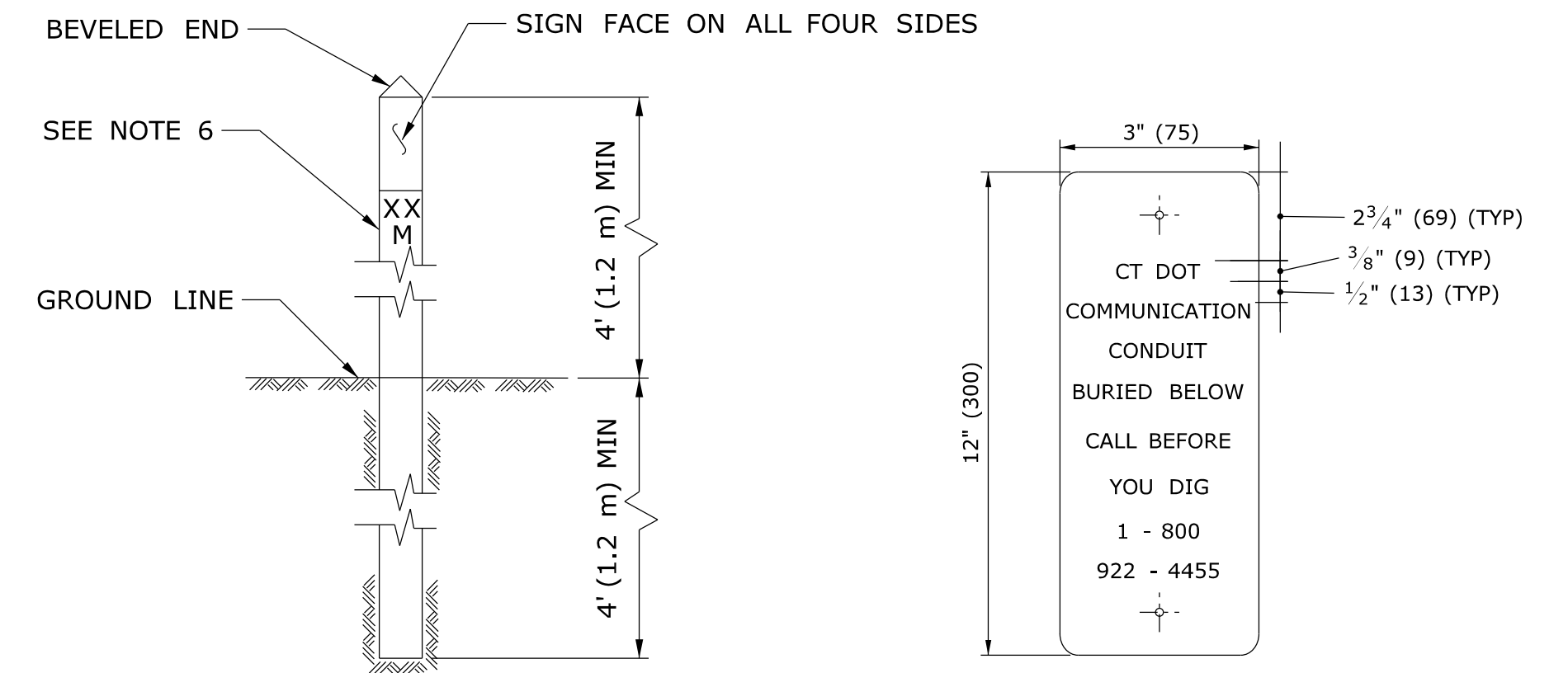
**DETECTABLE WARNING TAPE**

NOTE:

STANDARD SPECIFICATIONS, ARTICLE: 1.05.15

1. TAPE COLORS:

- COMMUNICATION - ORANGE BACKGROUND / BLACK LEGEND
- POWER - RED BACKGROUND / BLACK LEGEND

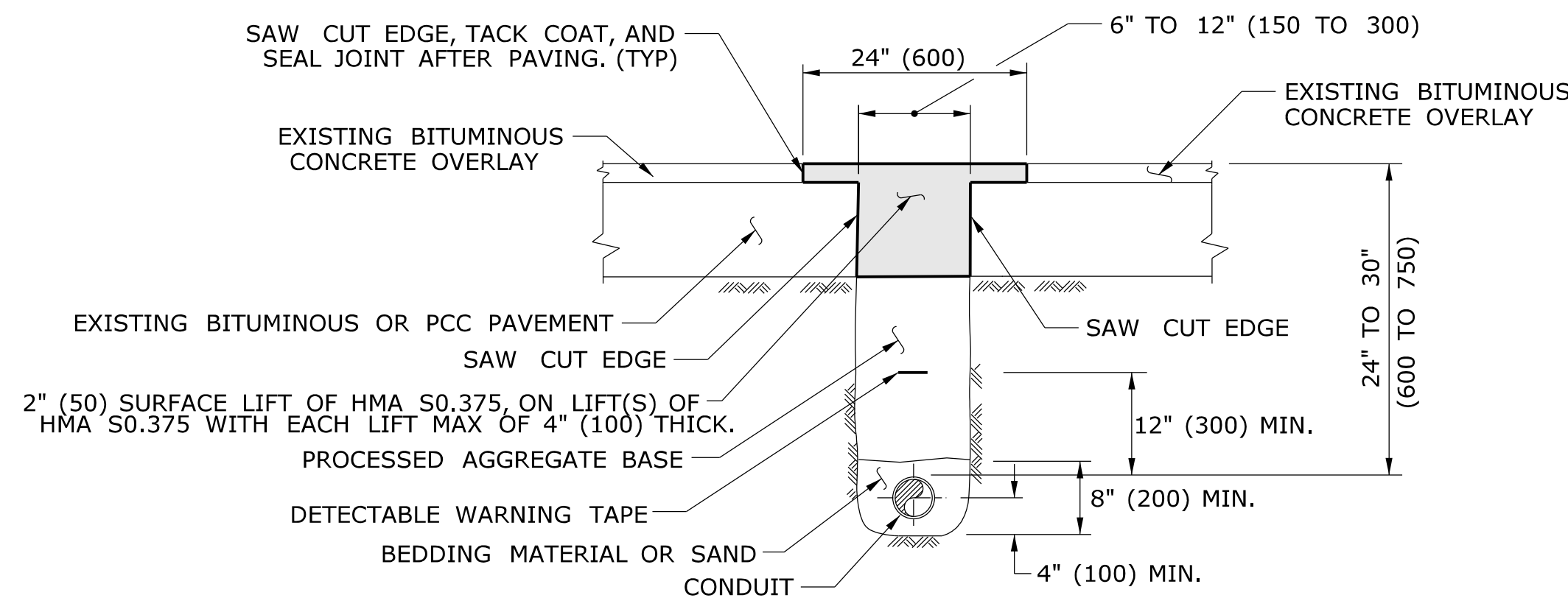


**INTERCONNECT CONDUIT IDENTIFICATION POST**

**SIGN FACE DETAIL**  
SIGN # 41-4669

NOTES:

- 4" x 4" (100 x 100) NOMINAL, PRESSURE TREATED WOOD POST.
- ATTACH SIGN TO POST WITH 1/4" x 1 1/4" (6 x 31) STAINLESS STEEL LAG SCREW WITH NYLON WASHER ON FACE OF SIGN.
- SIGN COLORS: BACKGROUND - ORANGE (RETROREFLECTIVE) LEGEND - BLACK (OPAQUE).
- INSTALL POST APPROX 24" (600) FROM RMC IN VICINITY OF EACH PULL BOX.
- INSTALL POSTS BETWEEN PULL BOXES, APPROX 10' (3.0 m) OFF CURB. SPACE POSTS 1500± (460 m±) APART.
- PERMANENTLY ATTACH STAINLESS STEEL NUMBERS INDICATING DISTANCE TO TRENCH IN FEET (METERS) CONTAINING COMMUNICATION CABLE. ATTACH NUMBERS TO SIDE OF POST FACING CONDUIT. INCLUDE "M" SUFFIX IF METERS.

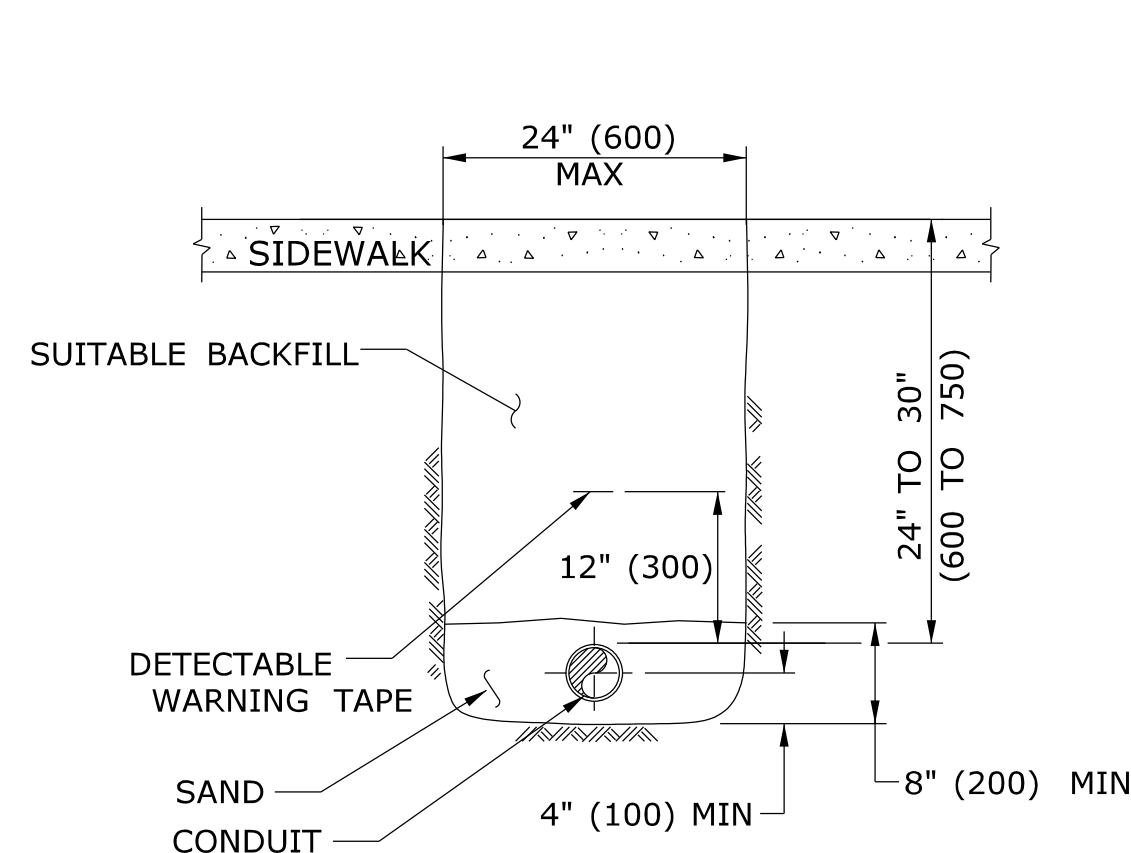


**PAVEMENT - BITUMINOUS CONCRETE OR OVERLAVED PORTLAND CEMENT CONCRETE**

NOTES:

STANDARD SPECIFICATIONS, ARTICLE: 3.04 & 4.06.03

- TOTAL HOT MIX ASPHALT (HMA) THICKNESS TO MATCH EXISTING BITUMINOUS CONCRETE AND PORTLAND CEMENT CONCRETE (PCC) THICKNESS.
- WHEN ALLOWED BY ENGINEER, USE CONTROLLED LOW STRENGTH MATERIAL (CLSM) AS BEDDING MATERIAL. TOP OF CLSM AT LEAST 20" (500) BELOW SURFACE.

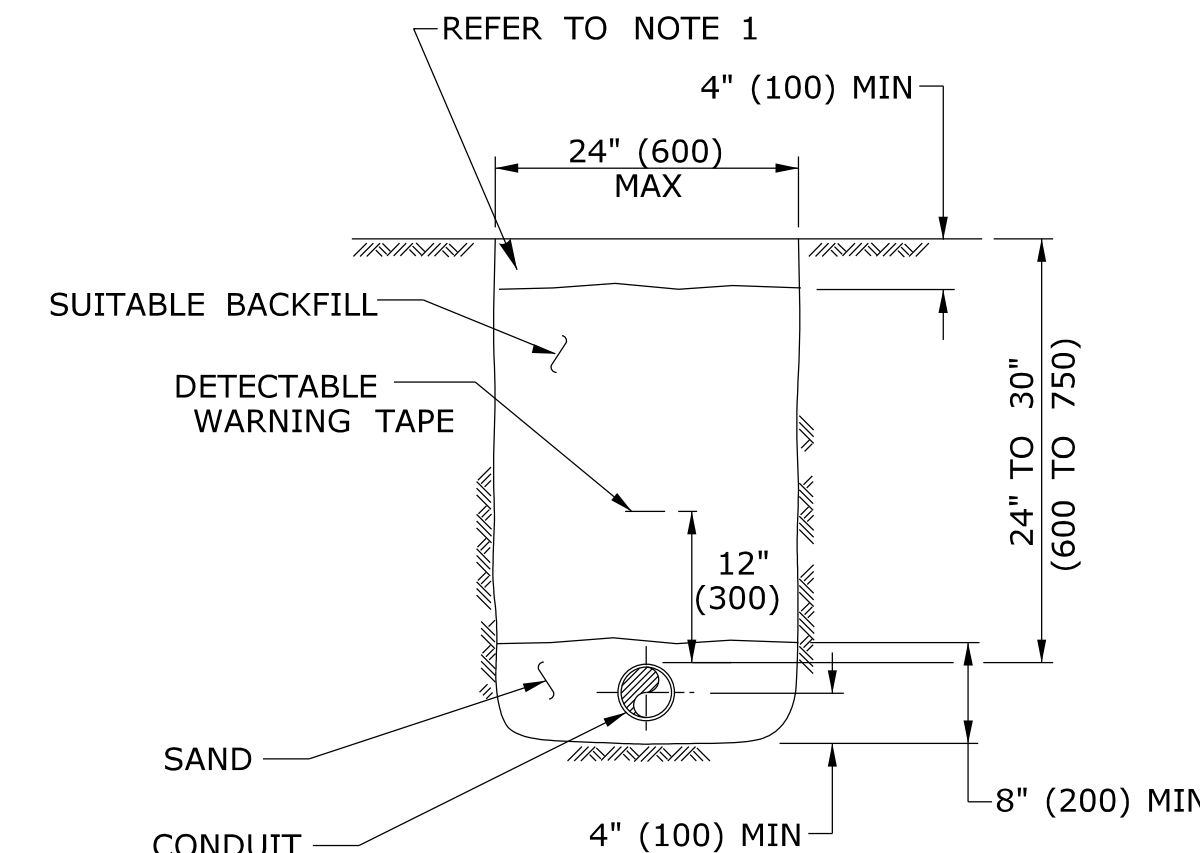


**SIDEWALK**

NOTES:

STANDARD SPECIFICATIONS, ARTICLE: 9.21 & 9.22

- WHERE CONCRETE SIDEWALK DAMAGED OR CUT, REPLACE THE ENTIRE SECTION BETWEEN JOINTS. REPLACEMENT SIDEWALK IS PAID FOR AT THE CONTRACT UNIT PRICE FOR "CONCRETE SIDEWALK".



**EARTH**

NOTES:

STANDARD SPECIFICATIONS, ARTICLE: 9.50

- IN MOWED AREAS: PLACE TOPSOIL, FERTILIZER, SEED, & MULCH.

GENERAL NOTES:

- TOP OF CONDUIT NO LESS THAN 24" (600) DEEP.
- COMPACT BACKFILL IN ≤ 6" (150) LIFTS. HAND COMPACTION NOT PERMITTED.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:  
--- RMC (RIGID METAL CONDUIT)

REV.	DATE	REVISION DESCRIPTION
1	4-2012	REVISED BITUMINOUS CONCRETE TO HMA, & MINOR REVISIONS.
		REVISION DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm).  
METRIC DIMENSIONS ARE ROUNDED:  
- OVER 1" TO NEAREST 5 mm  
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

Plotted Date: 4/14/2012

Filename: CTDOT\_TRAFFIC\_STD.dgn Model: TR-1001\_01

SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING

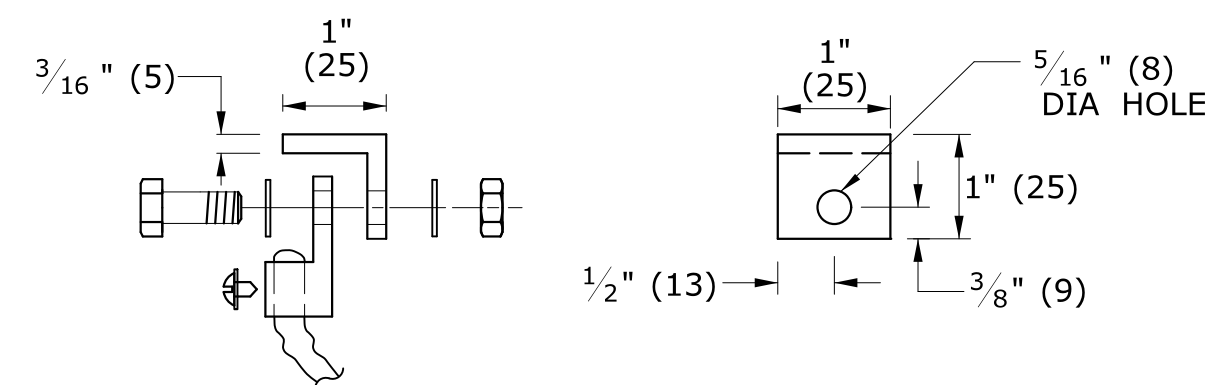
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**TRENCHING & BACKFILLING,  
ELECTRICAL CONDUIT**

STANDARD SHEET NO.:  
**TR-1001\_01**

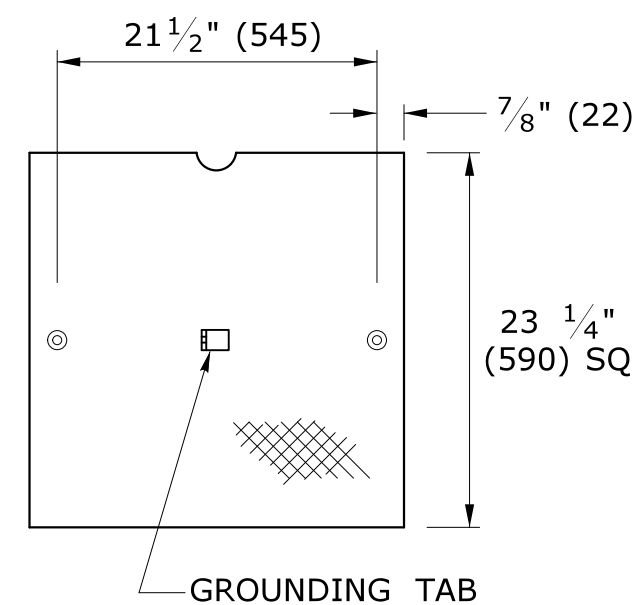


**COVER NOTES:**

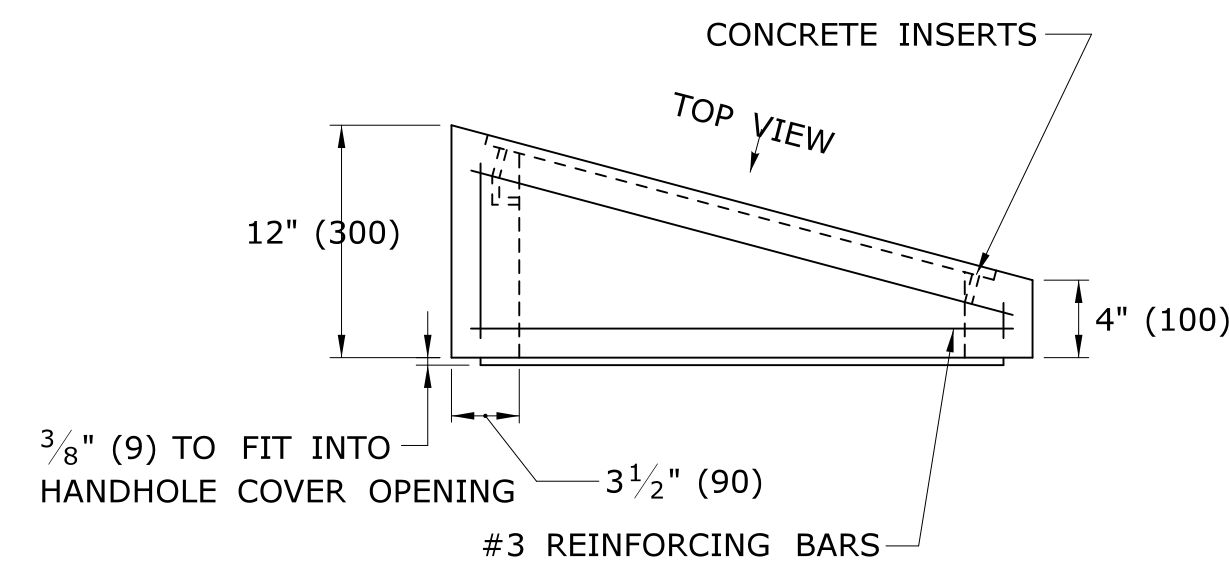
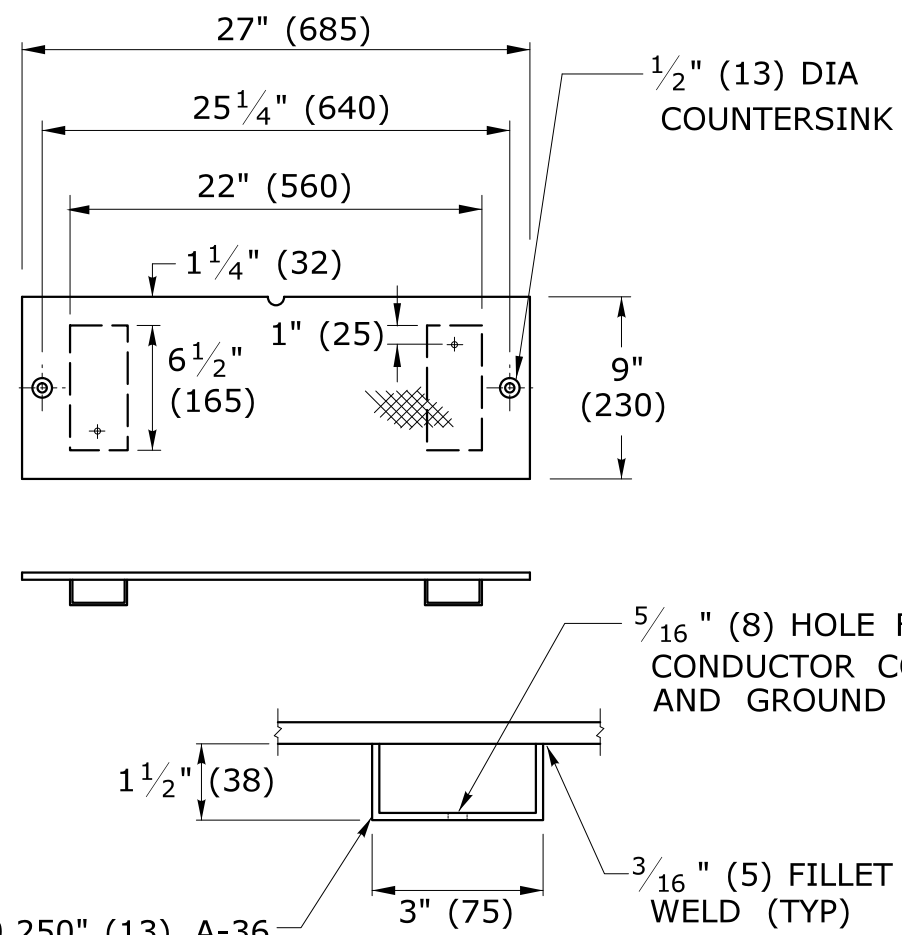
1. GROUNDING TAB WELDED TO BOTTOM CENTER OF COVER WITH  $\frac{3}{16}$ " (5) WELD (3 SIDES).
2. ATTACH 6' (2 m) LENGTH OF NO. 8 GROUND WIRE TO GROUNDING TAB WITH CONDUCTOR CONNECTOR,  $\frac{1}{4}$ " - 20 X  $\frac{3}{4}$ " (M6 X 20) LG SST HEX HEAD BOLT, AND SST FLAT WASHER. ATTACH FREE END OF GROUND WIRE TO CONDUIT BONDING BUSHING IN HANDHOLE.
3. CONDUCTOR CONNECTOR: COPPER ALLOY BODY, BRASS SCREW, BRASS OR COPPER ALLOY PRESSURE PLATE.
4. COVER SCREW INSERT:  $\frac{3}{8}$ "-16 (9-16),  $1\frac{1}{2}$ "L (37L), STAINLESS STEEL.
5. COVER SCREW:  $\frac{3}{8}$ "-16 (9-16), 1"L (25L), FLAT HEAD, SLOTTED, STAINLESS STEEL.



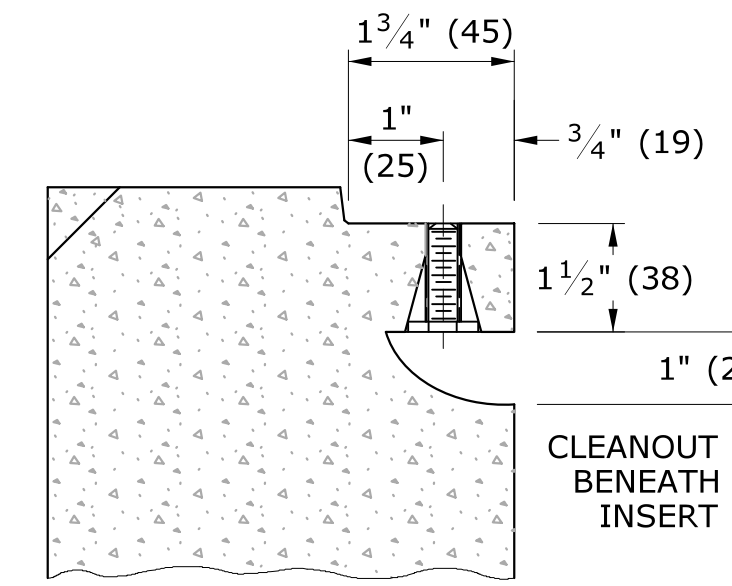
**STEEL GROUNDING TAB  
w/ CONDUCTOR CONNECTOR**



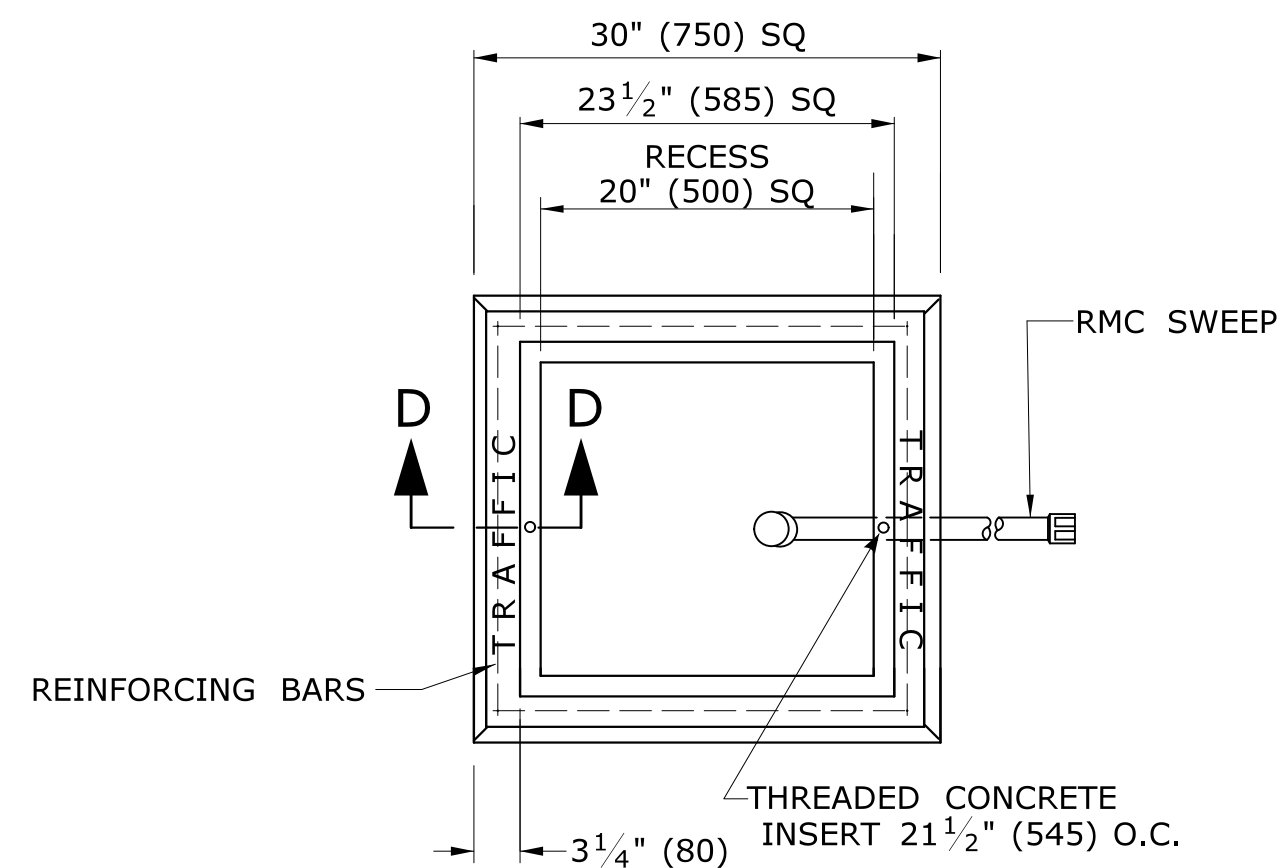
**NON SKID FLOOR PLATE  
GALVANIZED STEEL, 3/8" (10)**



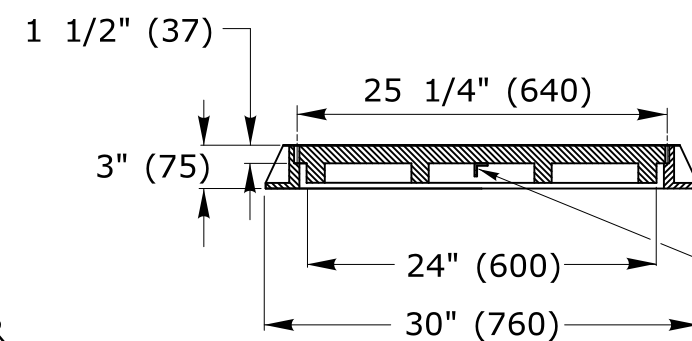
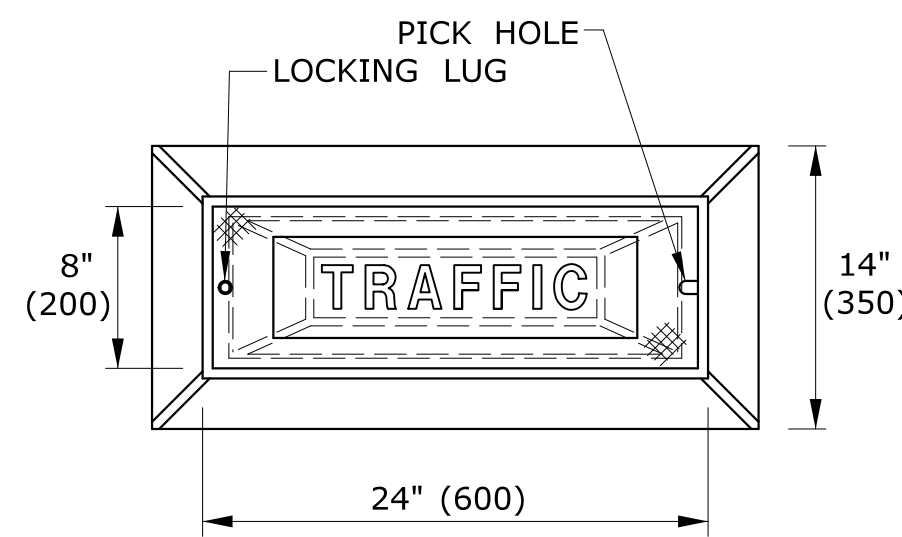
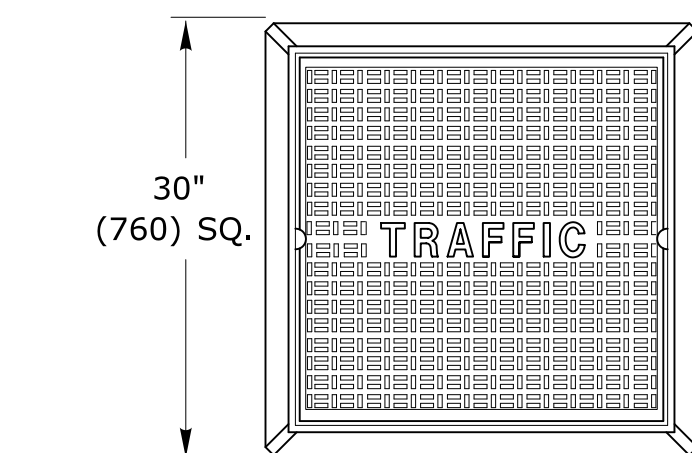
**BANK ADAPTER**



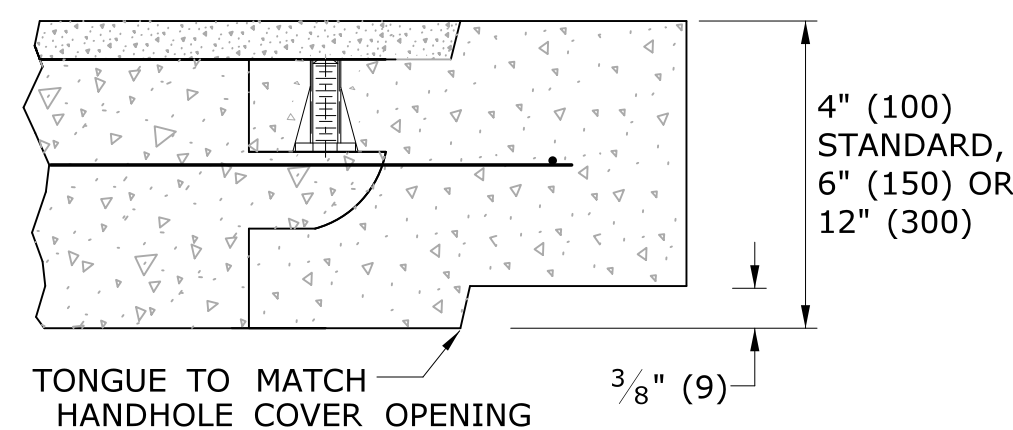
**SECTION D-D**



**PLAN VIEW**

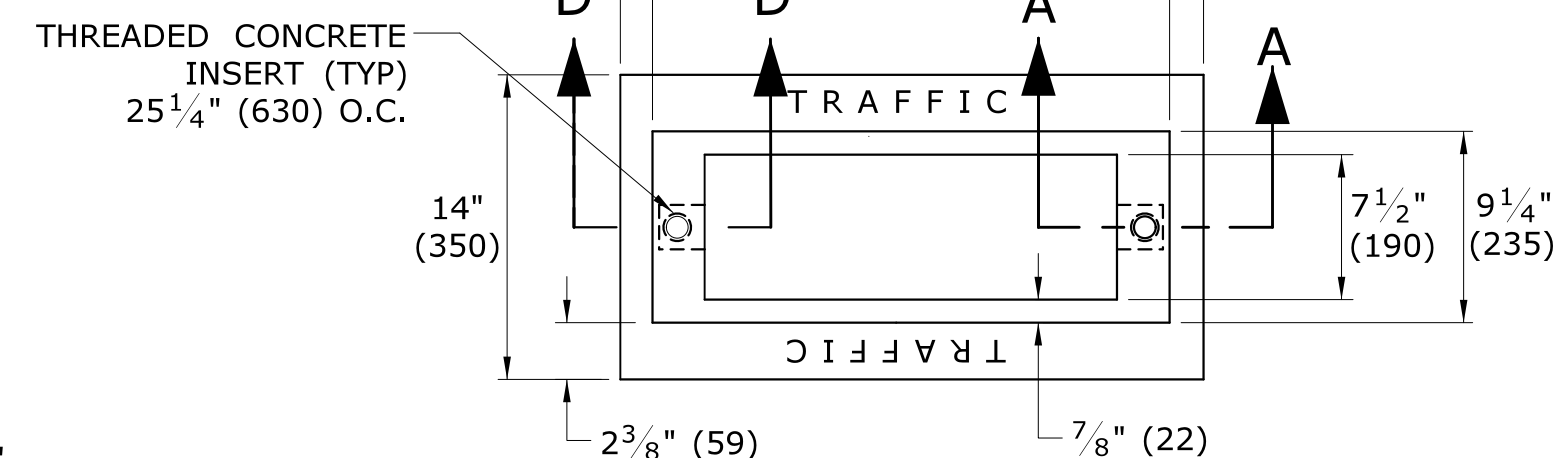


**CAST IRON  
HANDHOLE COVERS**

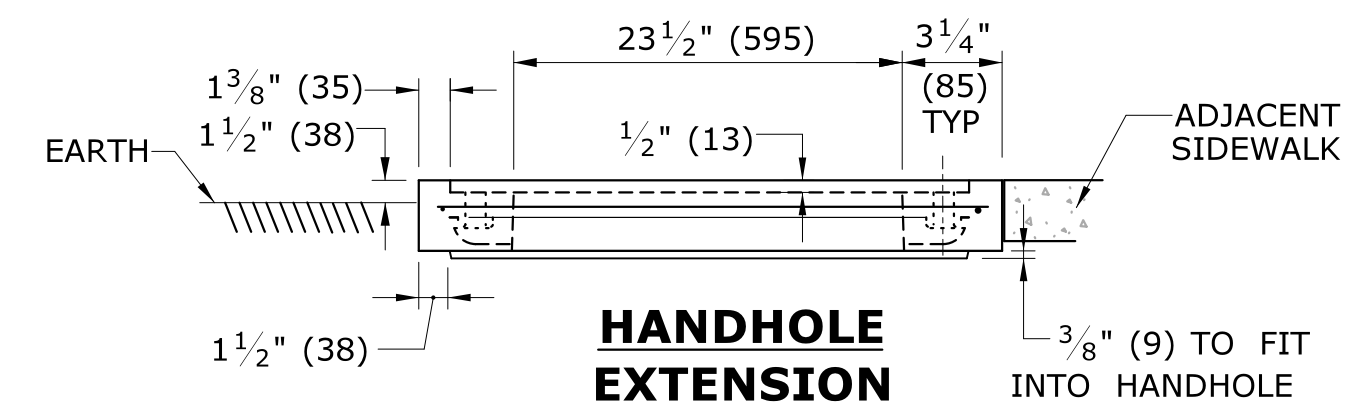


**SECTION A-A  
HANDHOLE EXTENSIONS**

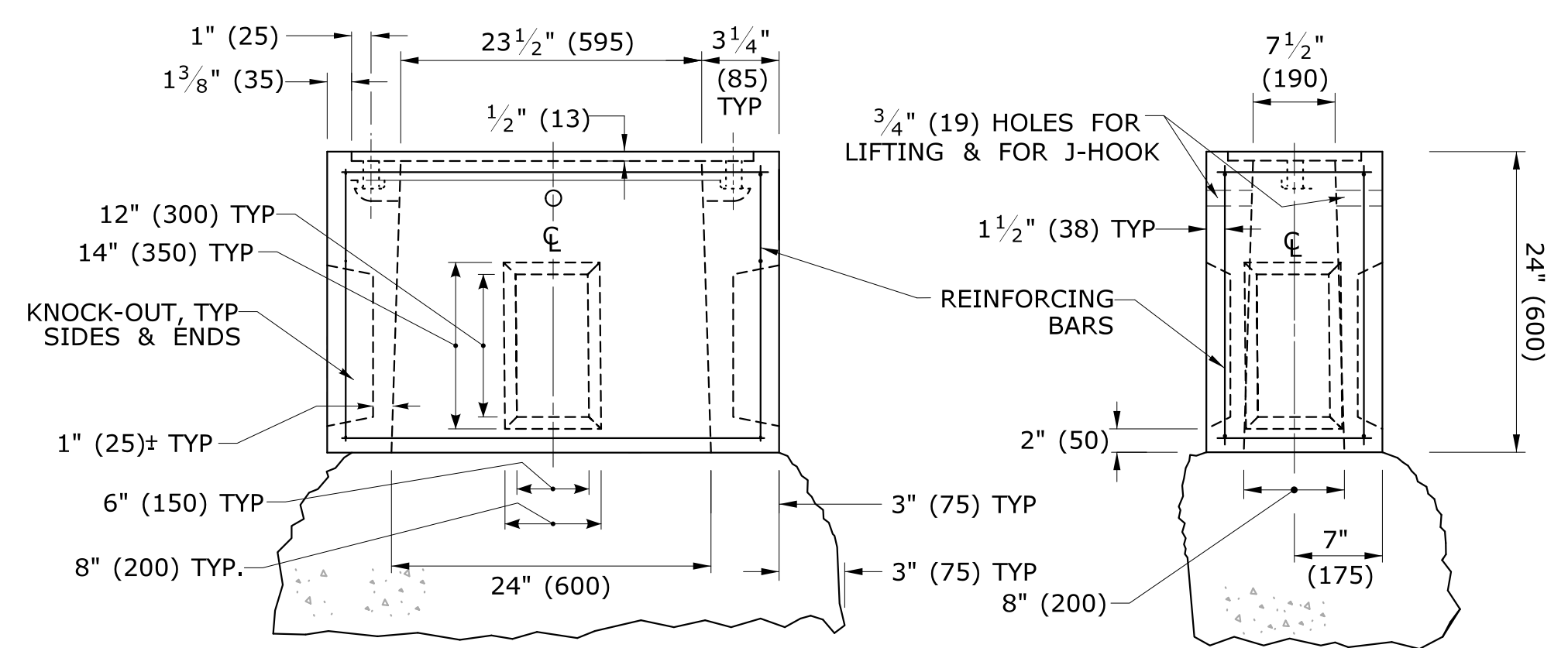
4 - #8 REINFORCING BARS REQ'D



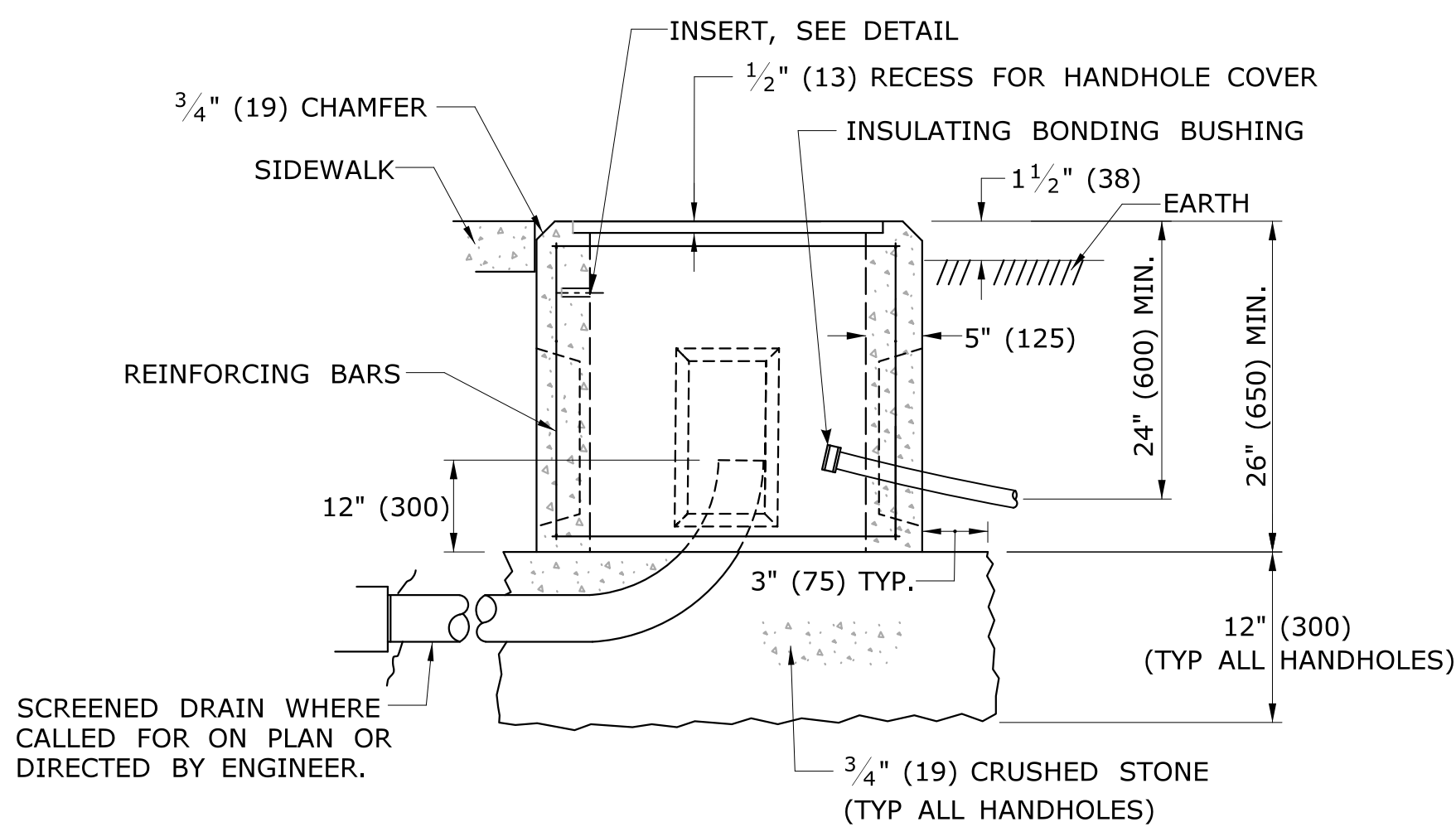
**PLAN VIEW**



**HANDHOLE  
EXTENSION**



**BASE SECTION  
CONCRETE HANDHOLE TYPE II**



**CONCRETE HANDHOLE TYPE I**

**HANDHOLE NOTES:**

1. MINIMUM CLASS "C" CONCRETE.
2. COMPLETE TYPE II HANDHOLE:  
IN EARTH AREAS, CONSISTS OF A BASE SECTION WITH 4" (100) HANDHOLE EXTENSION,  
IN SIDEWALK AREAS, CONSISTS OF A BASE SECTION WITH 4" (100) CAST IRON COVER.
3. PLAN VIEW DIMENSIONS, SECTION VIEW, & DETAILS, SAME FOR BASE SECTION, EXTENSIONS & BANK ADAPTER.
4. GROUT AROUND ALL CONDUITS.

5. INSTALL 30" (750) SIDE PARALLEL TO ROAD UNLESS OTHERWISE NOTED.
6. INSTALL HANDHOLES APPROX. 12" (300) BEHIND CURB OR  
IF NO CURB, 24" (600) BEHIND EDGE OF ROAD UNLESS OTHERWISE SPECIFIED.
7. CAST THE WORD "TRAFFIC" INTO TOP EDGE OF HANDHOLE, 1 1/2" (38) LETTERS.
8. WHERE AN EXISTING CONCRETE SIDEWALK SLAB ABUTTING A HANDHOLE IS DAMAGED OR CUT DURING INSTALLATION, REPLACE THE ENTIRE SIDEWALK SECTION.
9. 12-#3 REINFORCING BARS REQUIRED FOR ALL HANDHOLES. (8 HORIZONTAL, 4 VERTICAL)

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:	
□	PROPOSED HANDHOLE
■	EXISTING HANDHOLE

REV.	DATE	REVISION DESCRIPTION
2	4-2014	REVISED HANDHOLES NOTES, ADDED NOTE #6. ADDED "J" HOOK TO INSERT DETAIL.
1	4-2012	CAST IRON COVER: CHANGED BOLT TO PICK HOLE. ADDED EXTENSIONS, C-CHANNEL, CONDUCTOR CONNECTOR & MINOR REVISIONS.

Plotted Date: 4/11/2014

NOT TO SCALE

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm. - UNDER 1" TO NEAREST 1 mm.

**STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION**

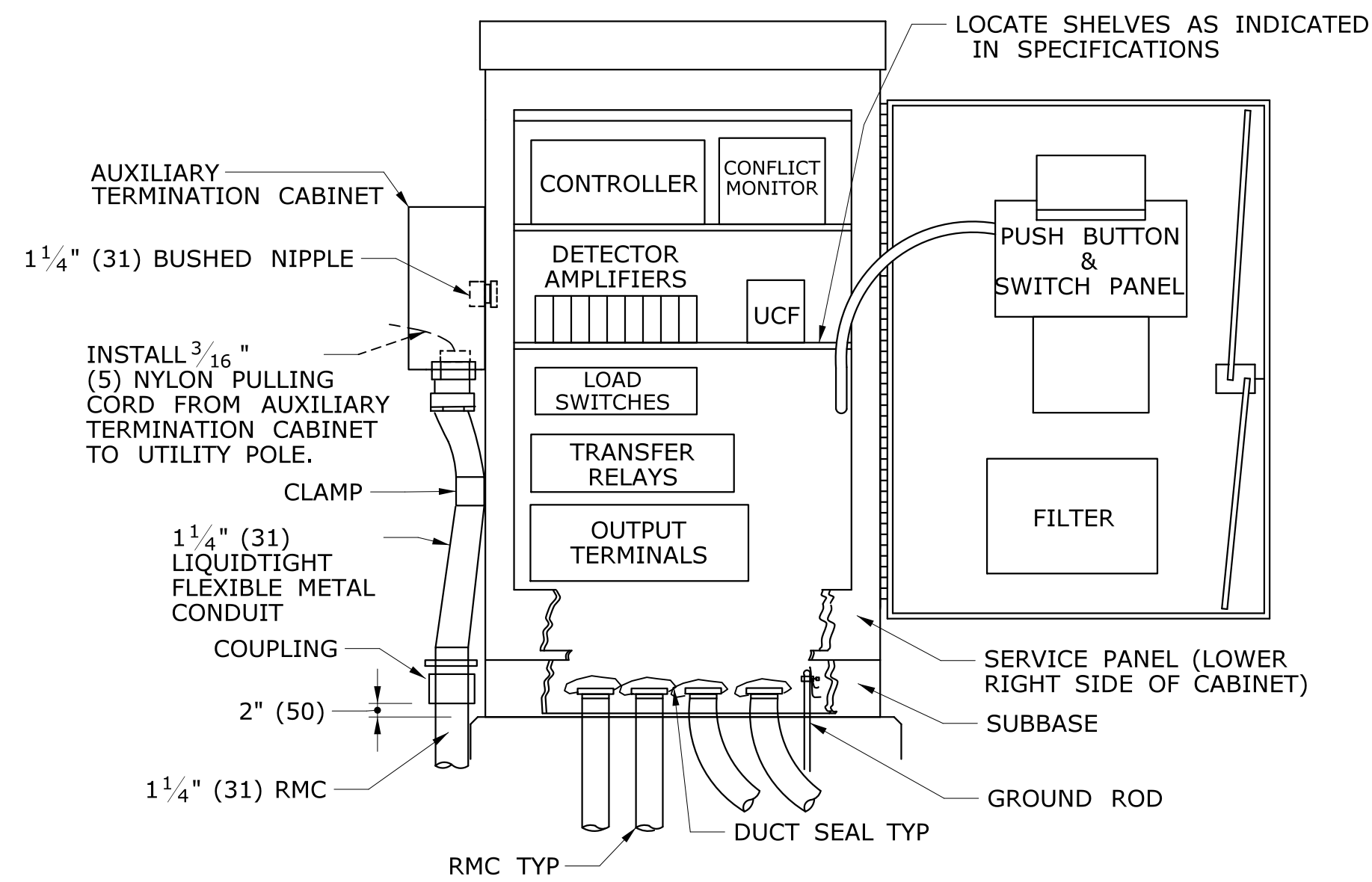
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SUBMITTED BY: NAME/DATE/TIME:  
APPROVED BY: NAME/DATE/TIME:

**CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING**

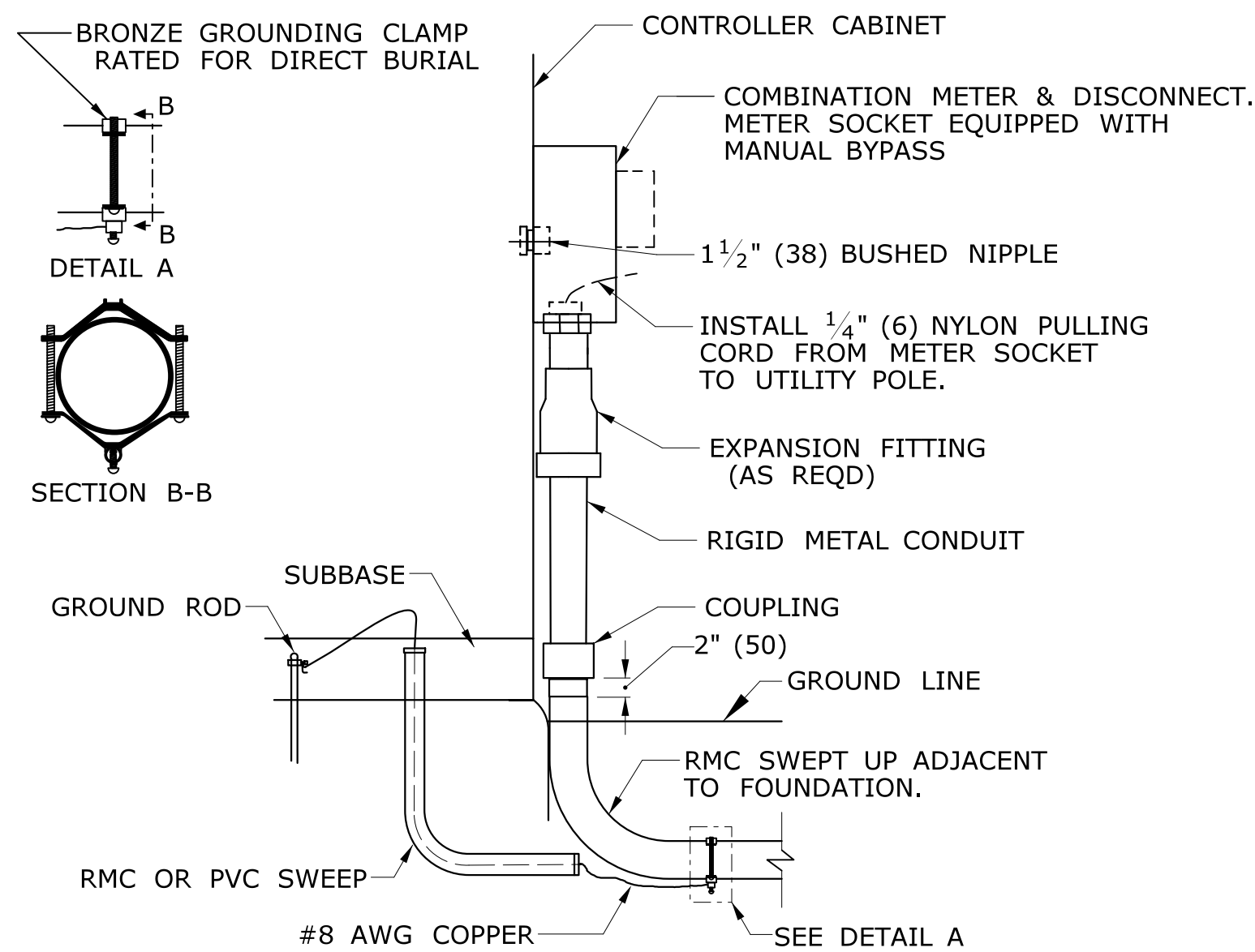
STANDARD SHEET TITLE:  
**CONCRETE HANDHOLE**

STANDARD SHEET NO.:  
**TR-1010\_01**

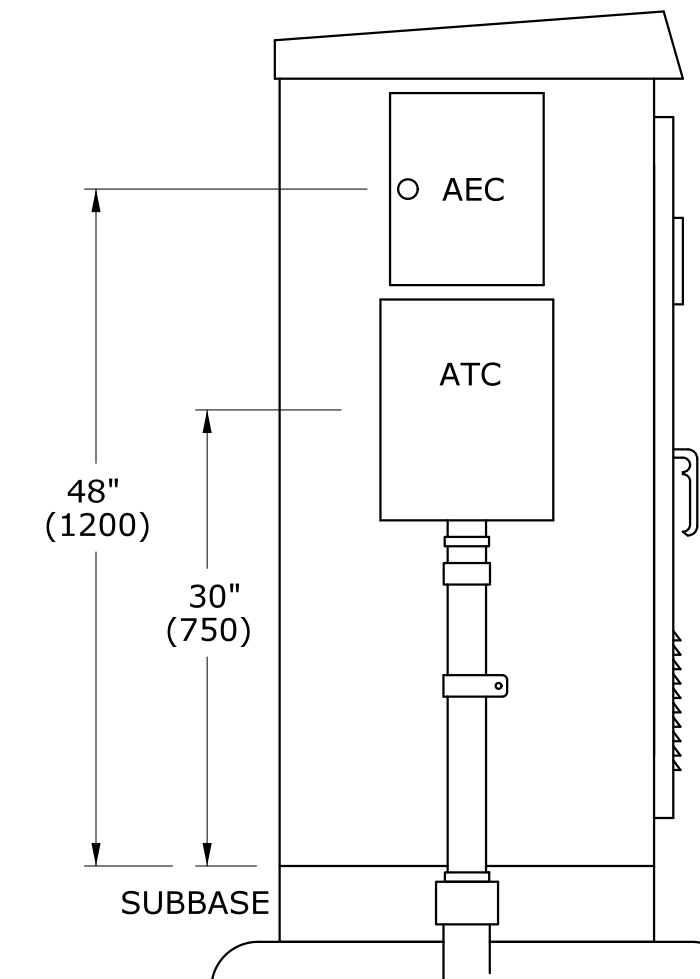


PROVIDE A MINIMUM CLEARANCE OF 6" (150) FROM THE CABINET BASE TO ALL COMPONENTS AND TERMINALS.

**TYPICAL BASE MOUNTED CONTROLLER ON TYPE IV FOUNDATION**



**CONTROLLER CABINET WITH METERED SERVICE**

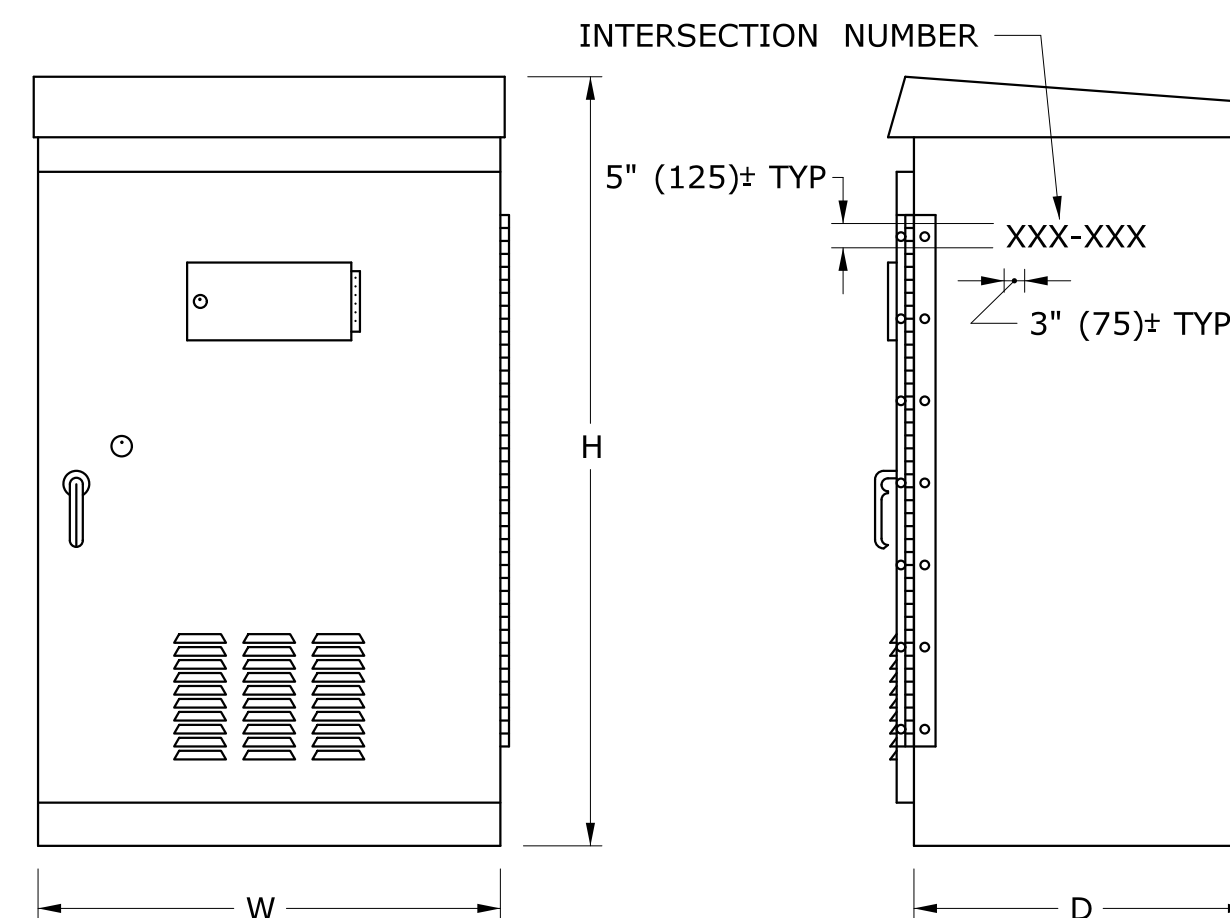


**AUXILIARY EQUIPMENT CABINET (AEC) AUXILIARY TERMINATION CABINET (ATC)**

CABINET TYPE	HEIGHT	WIDTH	DEPTH
ATC	16"(400)	12"(300)	6"(150)
AEC	14"(350)	11"(275)	11"(275)

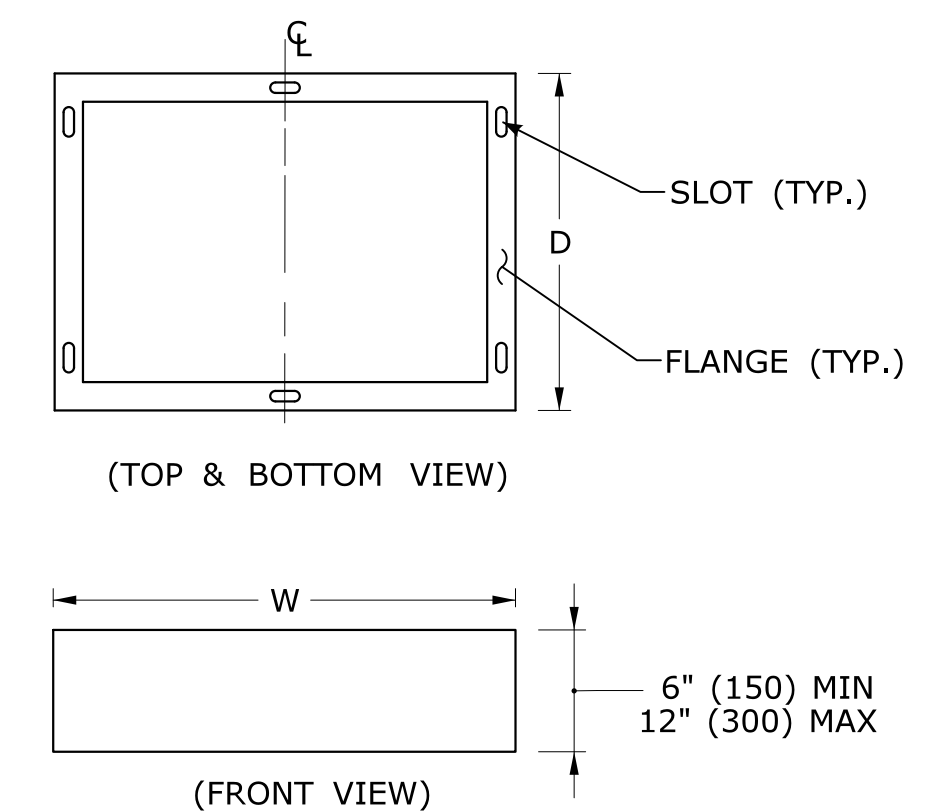
**GENERAL NOTES:**

- GROUT ALL BASES AFTER MOUNTING ON FOUNDATIONS, WHERE NECESSARY. 3'-0" (900) FROM SIDEWALK TO BOTTOM OF CONTROLLER.
- INSTALL PEDESTALS AND POLES SO THAT DOORS AND COVERS ARE ON THE SIDE AWAY FROM THE STREET, UNLESS OTHERWISE SPECIFIED.
- INSTALL CABINET SO THAT DOOR OPENS FIELD SIDE UNLESS OTHERWISE NOTED ON PLANS. CAULK SEAM BETWEEN SUBBASE AND FOUNDATION.
- STENCIL SIX DIGIT INTERSECTION NUMBER, USING BLACK PAINT ON SIDE, FRONT OR BACK OF CABINET MOST VISIBLE FROM THE ROAD.



**BASE MOUNTED TRAFFIC CONTROLLER (TYPE B, D & E)**

CABINET TYPE	DEPTH		WIDTH		HEIGHT	
	MIN	MAX	MIN	MAX	MIN	MAX
B	17" (425)	19" (475)	30" (750)	34" (850)	52" (1300)	56" (1400)
D	25" (625)	27" (675)	42" (1050)	45" (1125)	54" (1350)	59" (1475)
E	17" (425)	19" (475)	30" (750)	32" (800)	49" (1225)	52" (1300)



**SUBBASE**

SLOT AND FLANGE DIMENSIONS TO BE PER MANUFACTURER.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

- CONTROLLER ASSEMBLY
- AUXILIARY EQUIPMENT CABINET
- AUXILIARY TERMINATION CABINET

REV.	DATE	REVISION DESCRIPTION
2	5-2013	REVISED SUBBASE.
1	4-2012	REVISED CABINET TYPES & MINOR REVISIONS.

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Plotted Date: 5/15/2013

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: + OVER 1" TO NEAREST 5 mm. - UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

Submitted by: NAME/DATE/TIME: \_\_\_\_\_

Approved by: NAME/DATE/TIME: \_\_\_\_\_

Filename: CTDOT\_TRAFFIC\_STD.dgn Model: TR-1108\_01

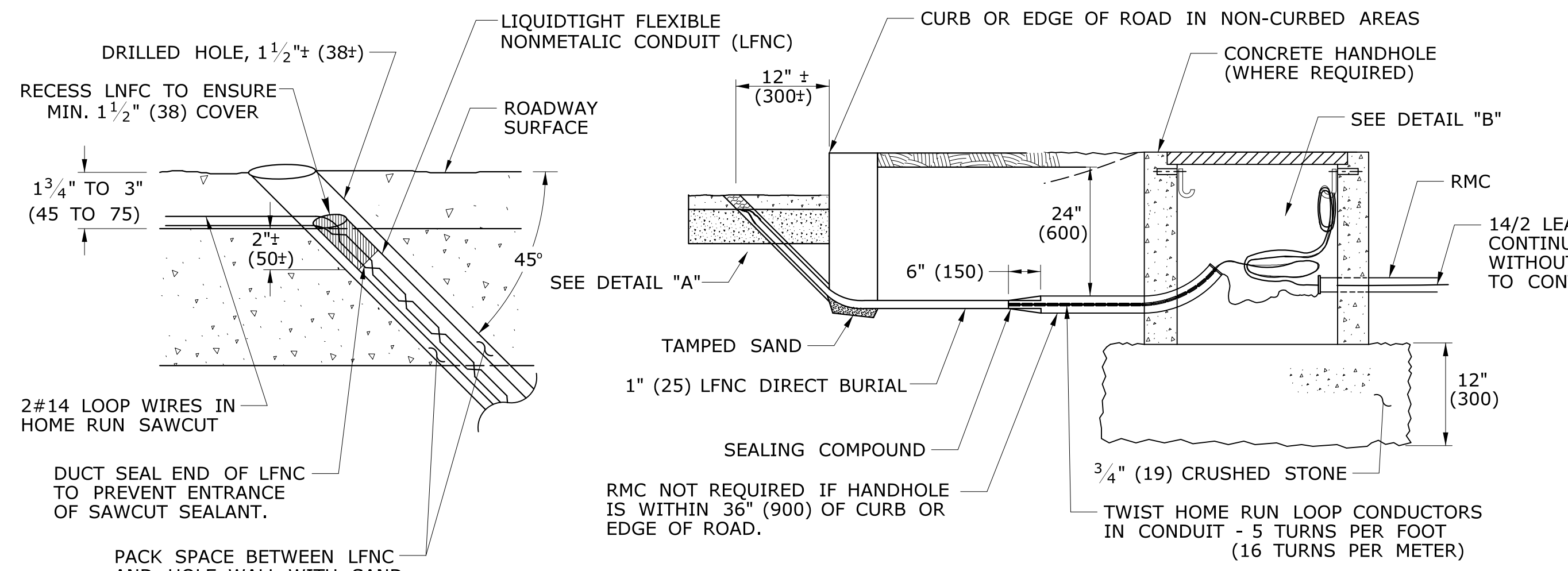
CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

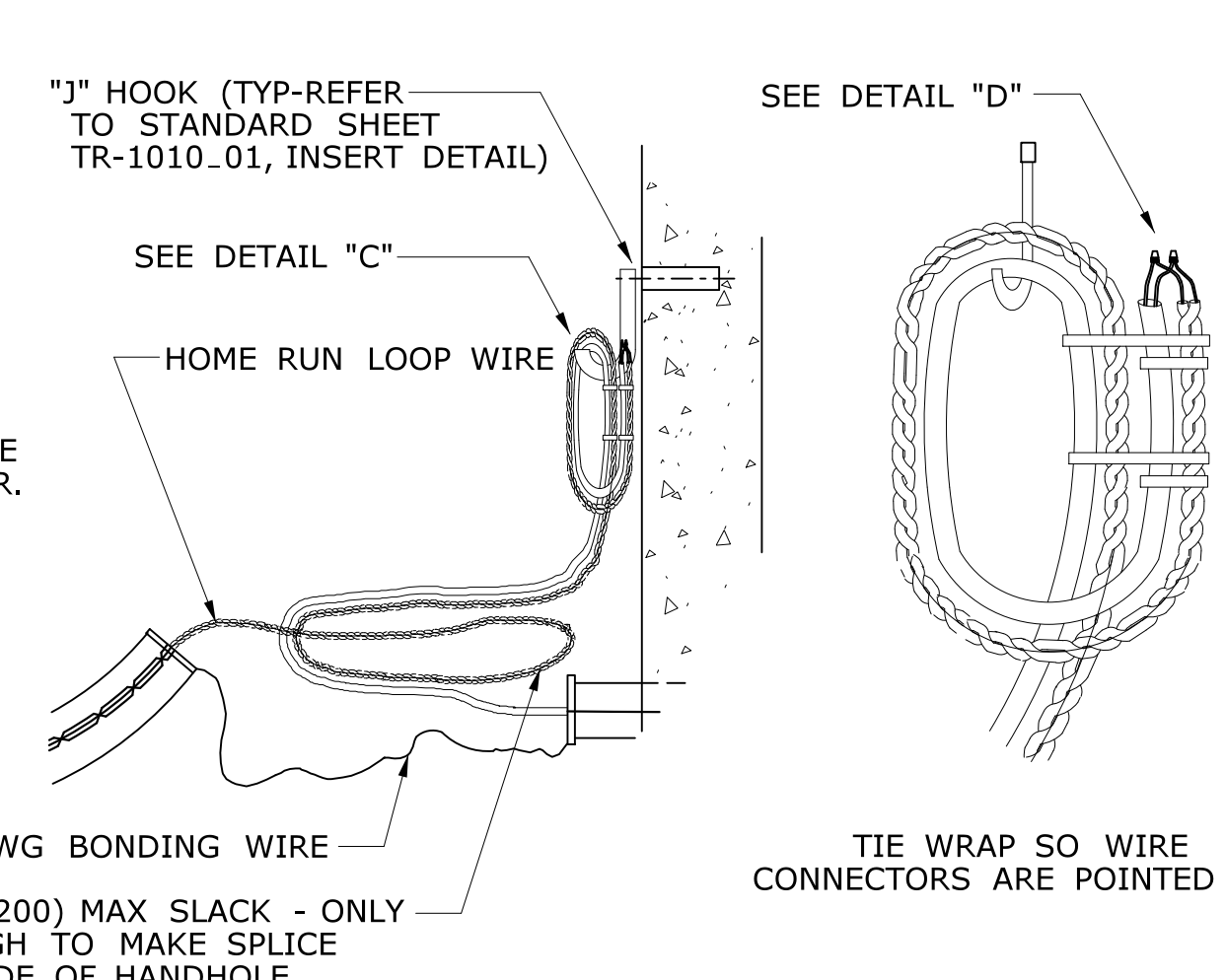
STANDARD SHEET TITLE: CONTROLLERS

STANDARD SHEET NO.: TR-1108\_01

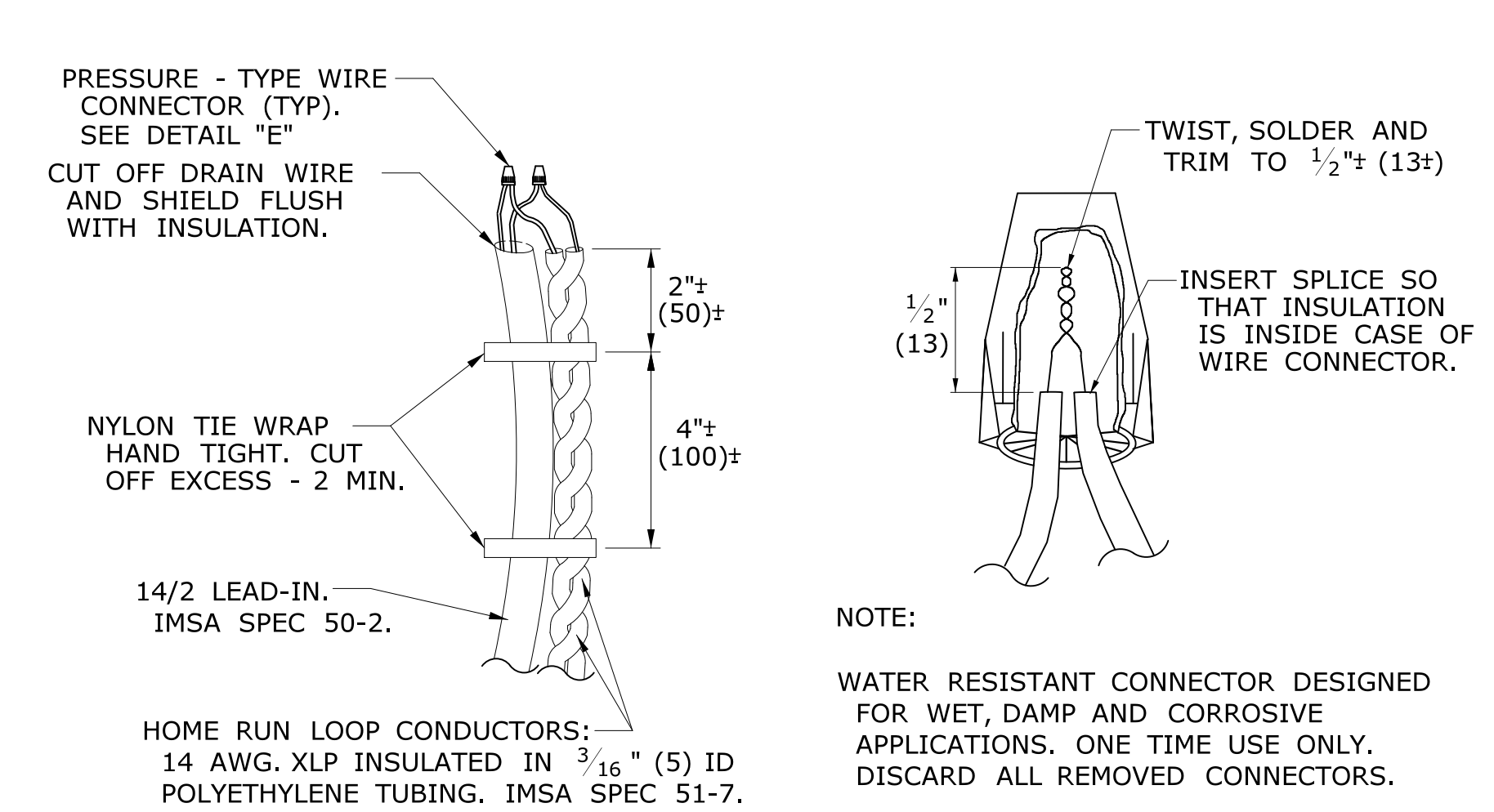




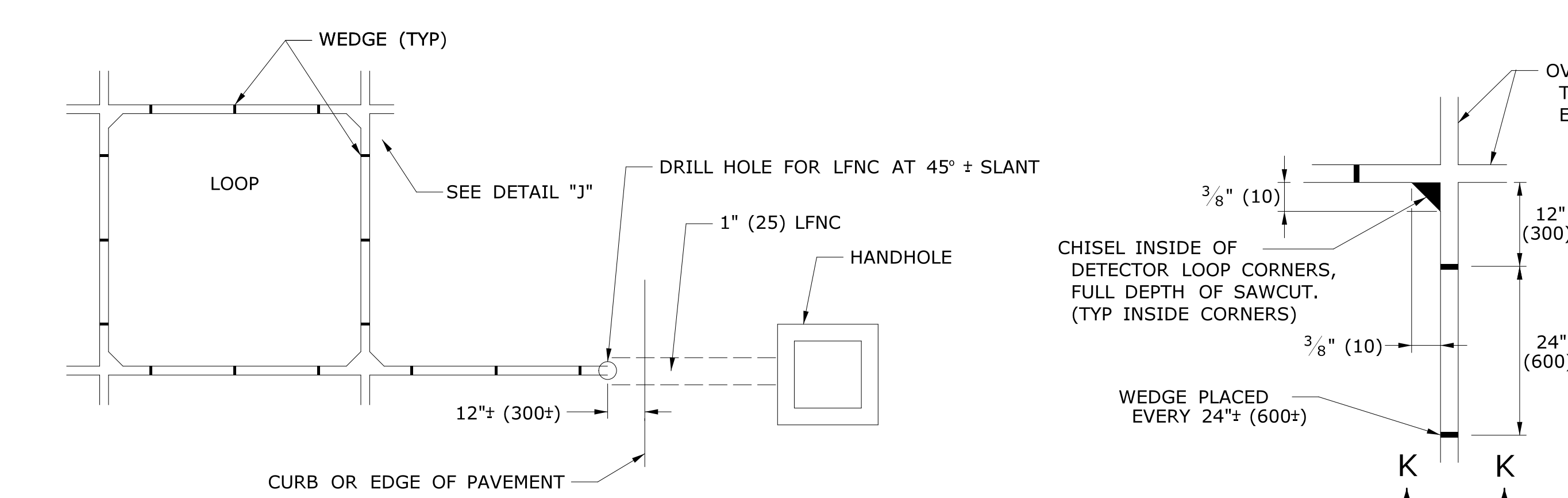
**TYPICAL ELEVATION VIEW  
LOOP DETECTOR LEAD-IN**



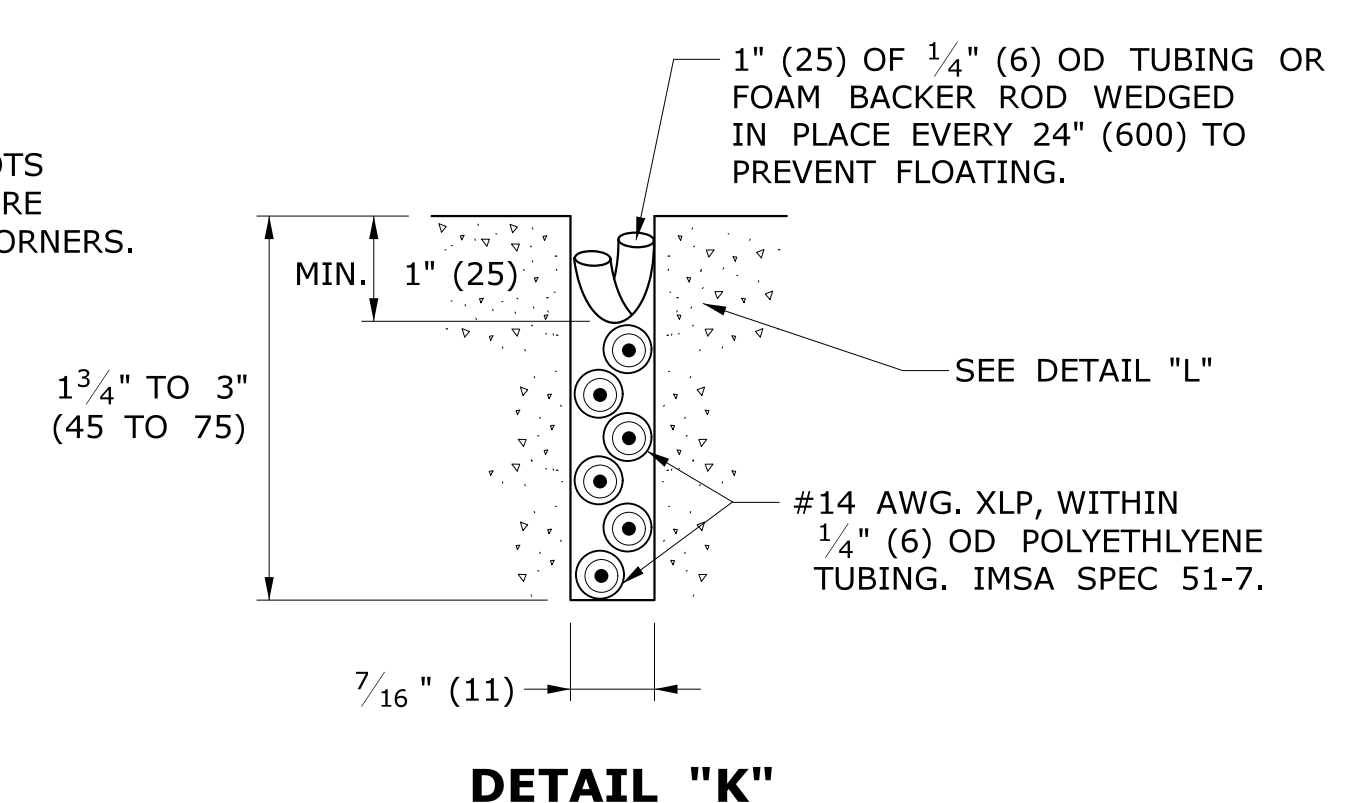
**DETAIL "B"  
DETAIL "C"  
FRONT VIEW**



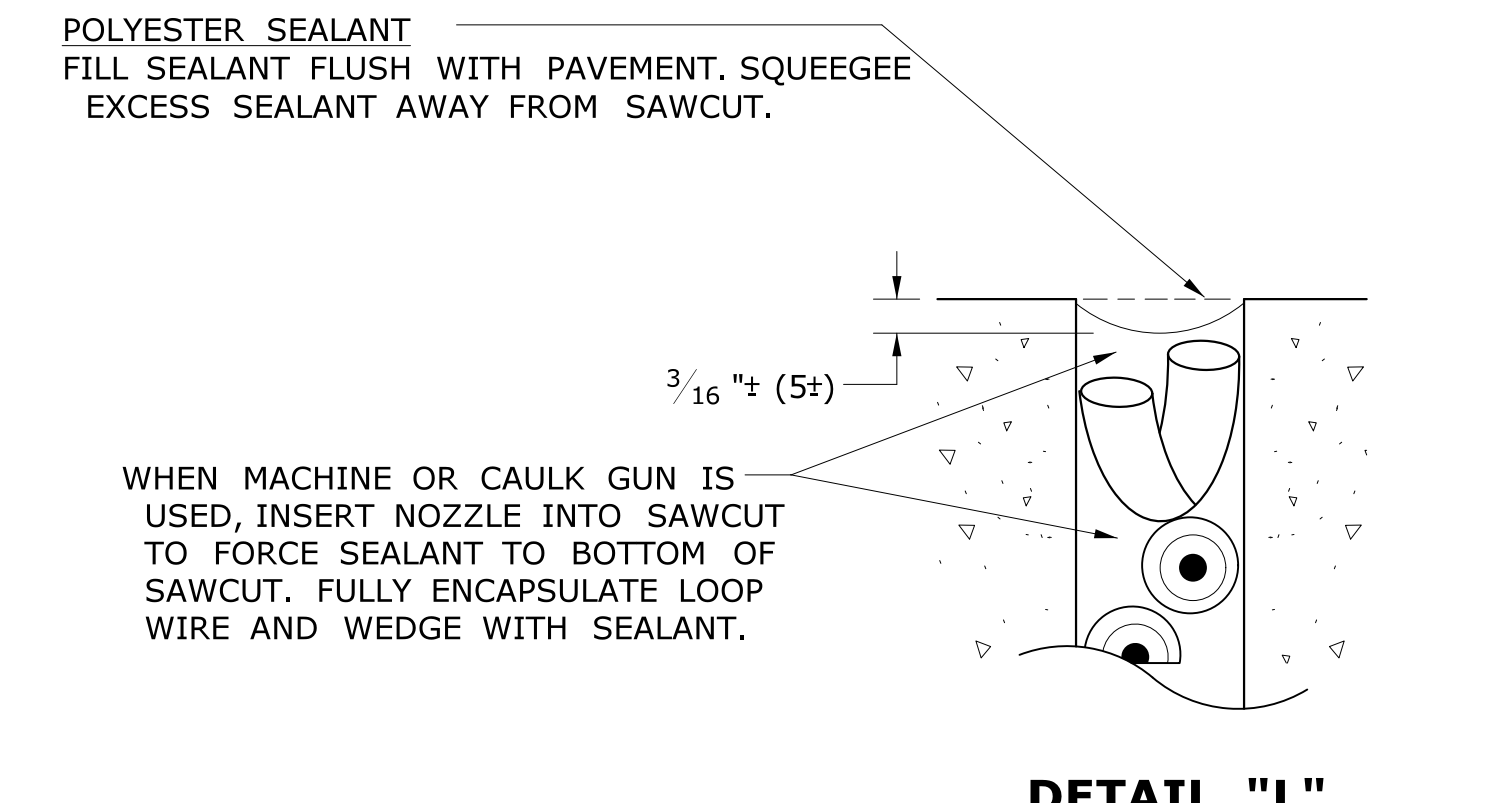
**DETAIL "D"  
DETAIL "E"**



**TYPICAL PLAN VIEW  
LOOP DETECTOR SAWCUT AND LEAD-IN**

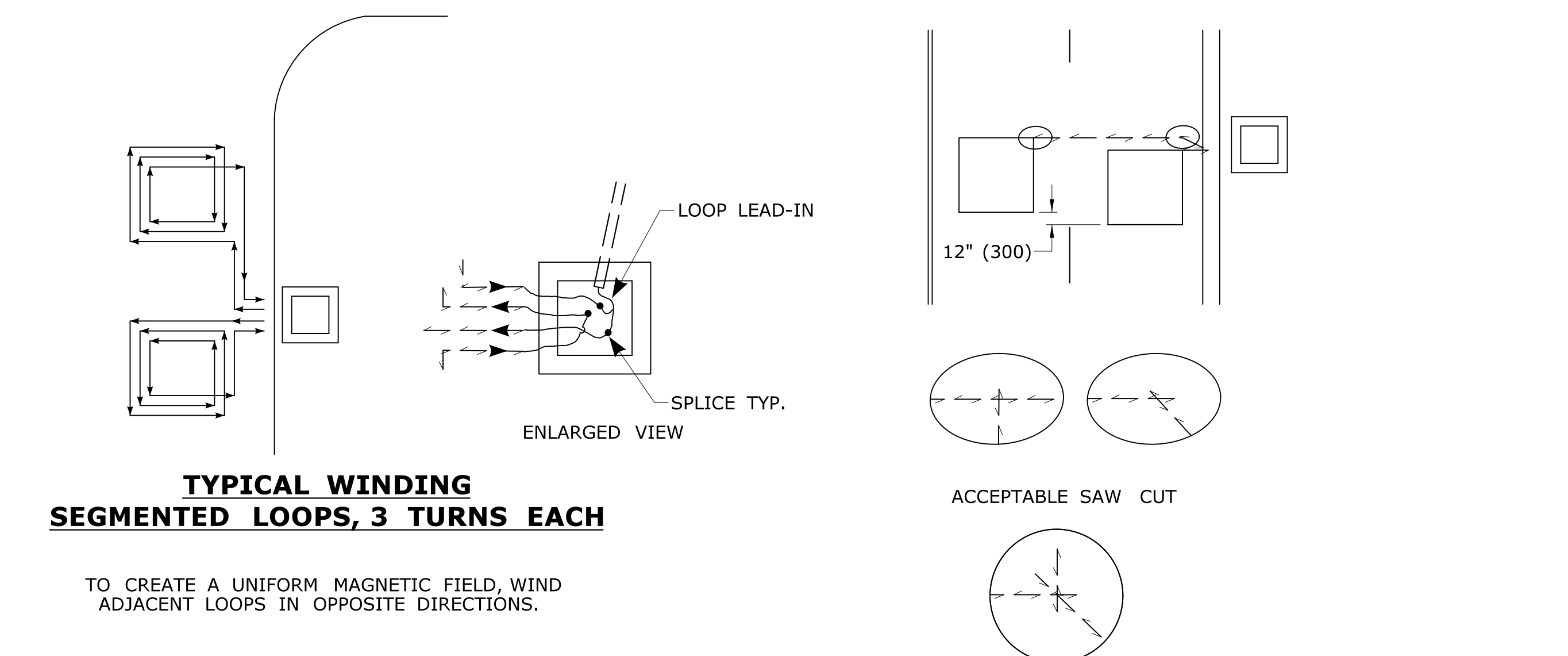


**DETAIL "K"**



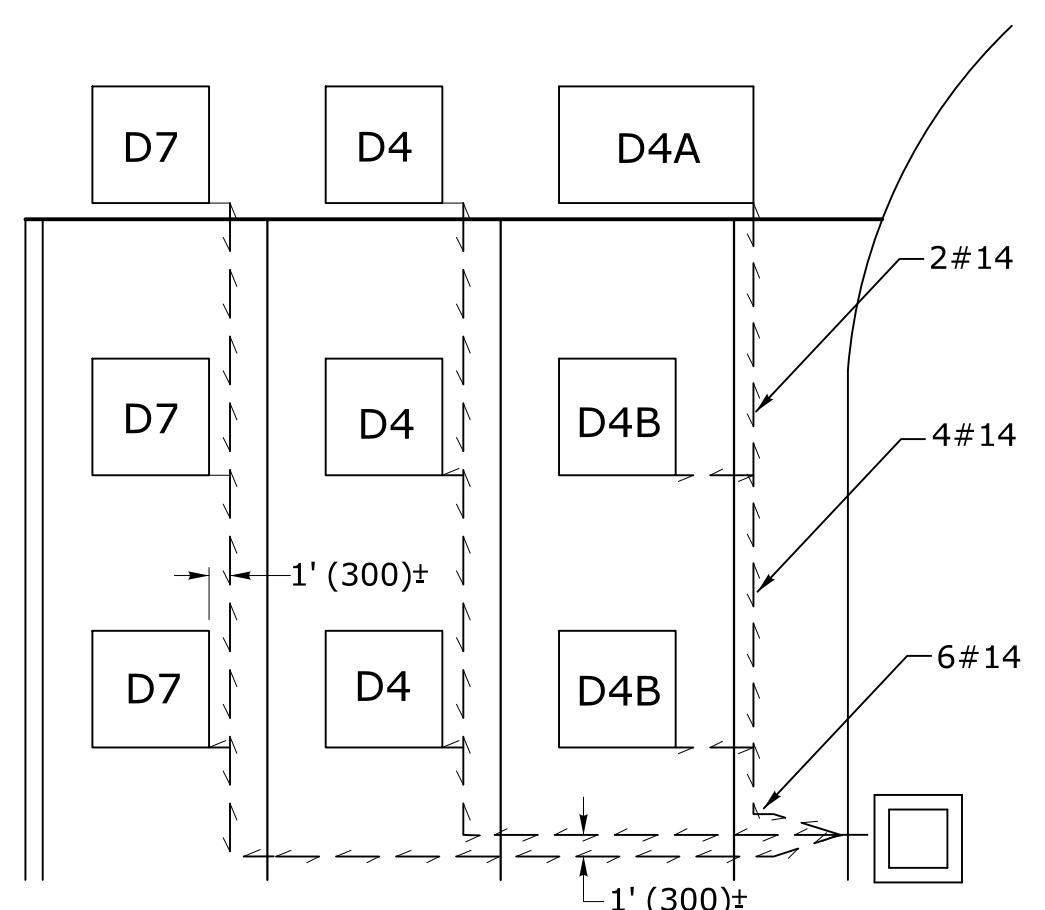
**DETAIL "L"**

NOTES:  
REFER TO STANDARD SPECIFICATIONS, SECTION 11-11.  
ONLY USE POLYESTER COMPOUND AS SEALANT, UNLESS OTHER TYPE IS APPROVED BY ENGINEER.  
WET SAW CUT ONLY, DRY SAW CUT NOT PERMITTED.  
RECOMMENDED SAW BLADE: 14" x 3/8" (350 x 10) PRODUCES 7/16" (11) SLOT.  
SAW CUT LOOP & HOME RUN DEPTH TO ENSURE MIN. 1" (25) SEALANT COVERAGE.

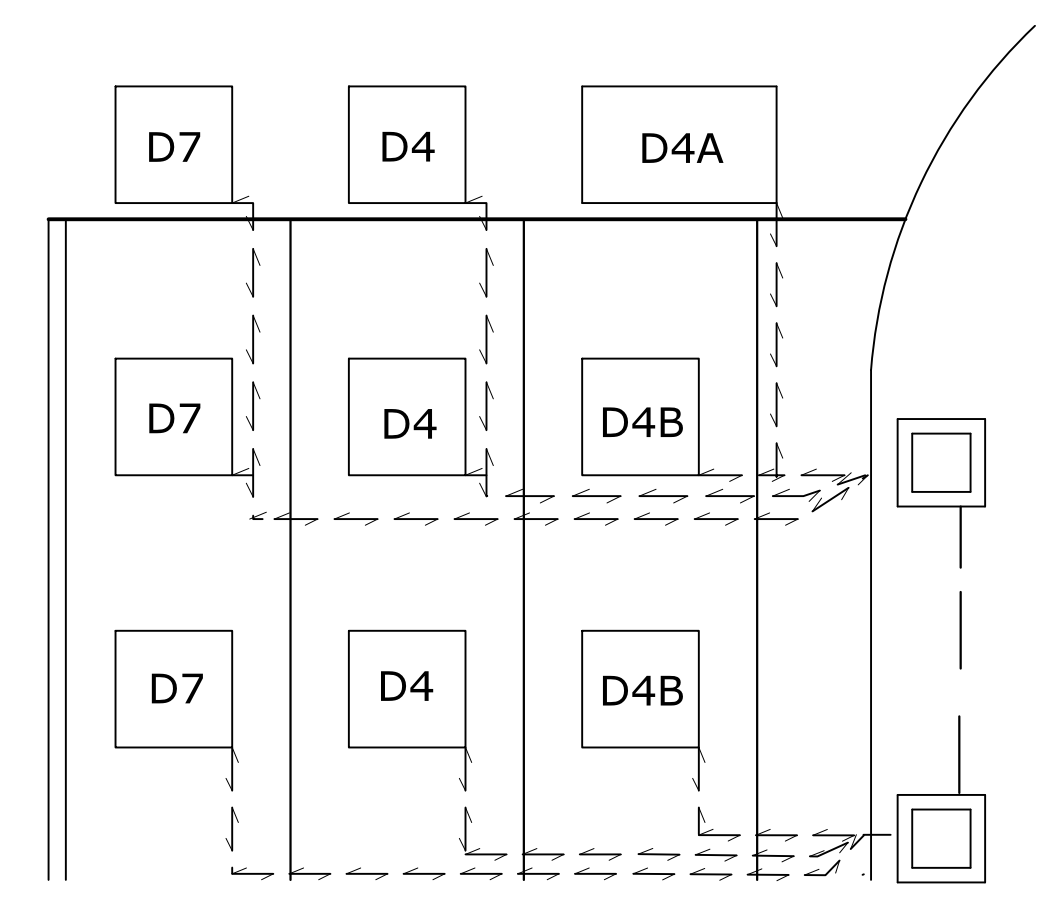


**TYPICAL WINDING  
SEGMENTED LOOPS, 3 TURNS EACH**

TO CREATE A UNIFORM MAGNETIC FIELD, WIND ADJACENT LOOPS IN OPPOSITE DIRECTIONS.



**EXAMPLE A**



**EXAMPLE B  
REAR SEGMENTS ALSO USED FOR VOLUME COUNTS**

SEE TRAFFIC SIGNAL PLAN FOR ACTUAL LOOP PLACEMENT, NUMBERS, ETC...  
LOOP SEGMENTS ON SAME AMPLIFIERS MAY SHARE HOME RUN SAW CUT. SPLICE SEGMENTS IN SERIES.  
LOOP SEGMENTS ON DIFFERENT AMPLIFIERS MUST BE IN SEPARATE HOME RUN SAW CUT.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

- INDUCTIVE LOOP DETECTOR
- - - SAW CUT
- RIGID METAL CONDUIT
- HANDHOLE

REV.	DATE	REVISION DESCRIPTION
3	4-2014	DETAIL "B" - REVISED "J" HOOK NOTE.
2	7-2012	DELETED URETHANE SEALANT NOTE FROM DETAIL "L"
1	4-2012	MINOR REVISIONS.

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Plotted Date: 4/21/2014

DIMENSIONS ARE IN ENGLISH ("') & METRIC UNITS (mm).  
METRIC DIMENSIONS ARE ROUNDED:  
- OVER 1" TO NEAREST 5 mm.  
- UNDER 1" TO NEAREST 1 mm.  
NOT TO SCALE

**STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION**

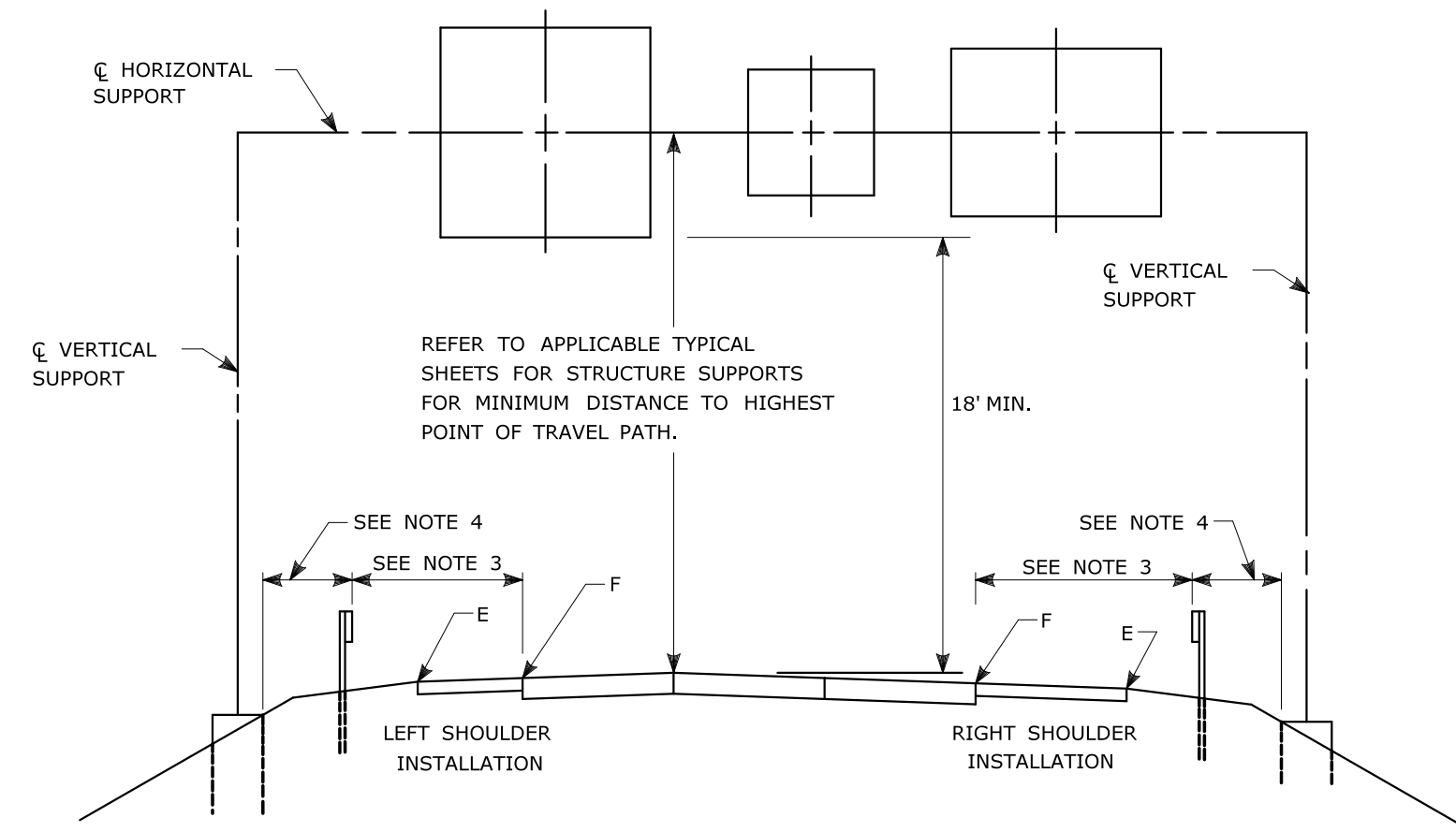
Filename: CTDOT\_TRAFFIC\_STD.DGN Model: TR-1111\_01

SUBMITTED BY: NAME/DATE/TIME:  
APPROVED BY: NAME/DATE/TIME:

**CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING**

STANDARD SHEET TITLE:  
**LOOP VEHICLE DETECTOR  
AND SAWCUT**

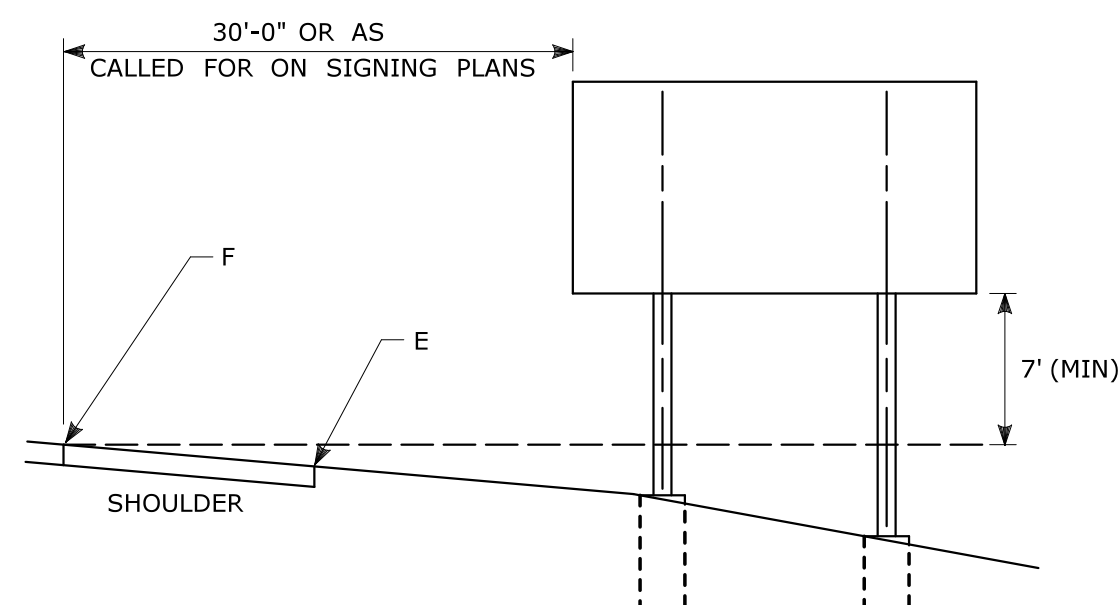
STANDARD SHEET NO.:  
**TR-1111\_01**



**TYPICAL PLACEMENT OF OVERHEAD SIGNS ON SIGN SUPPORTS**

**NOTES:**

- 1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
- 2) BARRIER SYSTEMS MAY BE REQUIRED FOR BOTH SIDES OF SUPPORTS IN MEDIANS.
- 3) IMPACT PROTECTION SHALL BE PROVIDED FOR THE SIGN SUPPORTS LOCATED WITHIN CLEAR ZONE.
- 4) SIGN SUPPORT FOUNDATIONS SHALL BE LOCATED OUTSIDE OF BARRIER SYSTEMS DEFLECTION AREA.
- 5) ALL SIGNS ARE TO BE LEVEL, REGARDLESS OF CAMBER IN SUPPORT.



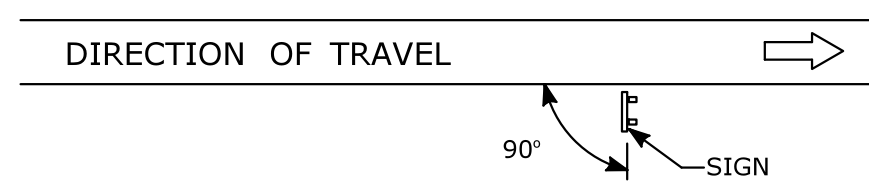
**TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS**

**NOTES:**

- 1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 7'.
- 2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
- 3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF OF SIGN FACE SHALL BE 6' MIN. FROM POINT "E".
- 4) IF 30'-0" MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.

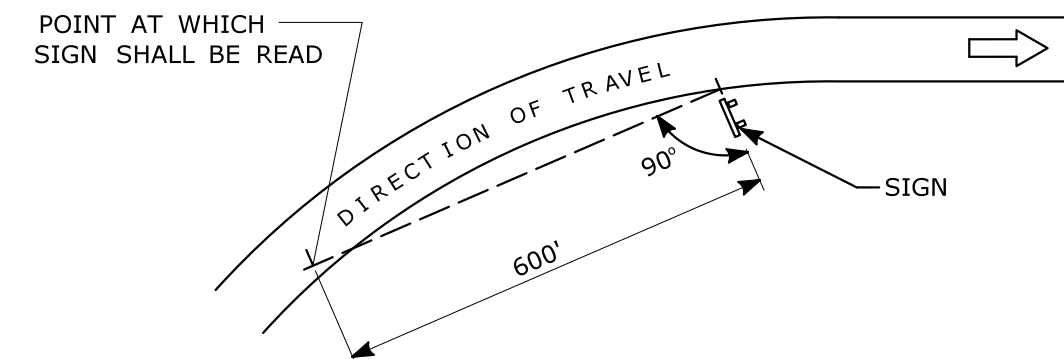
FOR MAXIMUM EFFECTIVENESS, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES. SIGNS LOCATED 30 FT OR MORE FROM THE EDGE OF THE ROAD SHALL BE TURNED APPROXIMATELY 3° TOWARD THE ROAD.



**DIAGRAM "A"**

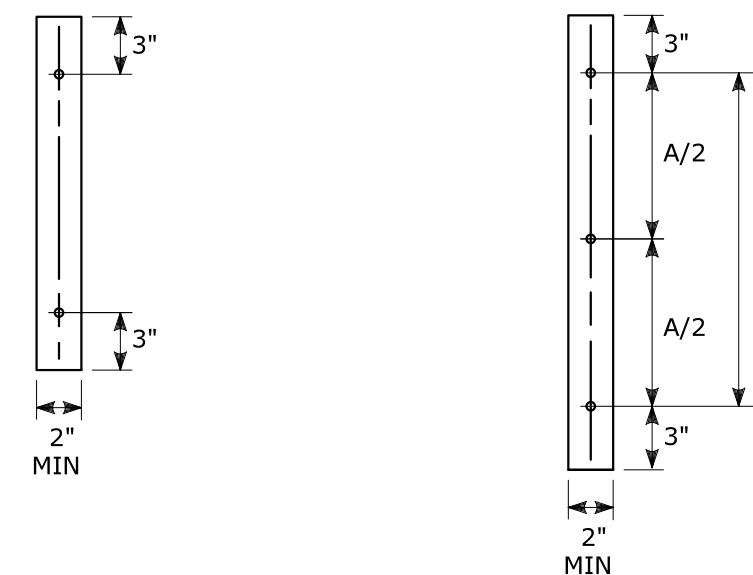
ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.



**DIAGRAM "B"**

**SIGN ORIENTATION DETAILS FOR SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS**

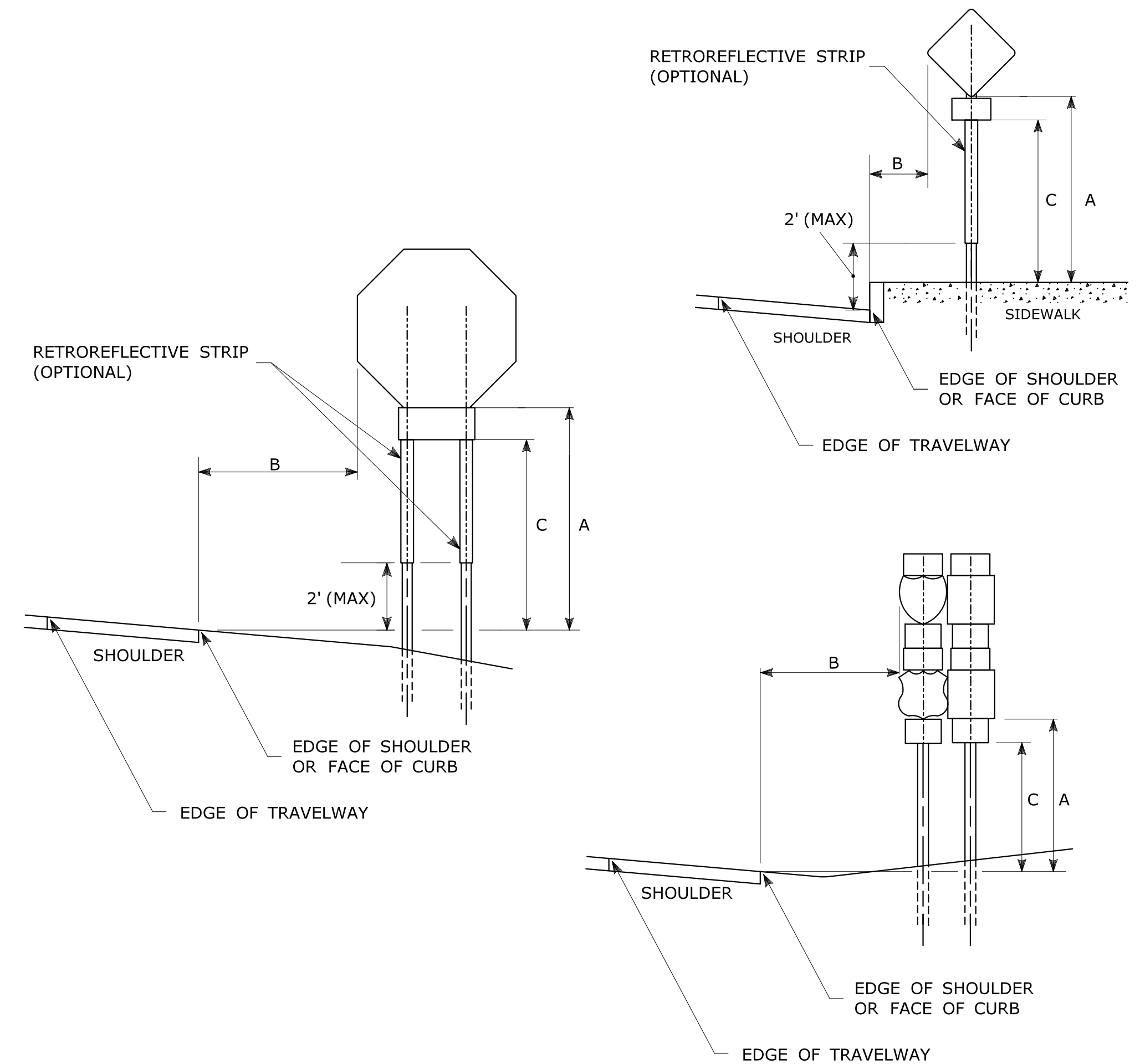
RETROREFLECTIVE STRIPS 48" LONG OR LESS:      RETROREFLECTIVE STRIPS OVER 48" LONG:



**RETROREFLECTIVE STRIP DETAIL**

**NOTES:**

RETROREFLECTIVE STRIPS WHICH ARE 48 IN LONG OR LESS SHALL BE ATTACHED USING 2 BOLTS AND RETROREFLECTIVE STRIPS OVER 48 IN LONG SHALL BE ATTACHED USING 3 BOLTS AS SHOWN ON THE DETAILS ABOVE. REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR MOUNTING DETAILS. RETROREFLECTIVE STRIP COLOR SHALL MATCH THE BACKGROUND COLOR OF THE SIGN, EXCEPT THAT THE COLOR OF THE STRIP FOR "YIELD" AND "DO NOT ENTER" SIGNS SHALL BE RED.



**TYPICAL SIGN PLACEMENT DETAIL**

**NOTES:**

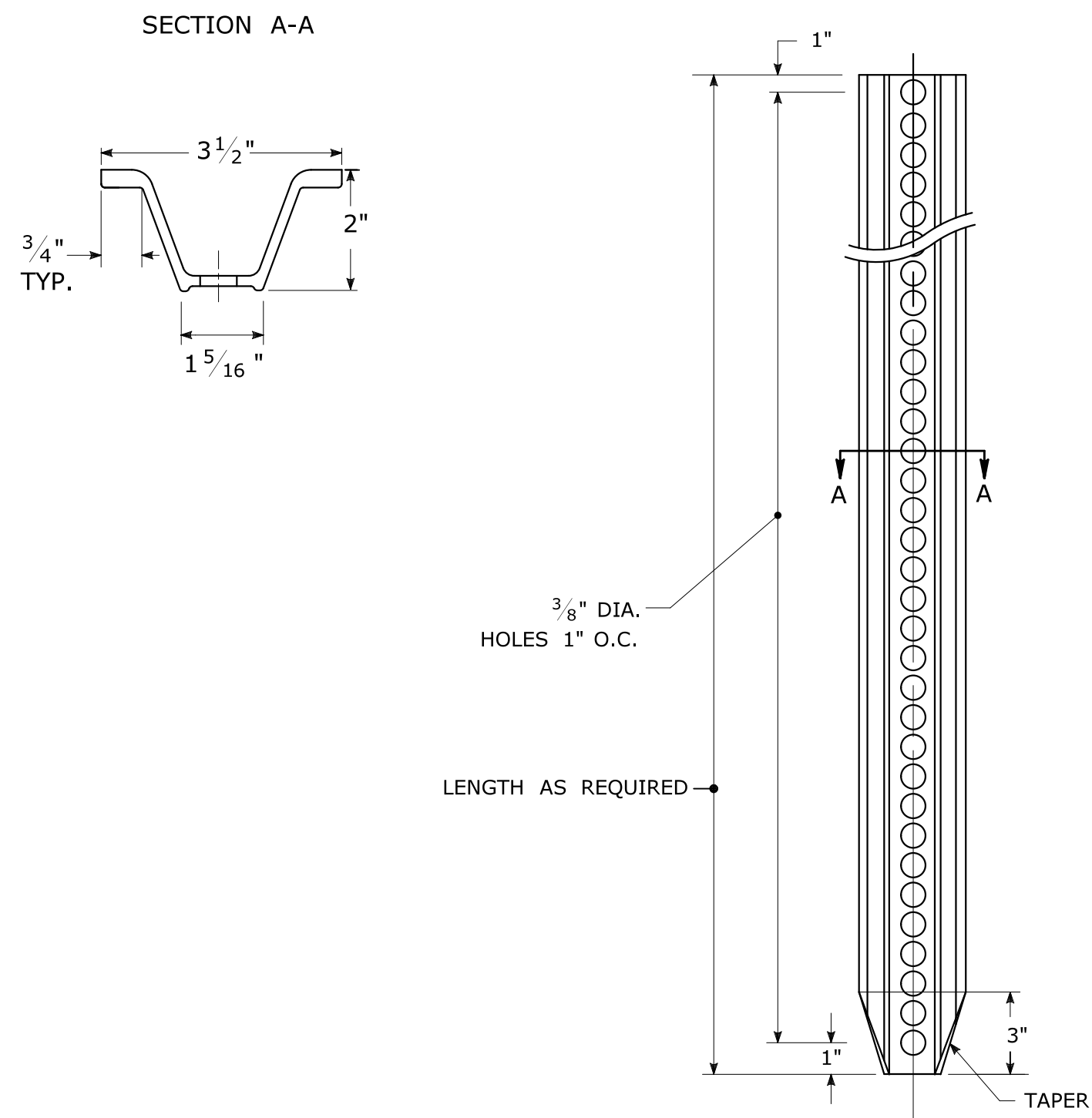
ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY. REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS AND SIGN MOUNTING. IF A RETROREFLECTIVE STRIP IS USED ON SIGN SUPPORT, IT SHALL BE PLACED FOR THE FULL LENGTH OF THE SUPPORT FROM THE BOTTOM OF THE SIGN TO WITHIN 2 FT ABOVE THE EDGE OF THE ROADWAY. PARKING SIGNS TYPICALLY USE 45° MOUNTING BRACKET.

DIM."A" MIN SIGN HEIGHT	DIM."B" MIN LATERAL OFFSET	DIM."C" MIN PLAQUE HEIGHT	ASSEMBLY LOCATION
7' <sup>1</sup>	6' <sup>2</sup>	5'	SIGNS ON FREEWAYS AND EXPRESSWAYS EXCEPT CHEVRON ALIGNMENT SIGNS, ONE-DIRECTION LARGE ARROW SIGNS, DO NOT ENTER SIGNS, AND WRONG WAY SIGNS
5'	2'	4'	• SIGNS IN RURAL AREAS • DO NOT ENTER AND WRONG WAY SIGNS ALONG EXIT RAMP • DO NOT ENTER AND WRONG WAY SIGNS ON LIMITED ACCESS HIGHWAYS
5'	2'	N/A	• CHEVRON ALIGNMENT SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMP, AND IN RURAL AREAS • ONE-DIRECTION LARGE ARROW SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMP, AND IN RURAL AREAS
4'	2'	4'	CENTRAL ISLANDS OF ROUNDABOUTS
7'	2' <sup>3</sup>	6'	BUSINESS & RESIDENTIAL AREAS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
7'	2' <sup>4</sup>	7'	SIDEWALKS <sup>5</sup>

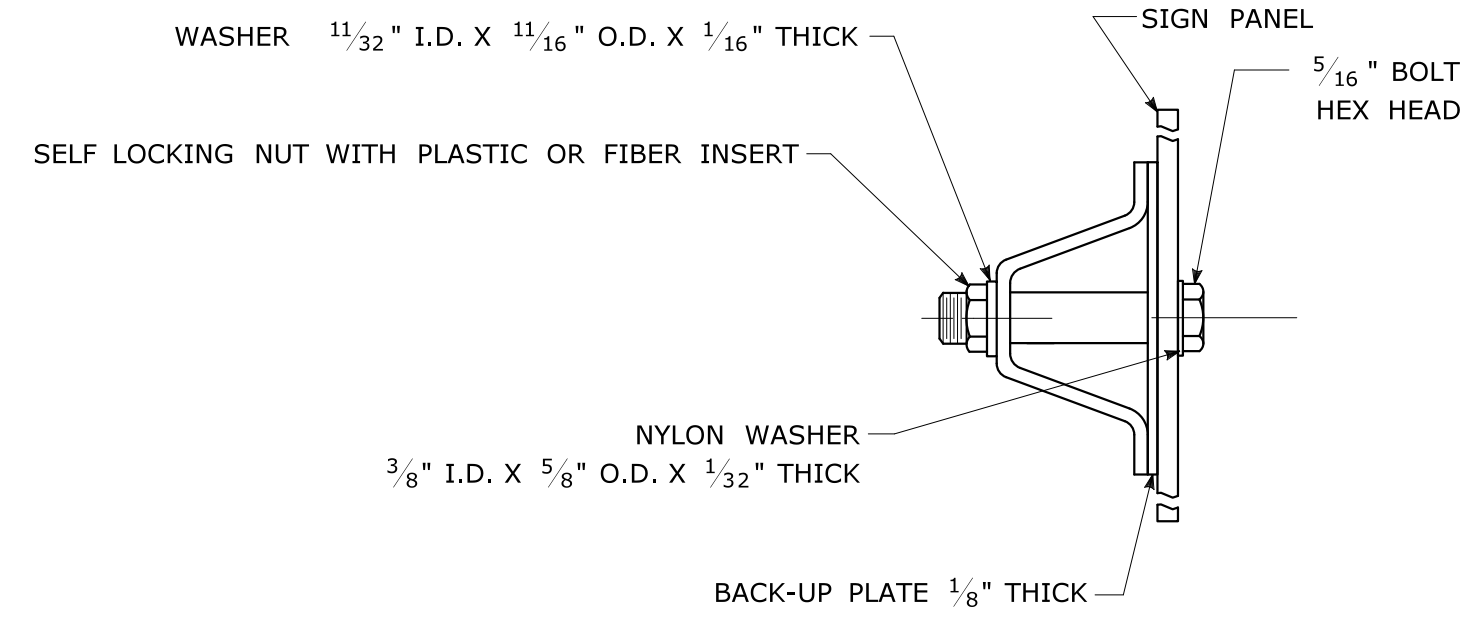
- <sup>1</sup> OR AS DIRECTED BY THE ENGINEER
- <sup>2</sup> 8 FT MINIMUM HEIGHT REQUIRED IF A SUPPLEMENTAL PLAQUE IS SUBMOUNTED BELOW THE MAJOR SIGN.
- <sup>3</sup> 6 FT FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6 FT WIDE  
12 FT FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6 FT WIDE.
- <sup>4</sup> A LATERAL OFFSET OF AT LEAST 1 FT FROM THE FACE OF THE CURB MAY BE USED WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING UTILITY POLES ARE CLOSE TO THE CURB.
- <sup>5</sup> A CLEAR PATH OF NOT LESS THAN 4 FT SHALL BE PROVIDED IN SIDEWALK AREAS.



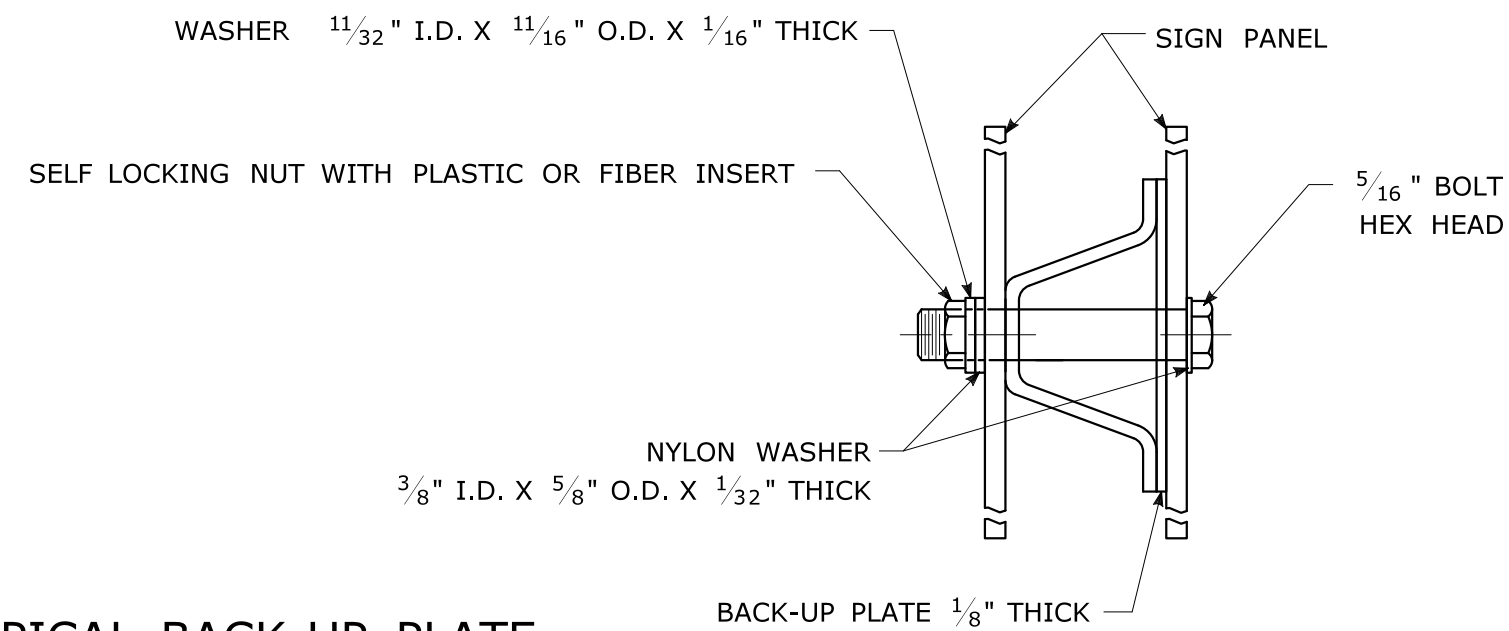
TYPICAL METAL SIGN POSTS



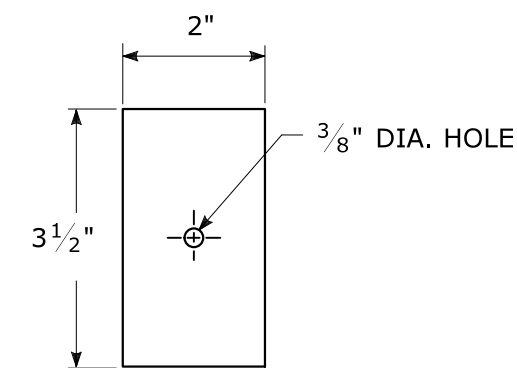
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT



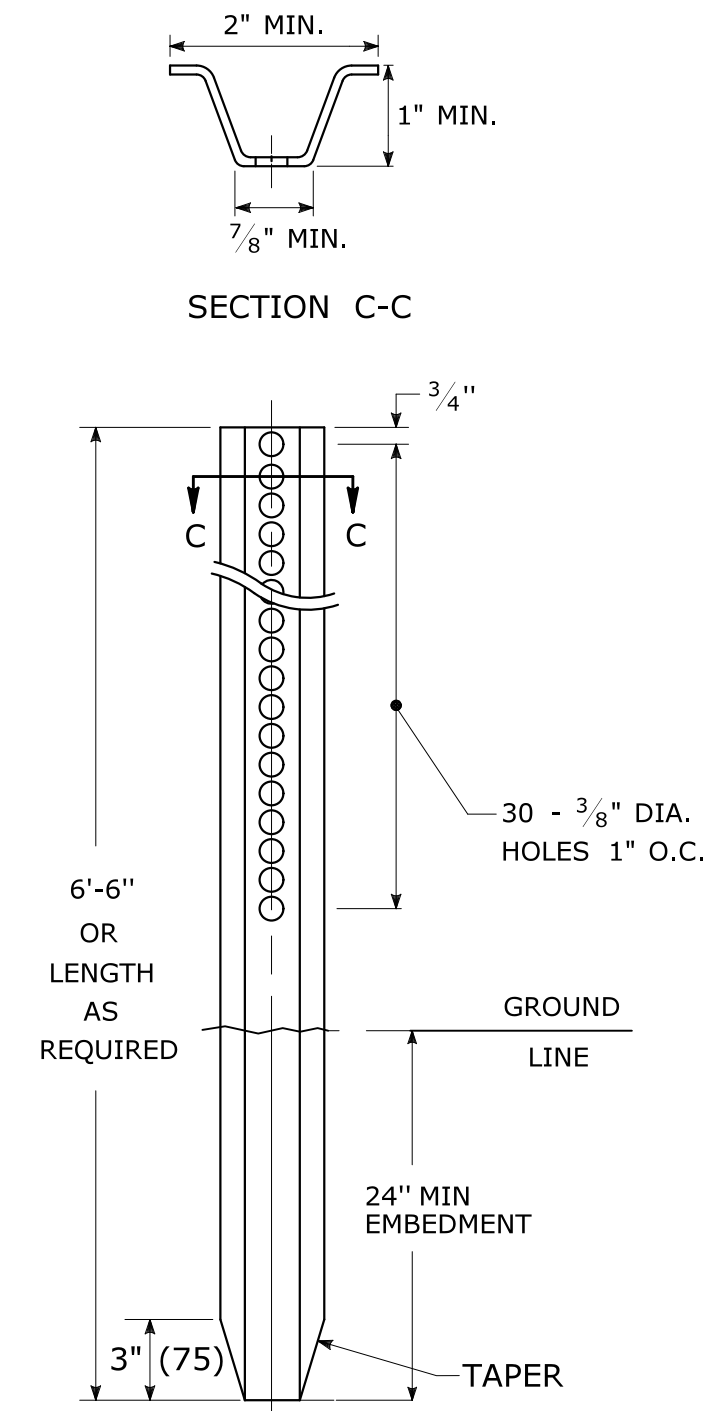
TYPICAL BACK-UP PLATE



BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).  
 SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).  
 WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240, (ALLOY TYPES 304 OR 316).

METAL DELINEATOR POST

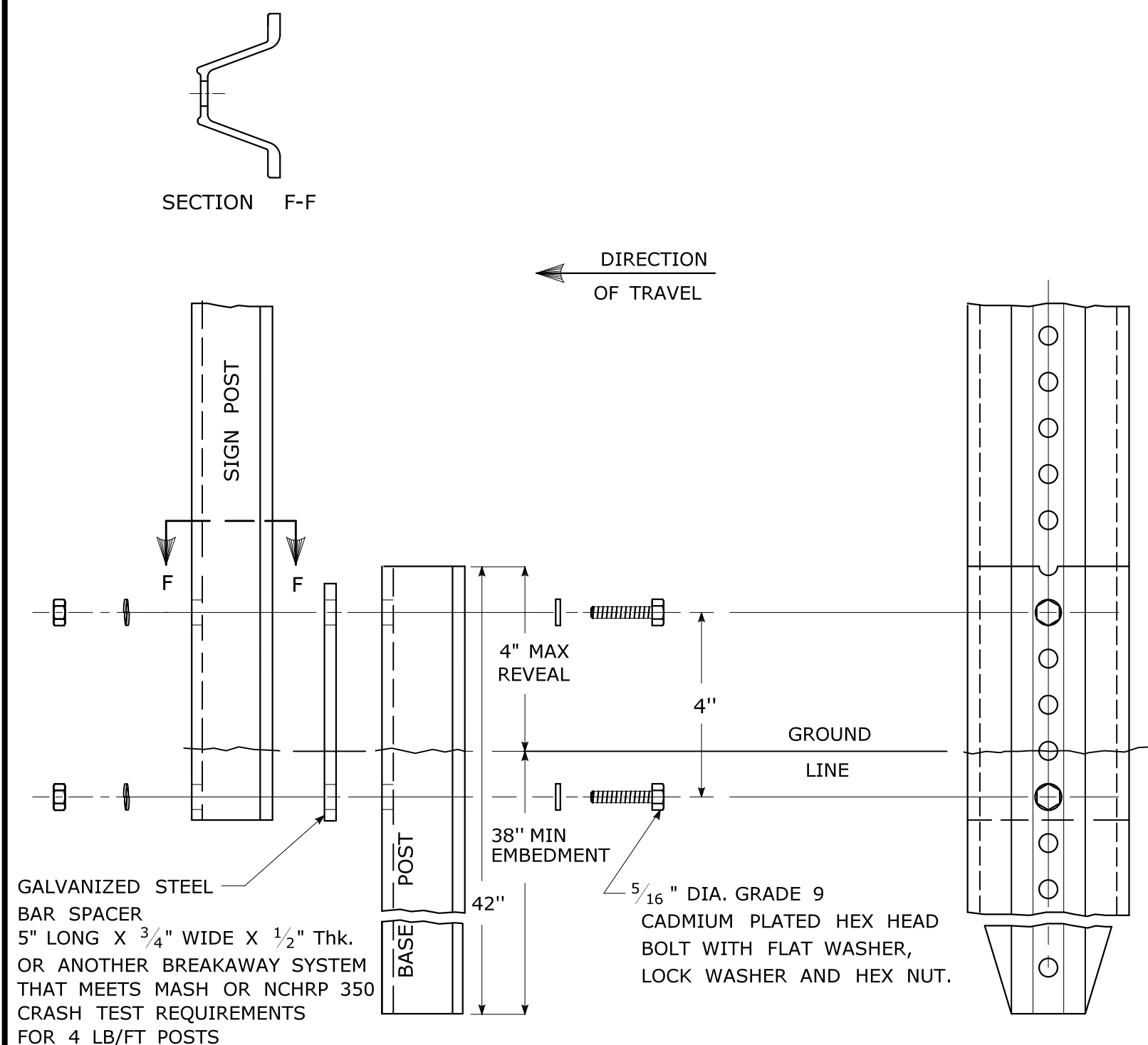
WT./FT. = 1.12 LBS./FT. MIN.



GENERAL NOTES:

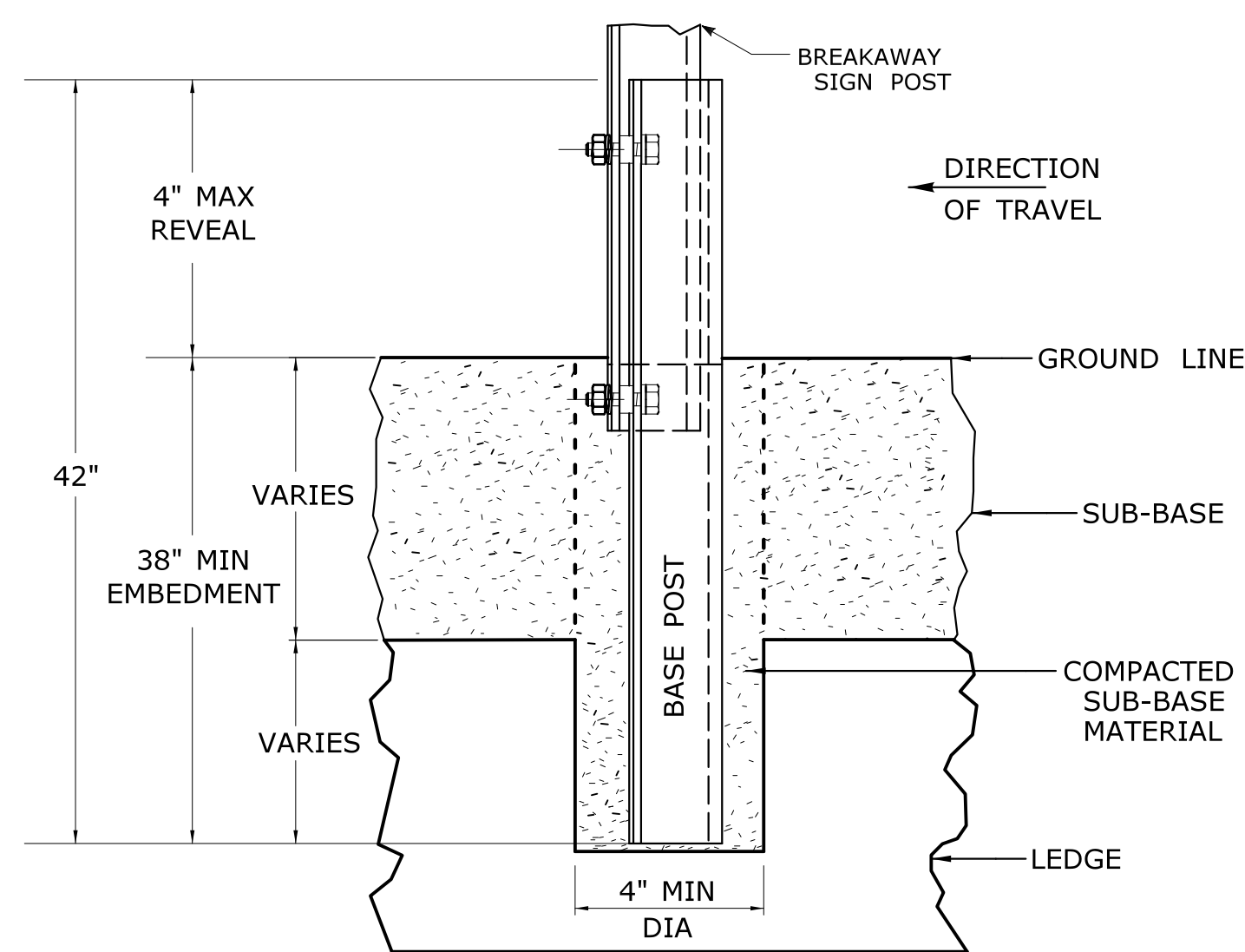
- STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
- AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
- WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
- SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
- ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
- ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 MPH WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- SIGN POSTS SHALL BE 4 LBS./FT.

BREAKAWAY INSTALLATION FOR 4 LBS./FT. POSTS

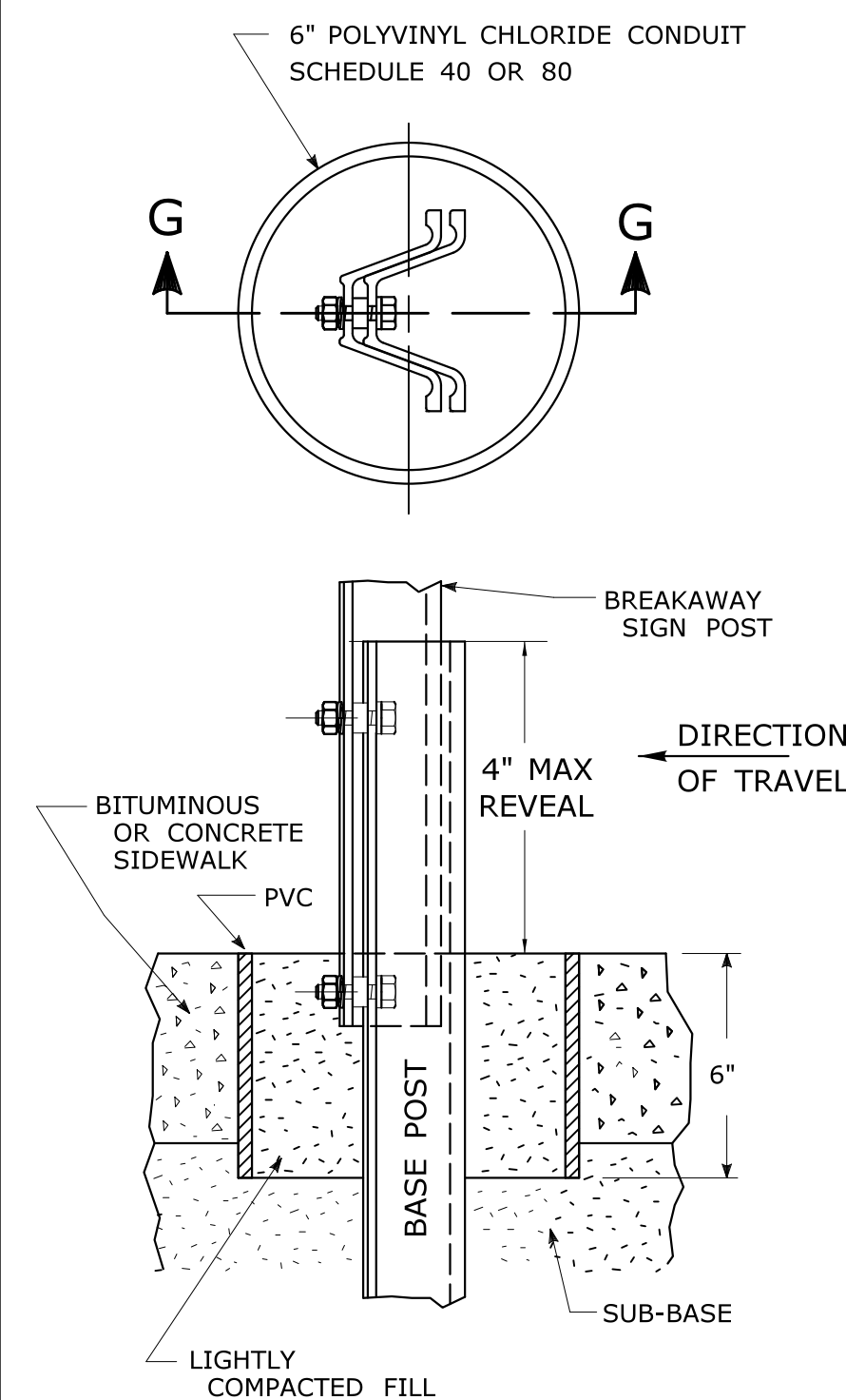


TYPICAL SIGN POST INSTALLATION IN LEDGE

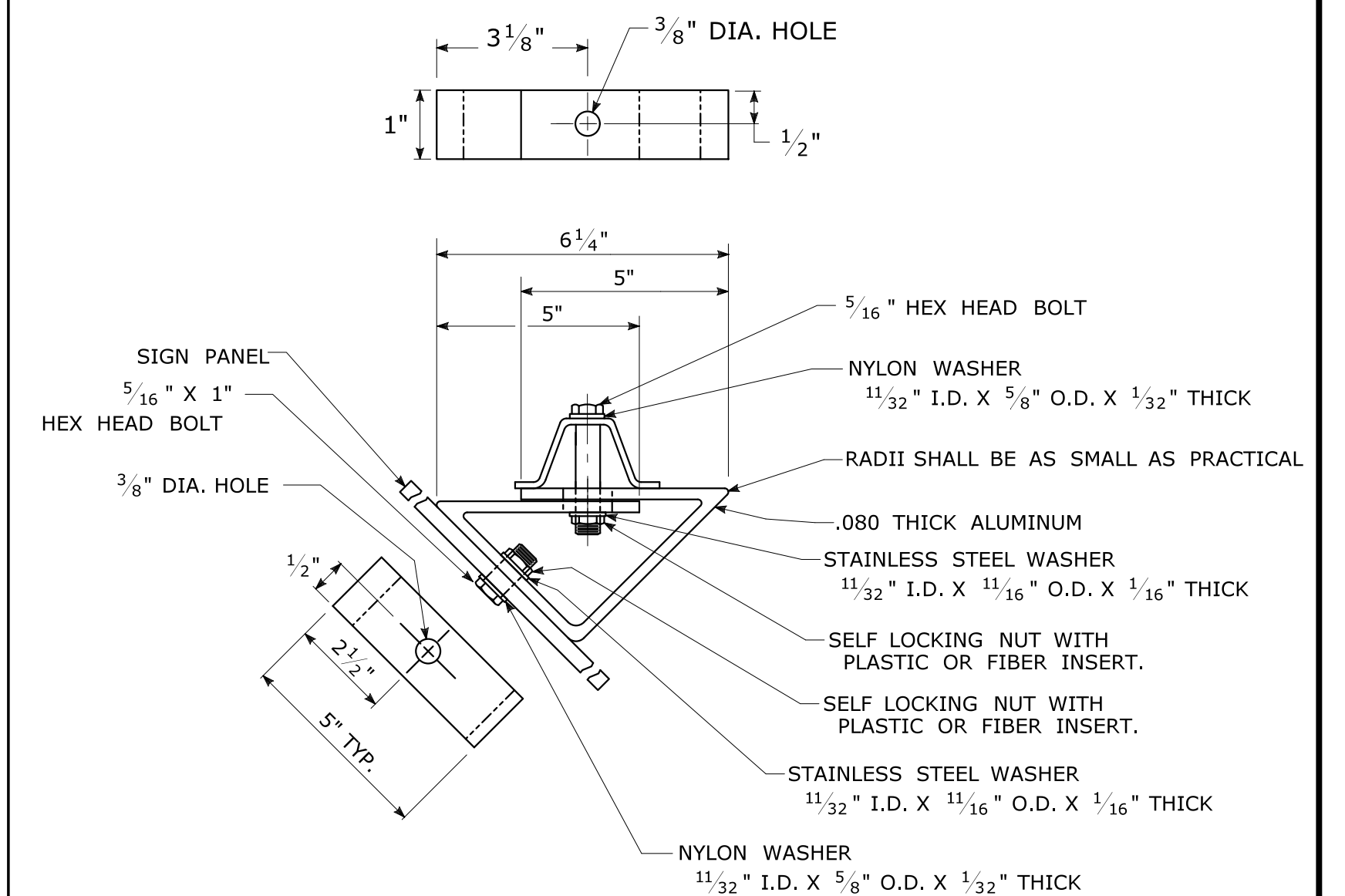
LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38".  
 HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



TYPICAL SLEEVE FOR PAVED AREAS



45° MOUNTING BRACKET FOR INSTALLATION OF PARKING SIGNS



REV.	DATE	REVISION DESCRIPTION
2	6-2017	SIGN POST REVISIONS.
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/6/2017

NOT TO SCALE



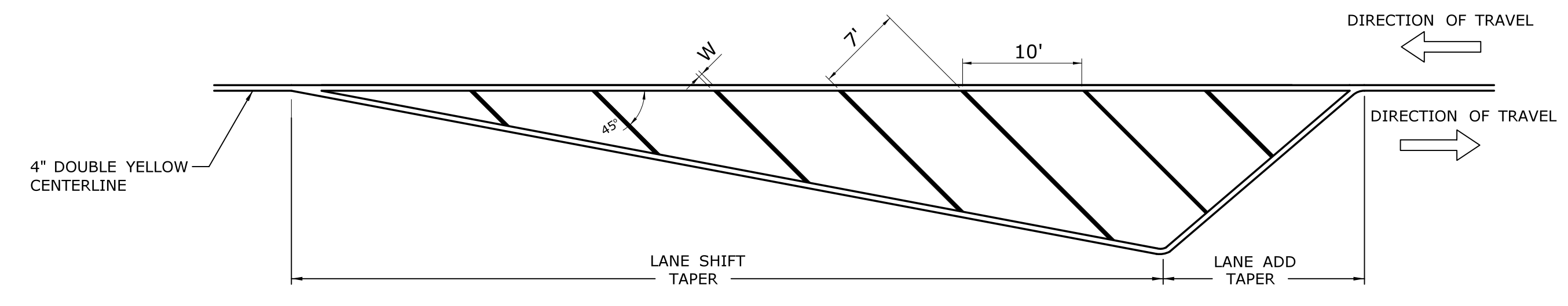
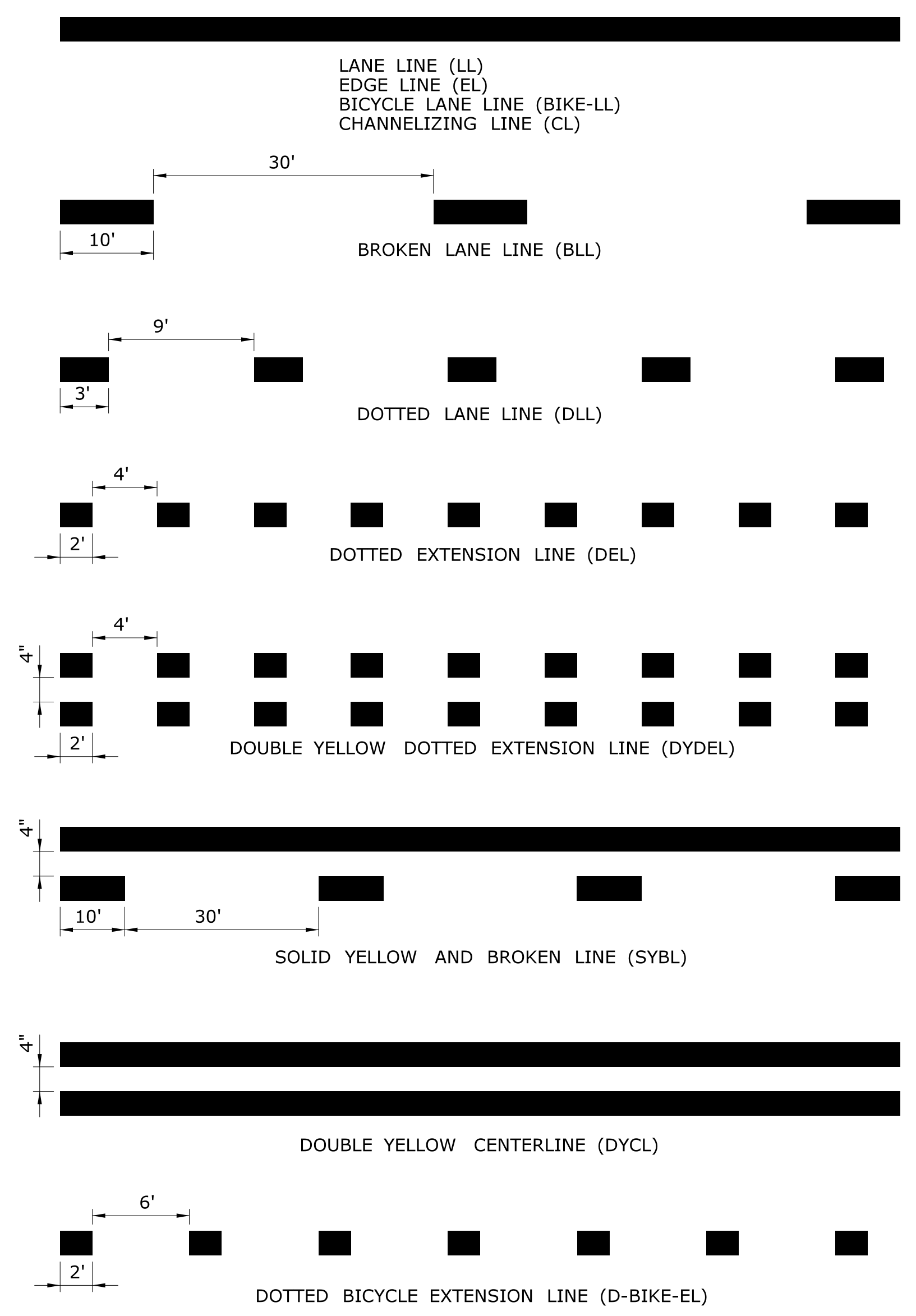
Filename: TR-1208\_02\_May\_2017\_Revision.dgn Model: TR-1208\_02

SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

CTDOT  
 STANDARD SHEET  
 OFFICE OF ENGINEERING

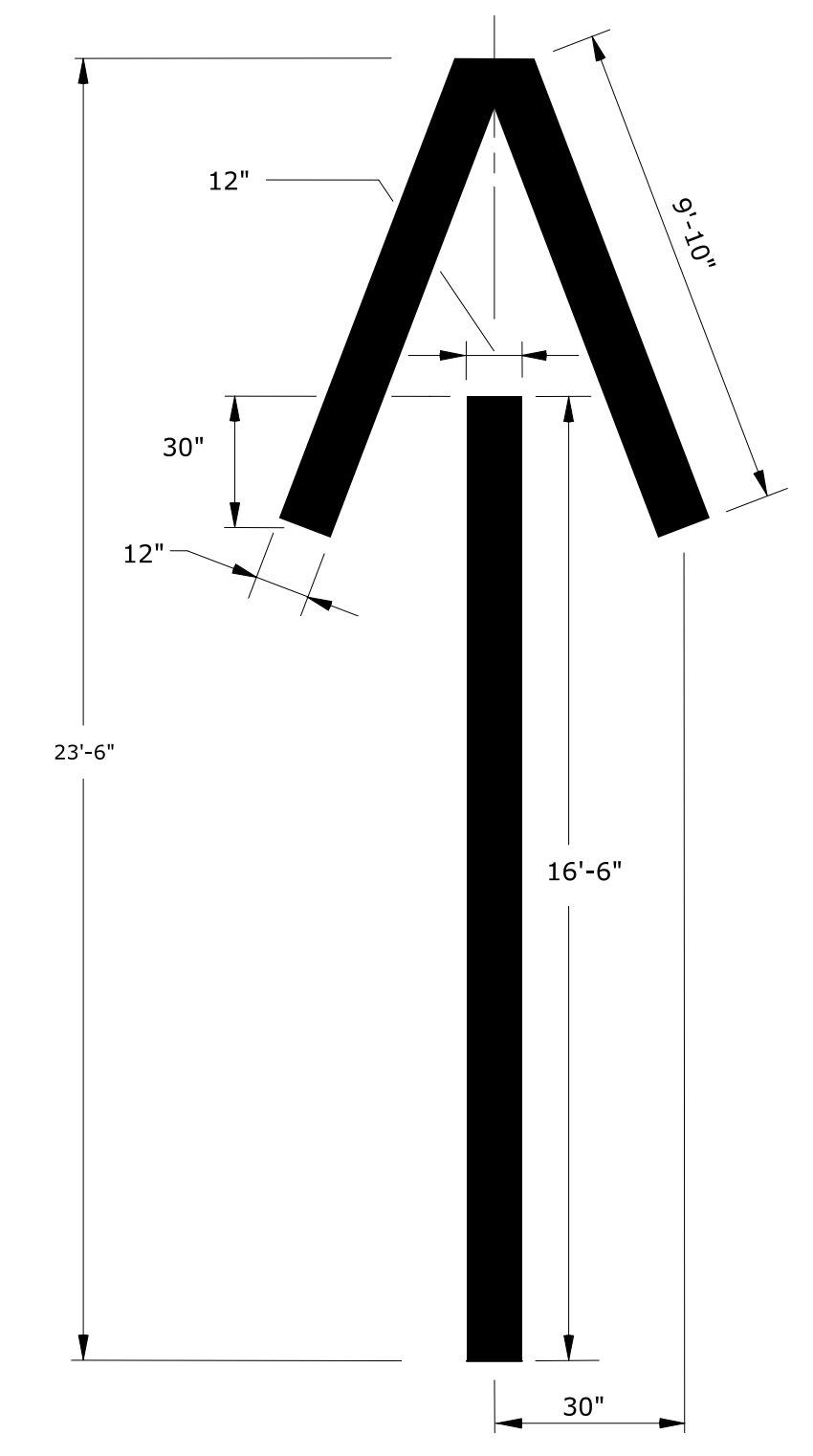
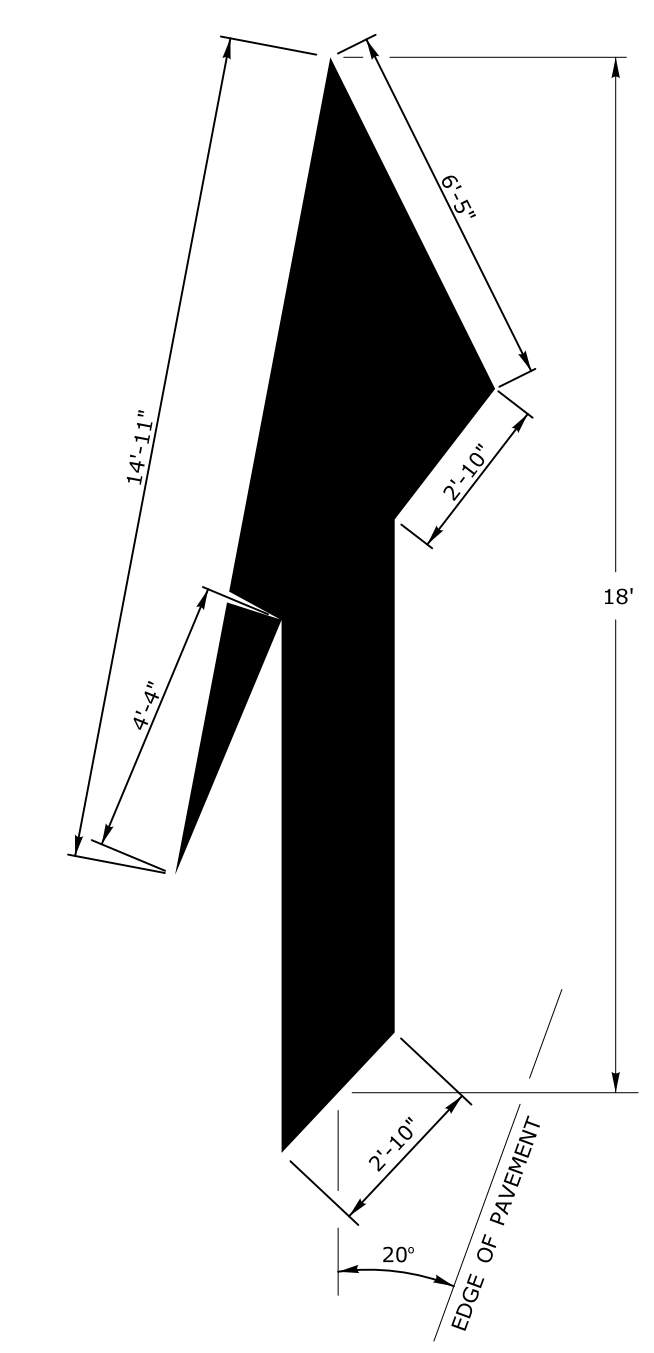
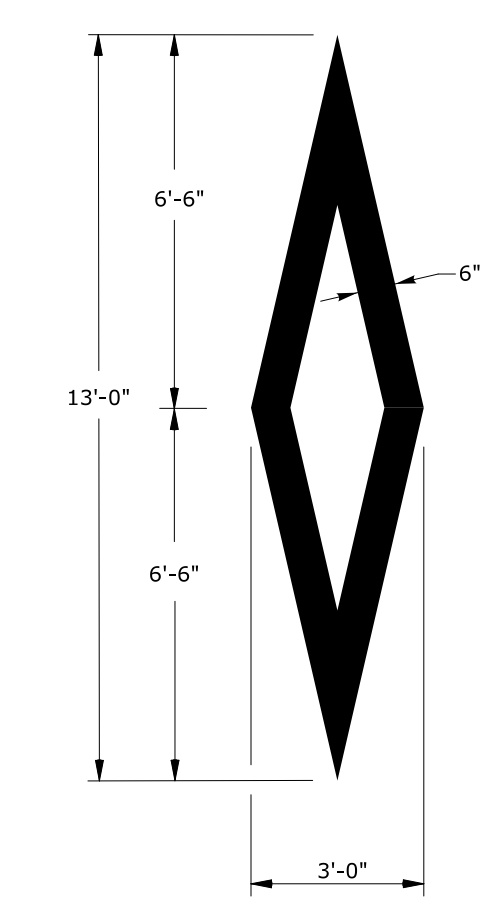
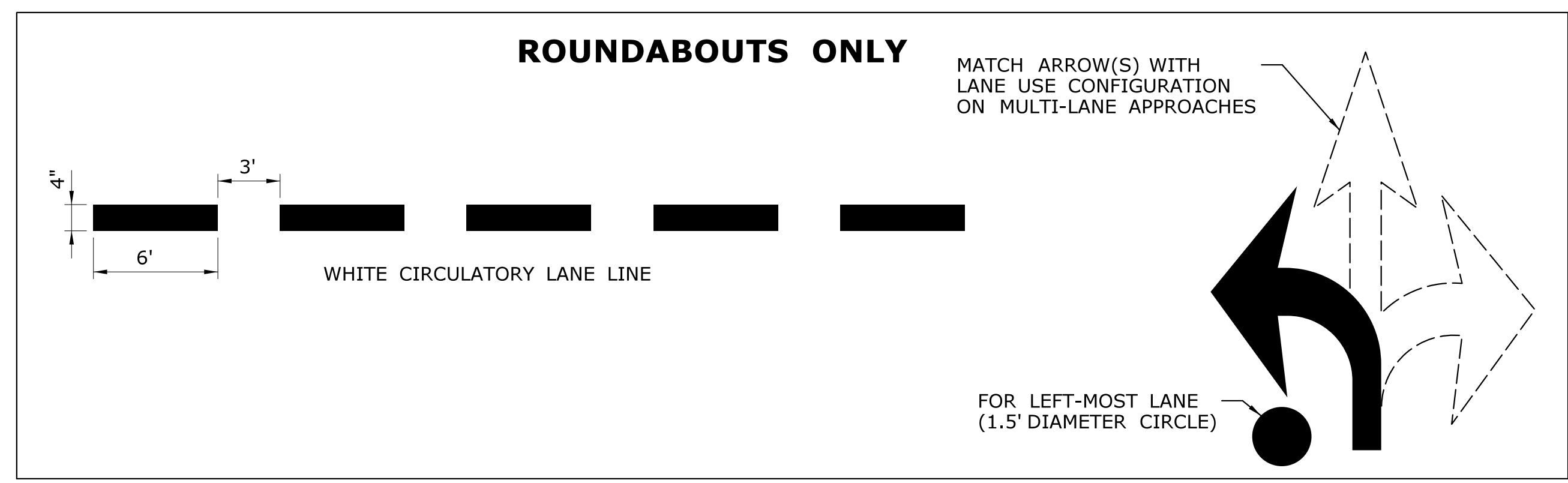
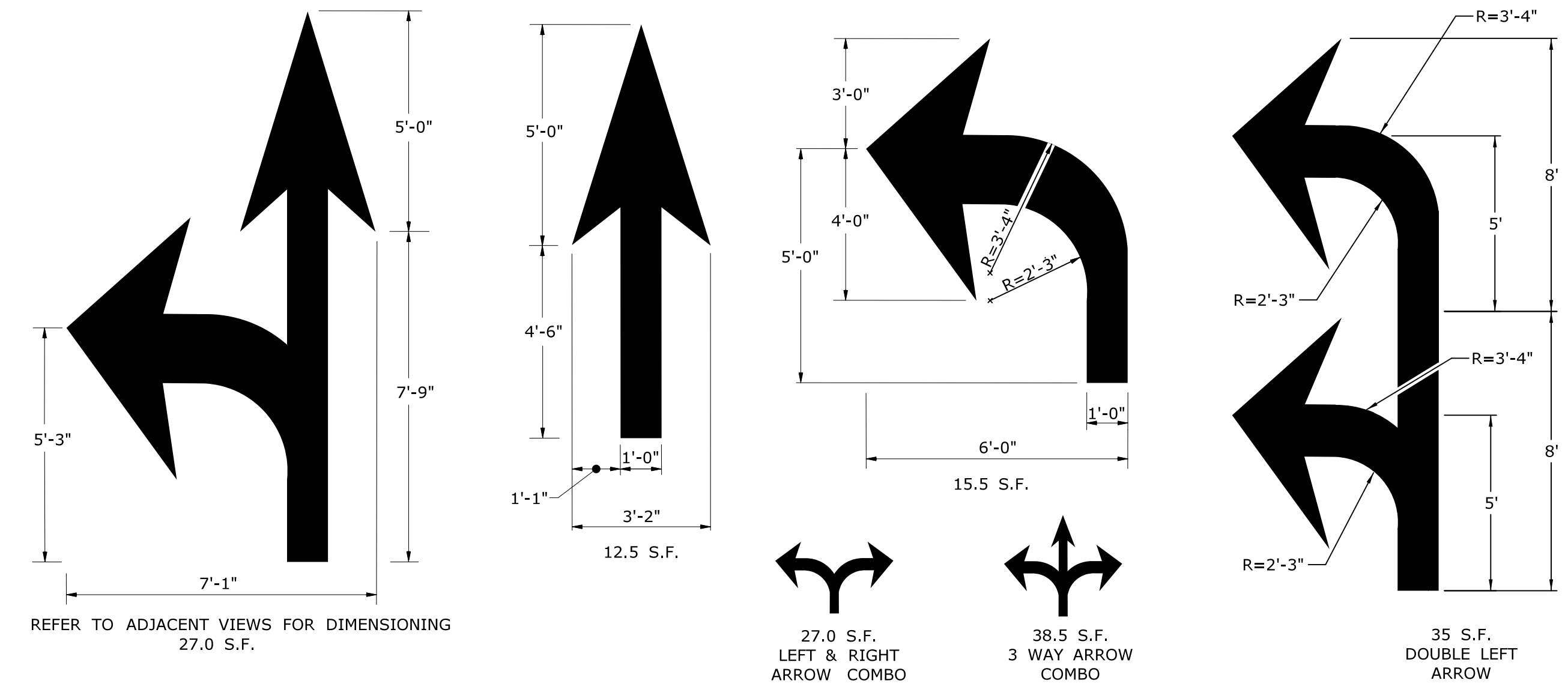
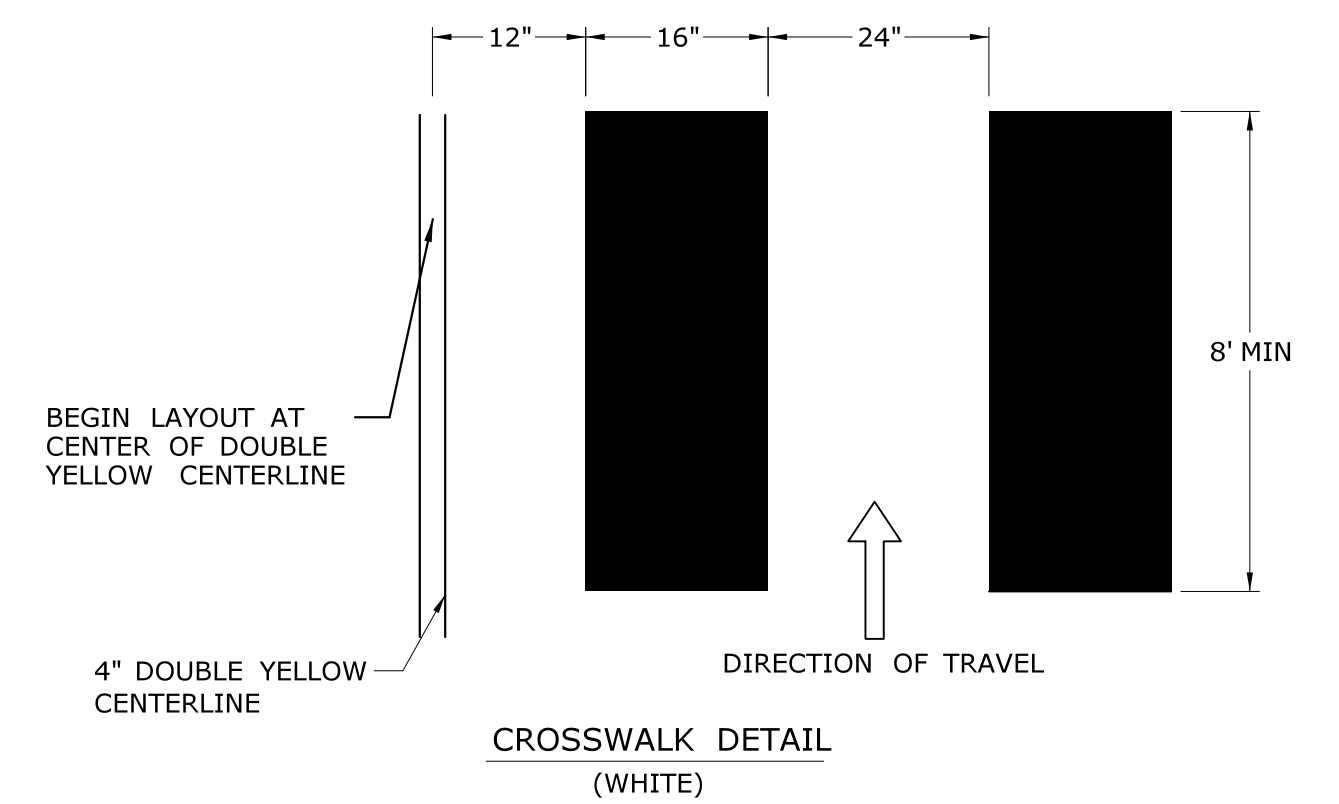
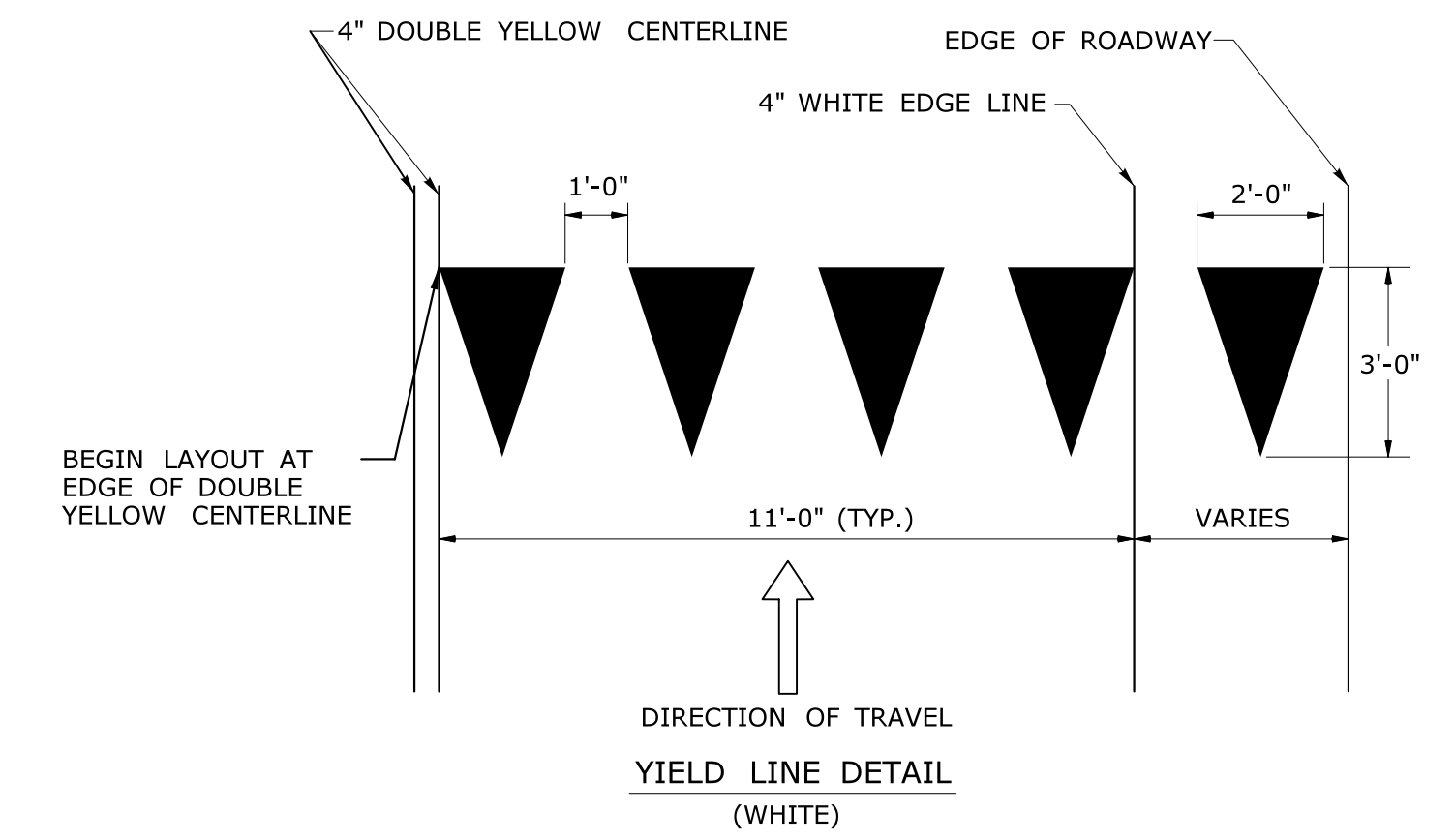
STANDARD SHEET TITLE:  
**METAL SIGN POSTS  
 AND SIGN MOUNTING DETAILS**

GUIDE SHEET NO.:  
**TR-1208\_02**



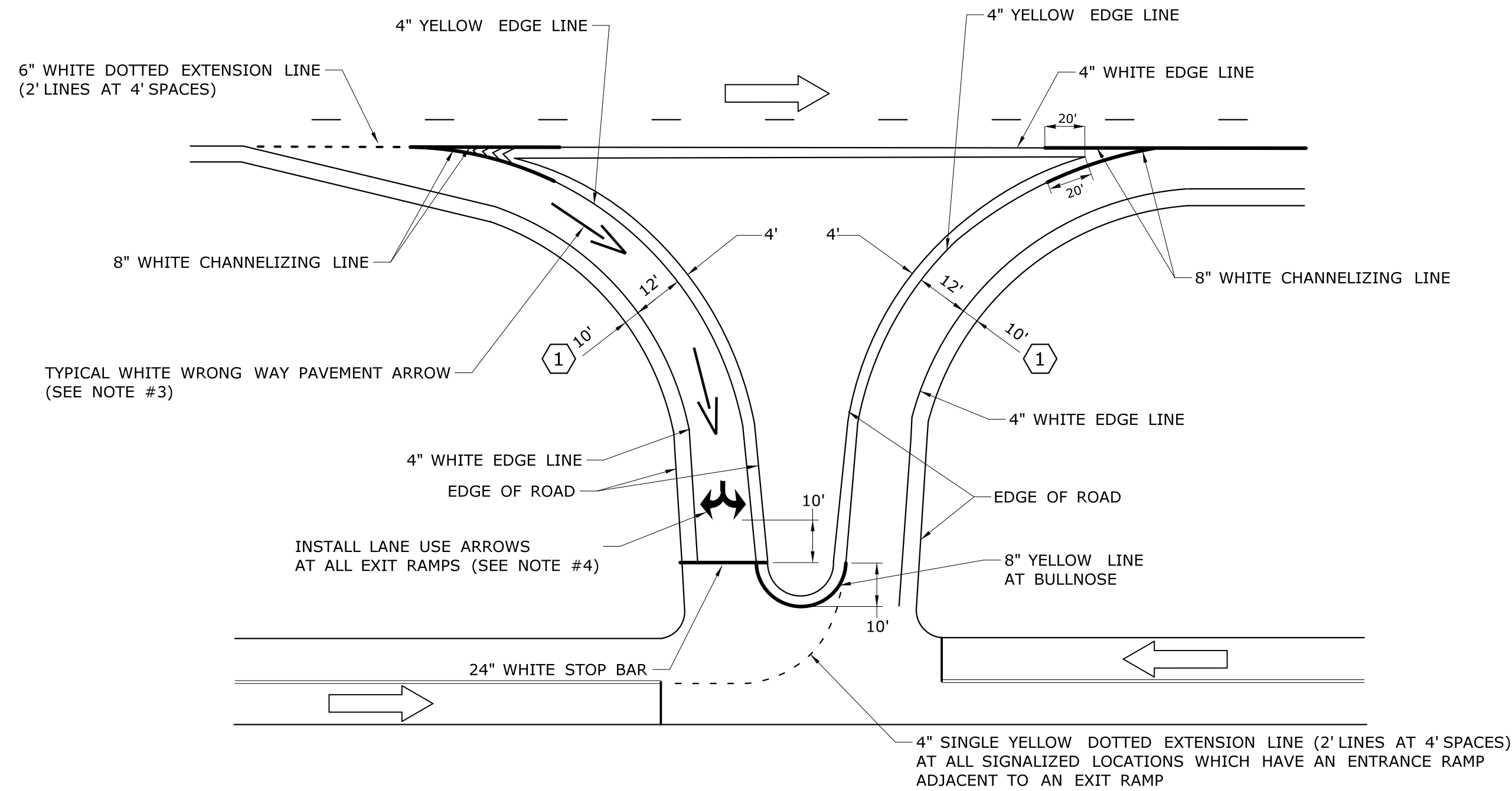
CROSS HATCHED ISLAND DETAIL  
(YELLOW)

W IS TO BE 6" WHEN POSTED SPEED ≤ 45 MPH  
W IS TO BE 12" WHEN POSTED SPEED > 45 MPH  
CROSS HATCHED ISLANDS ARE TO BE INSTALLED WHERE CALLED FOR ON THE PLANS

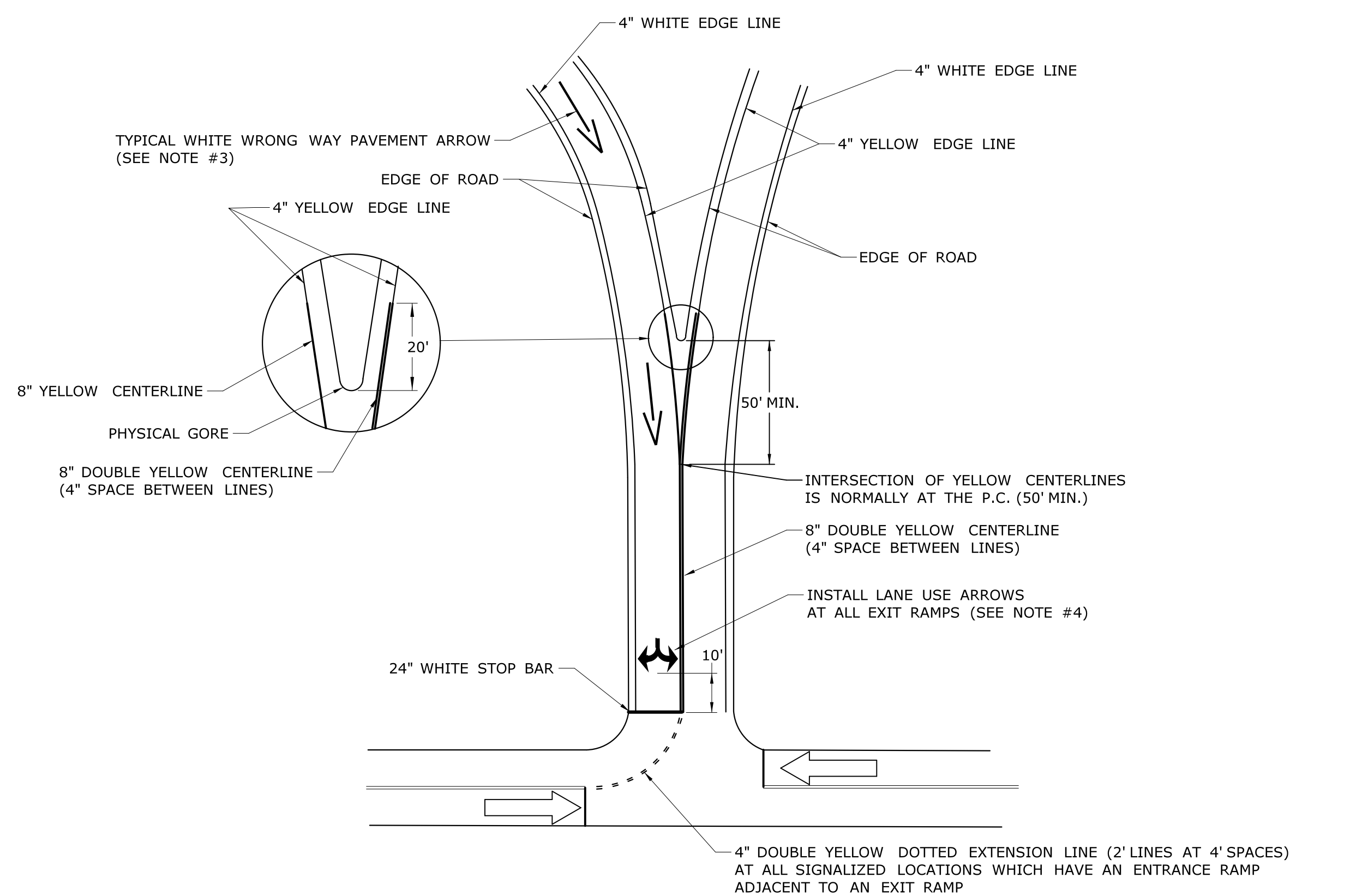


- NOTES :
1. AREA OF PAVEMENT MARKINGS AS INDICATED IS APPROXIMATE.
  2. RIGHT TURN PAVEMENT MARKING ARROWS ARE MIRROR IMAGE OF LEFT TURN PAVEMENT MARKING ARROWS.

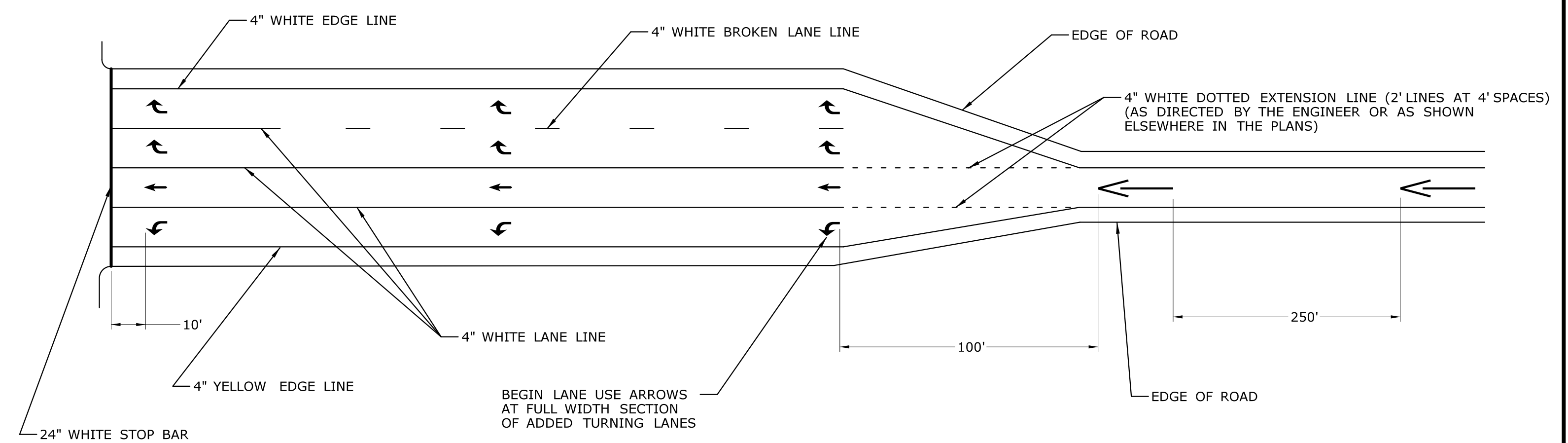




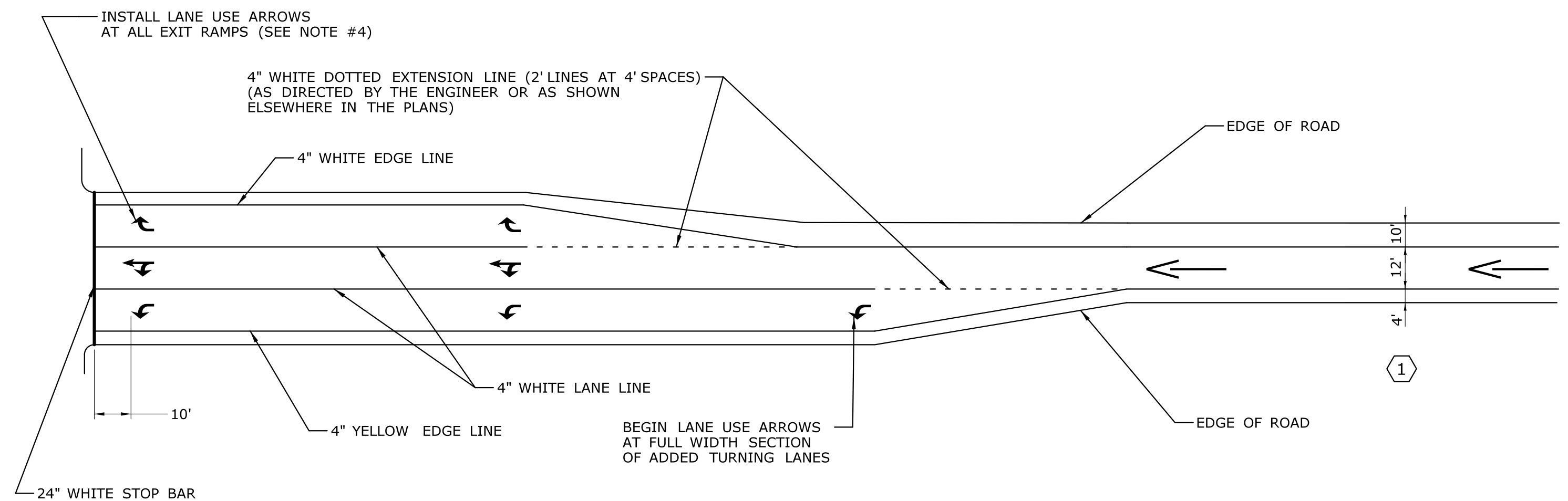
**R** TYPICAL RAMP TREATMENT ON ABUTTING RAMP WITH MEDIAN



**S** TYPICAL RAMP TREATMENT ON ABUTTING RAMP WITHOUT MEDIAN



**T** TYPICAL RAMP TREATMENT FOR DOUBLE TURN LANES

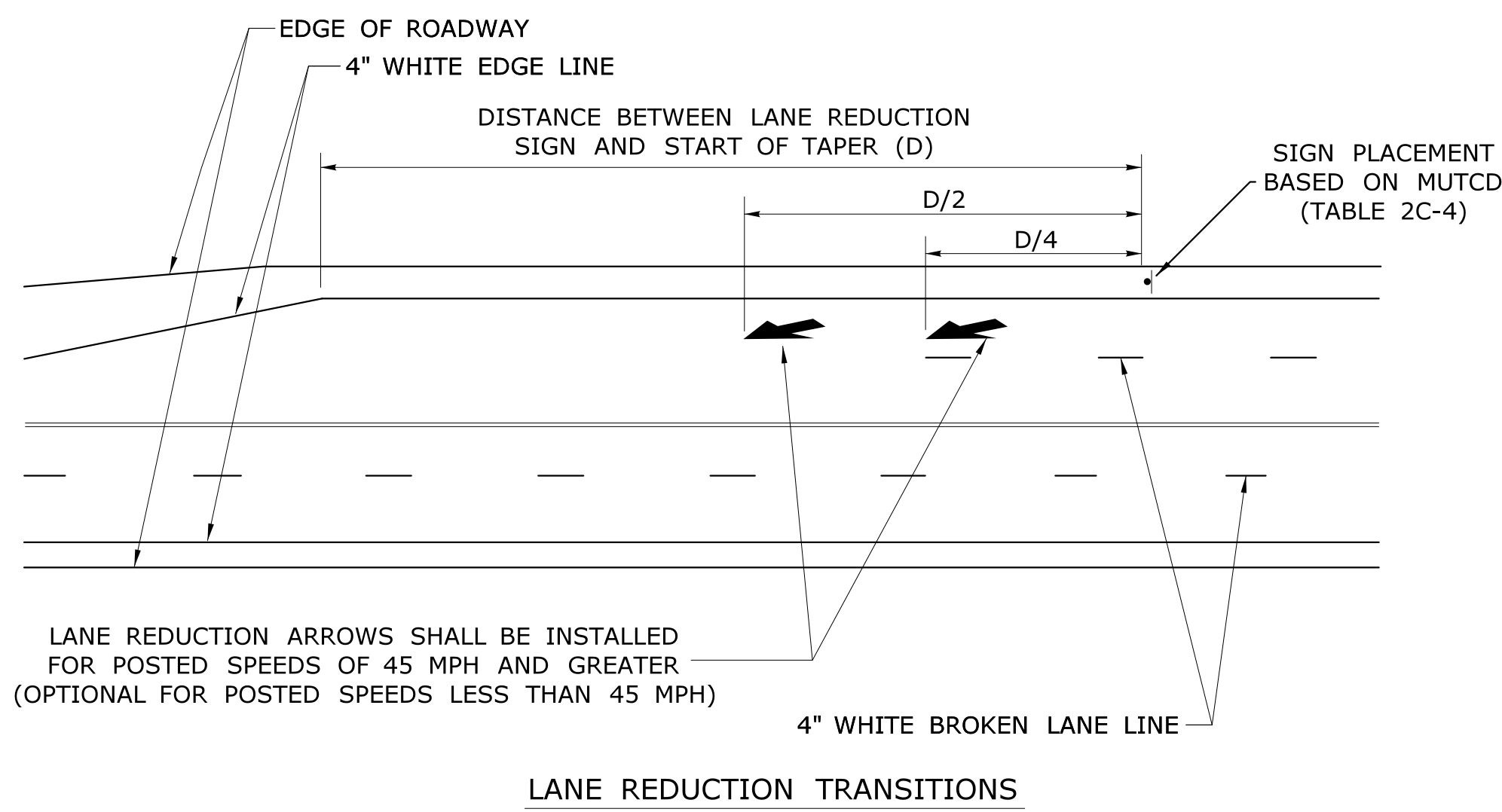
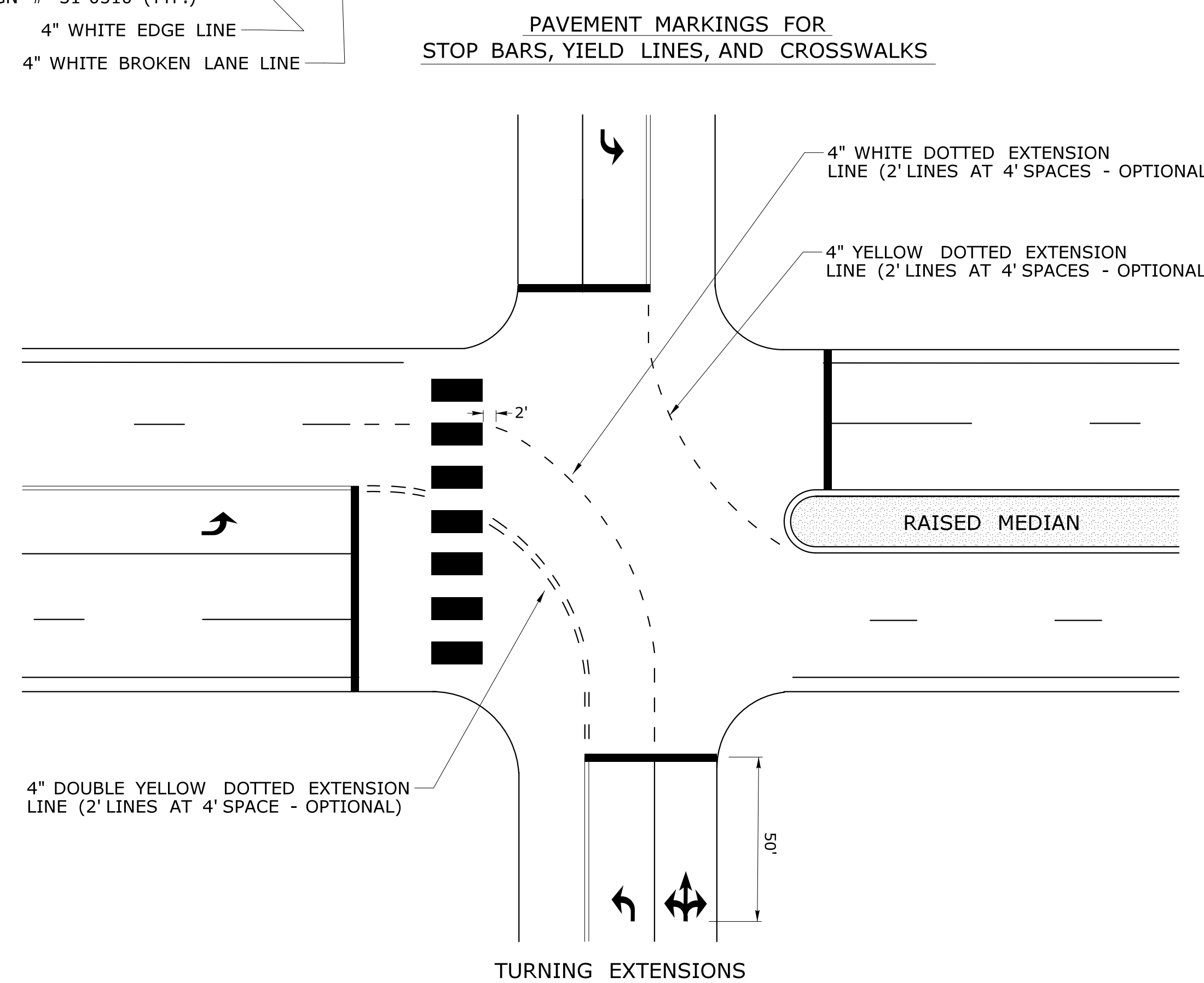
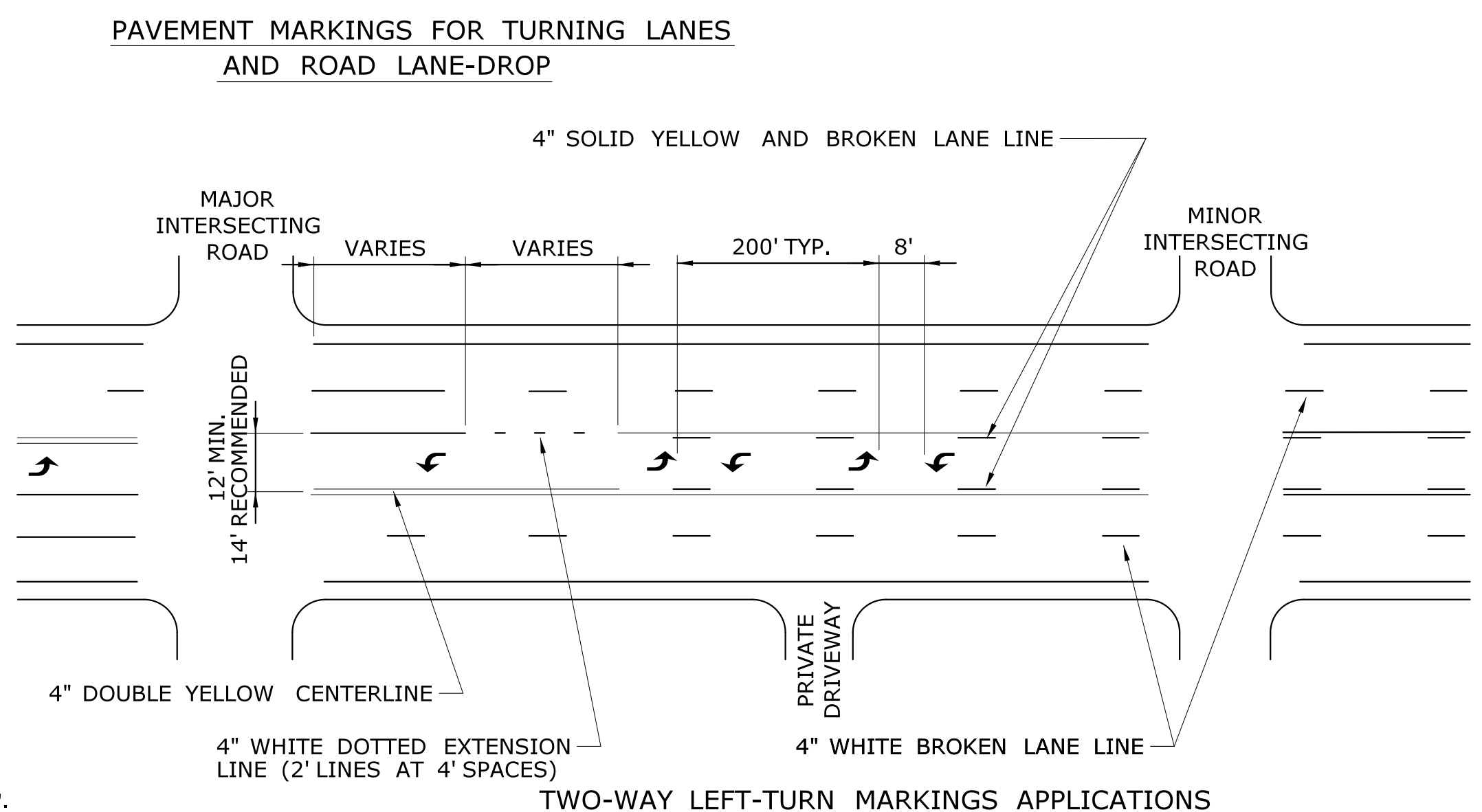
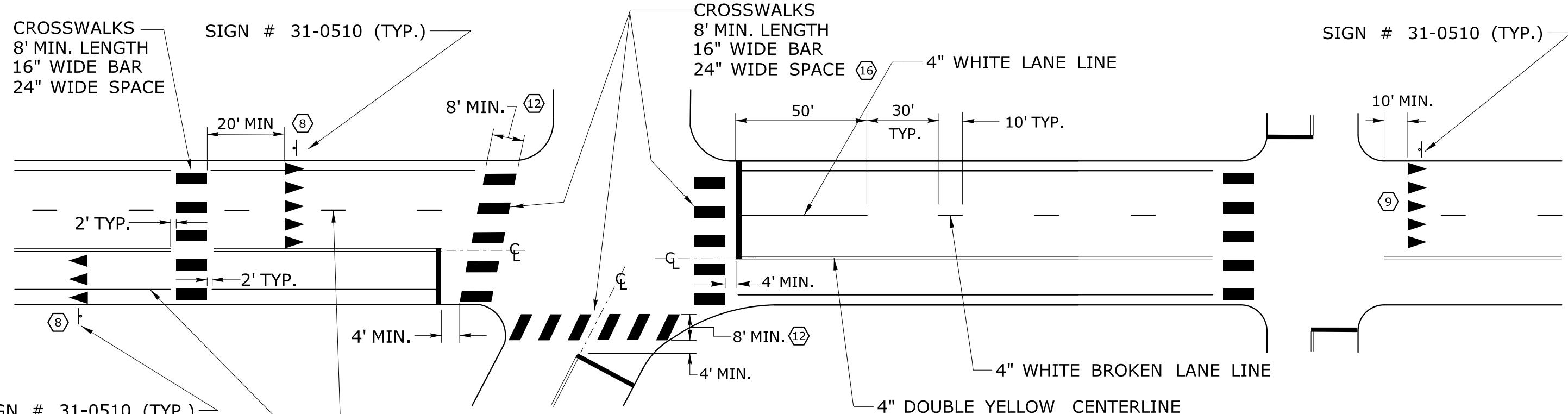
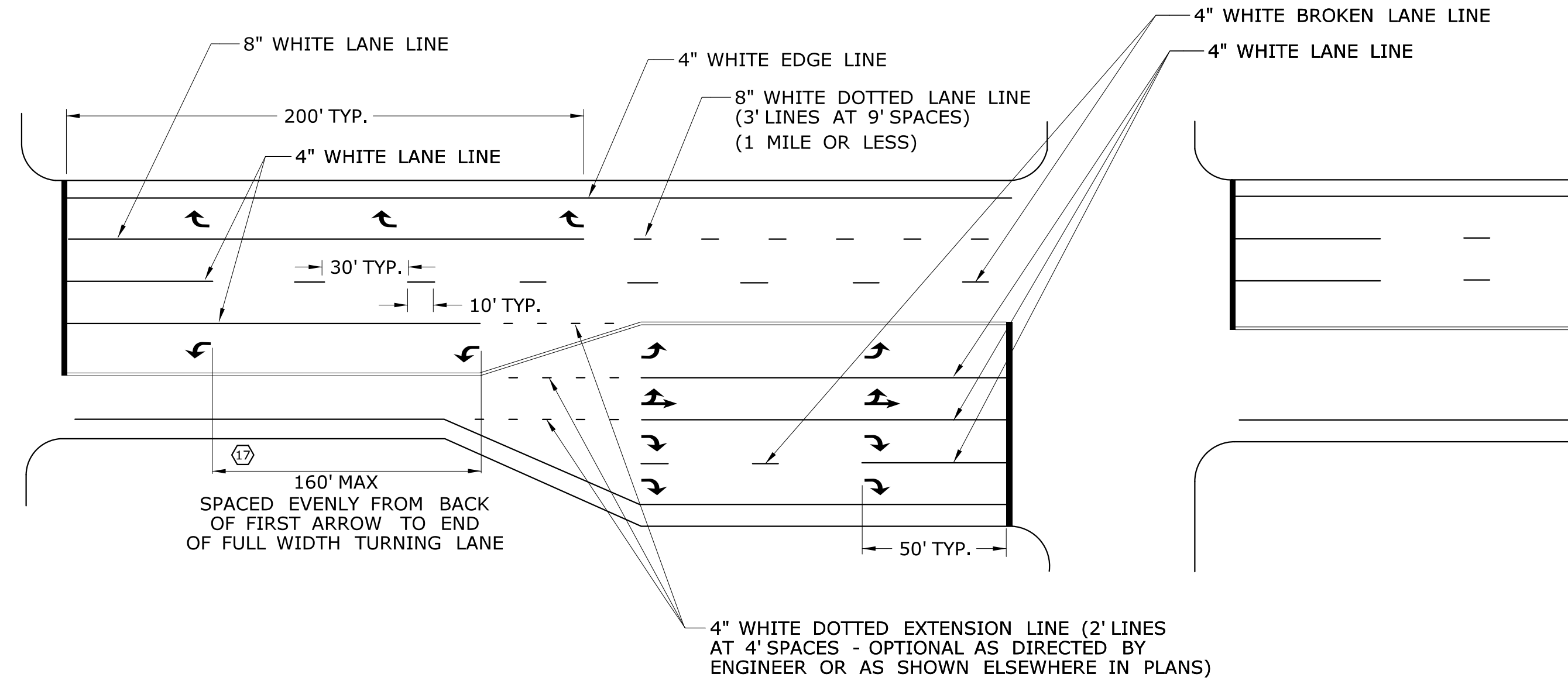
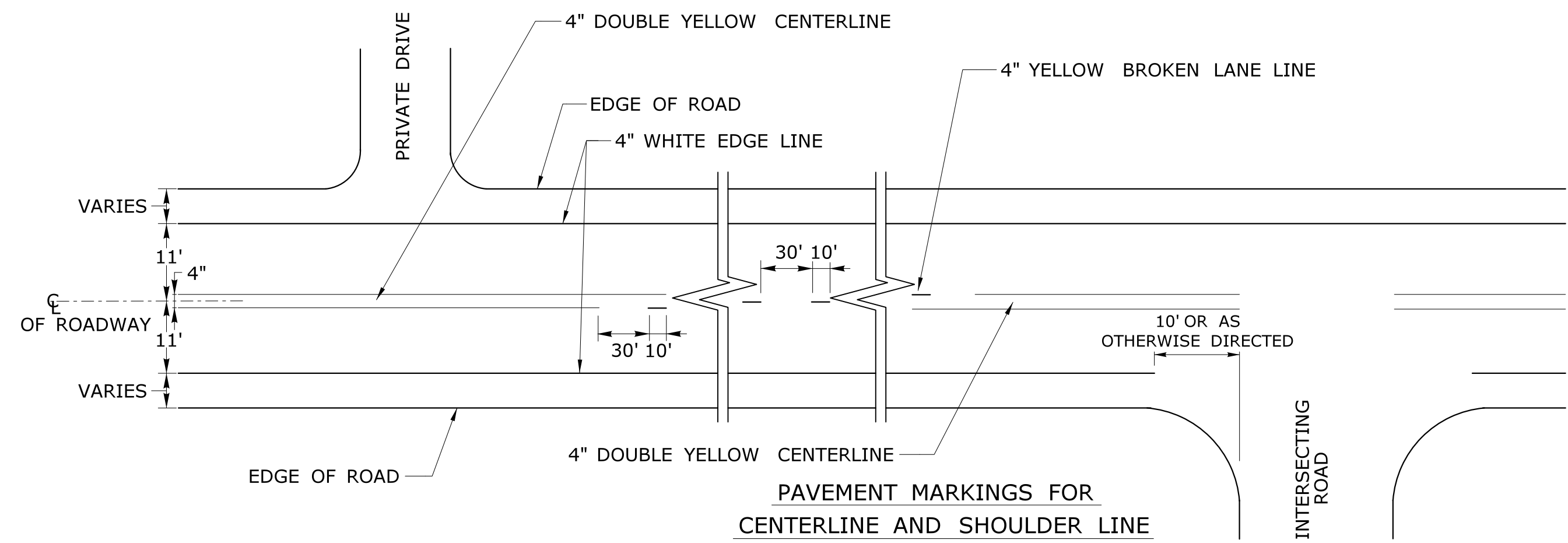


**U** TYPICAL RAMP TREATMENT FOR SHARED DOUBLE TURN LANES

**NOTES :**

1. IF LANE OR SHOULDER WIDTH IS LESS THAN DIMENSION SHOWN, CONTACT THE ENGINEER.
2. SEE DETAILS **J** AND **P** ON STANDARD SHEET "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS" (SHEET #TR-1210\_06) FOR RELATIONSHIP OF DIRECTIONAL LINES AND NORMAL EDGE LINE TO EDGE OF TRAVELWAY.
3. TWO WHITE WRONG WAY PAVEMENT ARROWS SHOULD BE INSTALLED ON ALL EXIT RAMP. THE FIRST SHALL BE INSTALLED 100' FROM THE LANE-USE ARROW(S), THE SECOND SHOULD BE 250'+ FROM THE FIRST WHERE SPACE PERMITS.
4. FOR EXIT RAMP WITH MORE THAN ONE LANE, INSTALL LANE USE ARROWS IN EACH LANE. INSTALL SETS OF LANE USE ARROWS EQUALLY SPACED FROM THE STOP BAR TO THE BEGINNING OF THE MULTI LANE SECTION (160' MAX SPACING).
5. USE A 4" WHITE LANE LINE TO SEPARATE AN EXCLUSIVE LEFT (OR RIGHT) TURN LANE FROM AN ADJACENT THROUGH LANE OR AN ADJACENT SHARED THROUGH LANE.
6. USE A 4" BROKEN LANE LINE TO SEPARATE DUAL EXCLUSIVE LEFT (OR RIGHT) TURN LANES.
7. DIMENSION FROM THE FIRST LANE USE ARROW TO STOP BAR IS FOR EXIT RAMP ONLY.
8. SEE STANDARD SHEETS "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS" (SHEET #TR-1210\_05 & SHEET #TR-1210\_06) FOR DETAILS **A** THRU **Q**.

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REV.	DATE	REVISION DESCRIPTION		Plotted Date: 4/3/2017		Filename: TR-1210_07.dgn		Model: CT_Civil_2D_Sheet					

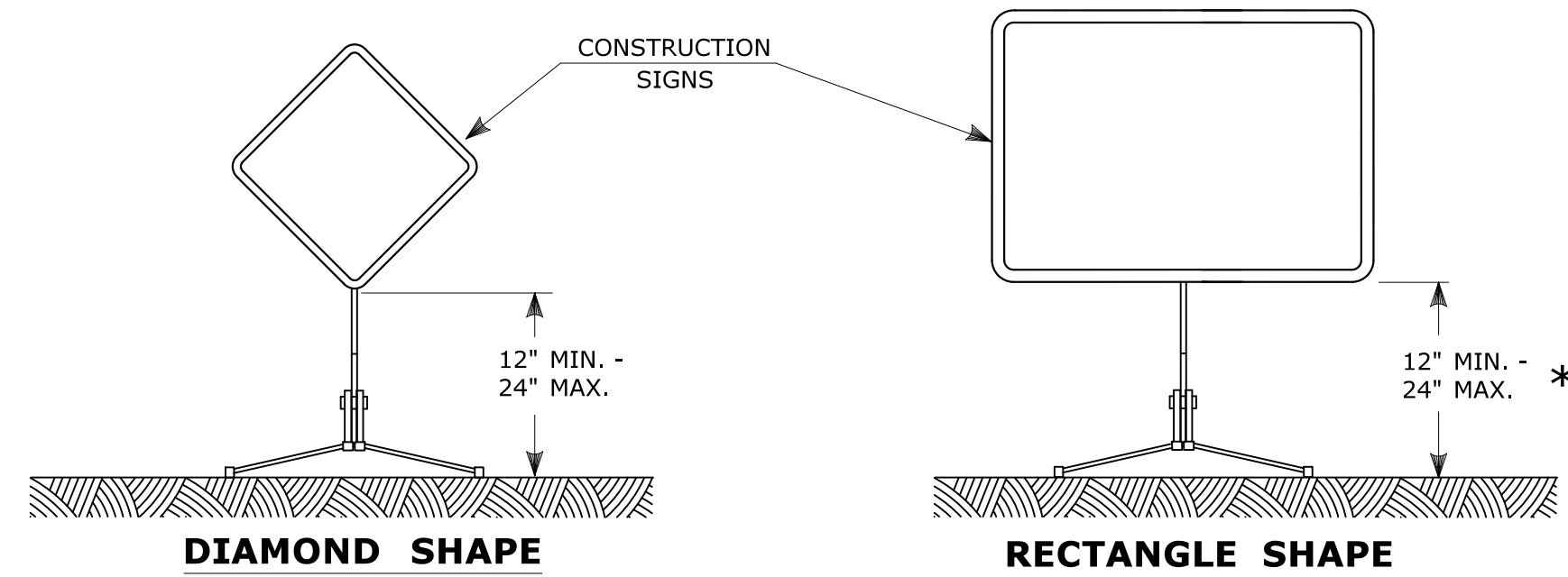


- NOTES:**  
**STOP BARS AND YIELD LINES**
1. STOP BARS AND YIELD LINES SHALL BE WHITE.
  2. STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
  3. STOP BARS TO BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
  4. IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
  5. THE STOP SIGN SHOULD BE PLACED IN LINE WITH THE STOP BAR. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
  6. FOR STOP BARS AT RAMP SEE DETAILS "R", "S", "T", & "U" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210 07 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS EXIT RAMP".
  7. FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.
- ⑧ MID-BLOCK CROSSWALKS ARE CROSSWALKS LOCATED MORE THAN 50 FEET FROM A SIGNALIZED OR UNSIGNALIZED INTERSECTION. YIELD LINES ASSOCIATED WITH MIDBLOCK CROSSWALKS SHOULD BE INSTALLED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE OR AS DIRECTED BY THE ENGINEER. SIGN # 31-0510 IS REQUIRED.
- ⑨ FOR CROSSWALKS AT UNSIGNALIZED INTERSECTIONS WITH MINOR STREET STOP CONTROL, YIELD LINES SHALL BE INSTALLED ON MULTI-LANE APPROACHES, BUT NOT SINGLE LANE APPROACHES.
10. THE YIELD SIGN SHOULD BE PLACED IN LINE WITH A YIELD LINE. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD, THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.
- CROSSWALKS**
11. CROSSWALK MARKINGS SHALL BE WHITE.
  12. AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO  $\epsilon$  AND ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.
  13. BARS SHOULD BE NO CLOSER THAN 1' FROM EDGE OF ROAD.
  14. ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
  15. DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.
  16. 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.
- PAVEMENT MARKINGS FOR TURNING LANES**
17. INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.



E5 - SERIES	G20 - SERIES	M4 - SERIES	R1 - SERIES	R9 & R11 - SERIES	W1 - SERIES	W3 - SERIES																																																																																																																																																																												
<p>E5-1</p> <p>COPY &amp; BORDER - WHITE BACKGROUND - GREEN</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>51-6147</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	51-6147	2	<p>G20-2a</p> <p>ROAD WORK NEXT 0 MILE(S) BE PREPARED TO STOP</p> <p>VARIABLE MILEAGE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>8.0</td><td>48X24</td><td>80-9612</td><td>2</td></tr> <tr><td>9.0</td><td>48</td><td>80-9711</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9712</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	8.0	48X24	80-9612	2	9.0	48	80-9711	1	16.0	48	80-9712	2	<p>M4-8</p> <p>VARIABLE ARROW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>2.0</td><td>24X12</td><td>80-9707</td><td>1</td></tr> </table> <p>M4-9b</p> <p>VARIABLE ARROW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>5.0</td><td>30X24</td><td>80-9703</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24X12	80-9707	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.0	30X24	80-9703	1	<p>R1-1</p> <p>* COPY &amp; BORDER - WHITE BACKGROUND - RED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>5.19</td><td>30</td><td>31-0552</td><td>1</td></tr> <tr><td>13.30</td><td>48</td><td>31-0557</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.19	30	31-0552	1	13.30	48	31-0557	2	<p>R9-9</p> <p>COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>3.75</td><td>30X18</td><td>80-9076</td><td>1</td></tr> </table> <p>R11-3a</p> <p>VARIABLE MILEAGE COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>12.5</td><td>60X30</td><td>80-9077</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.75	30X18	80-9076	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.5	60X30	80-9077	2	<p>W1-4</p> <p>(L) (R)</p> <p>W1-6</p> <p>(L) (R)</p>	<p>W3-1</p> <p>OCTAGON - RED W/ WHITE BORDER ARROW &amp; BORDER - BLACK BACKGROUND - FLUORESCENT ORANGE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9808</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9808	1																																																																																																
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D.O.T. #	POSTS	9.0	36	80-9901	1	16.0	48	80-9902	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9910L	2	16.0	48	80-9911R	2	<p>W13 - SERIES</p> <p>W13-1</p> <p>SUBPLATE VARIABLE SPEED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>4.0</td><td>24</td><td>80-9569</td><td>1</td></tr> <tr><td>6.25</td><td>30</td><td>80-9569</td><td>1</td></tr> <tr><td>9.0</td><td>36</td><td>80-9569</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9567</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	4.0	24	80-9569	1	6.25	30	80-9569	1	9.0	36	80-9569	1	16.0	48	80-9567	1	<p>W20 - SERIES</p> <p>W20-1</p> <p>(L) (R)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. 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D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9805</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9806</td><td>2</td></tr> </table> <p>W20-4</p> <p>VARIABLE MILEAGE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9834</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9835</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	6.25	30	80-9602	1	9.0	36	80-9603	1	16.0	48	80-9604	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9803	1	16.0	48	80-9804	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9614	1	16.0	48	80-9615	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9805	1	16.0	48	80-9806	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9834	1	16.0	48	80-9835	2	<p>W21 - SERIES</p> <p>W21-6</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9607</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9607	1	<p>W22 - SERIES</p> <p>W22-1</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9620</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9625</td><td>2</td></tr> </table> <p>W22-2</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9933</td><td>1</td></tr> <tr><td>12.0</td><td>96X18</td><td>80-9914</td><td>2</td></tr> <tr><td>3.33</td><td>48X10</td><td>80-9916</td><td>2</td></tr> </table> <p>W22-3</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>10.5</td><td>42X36</td><td>80-9623</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9620	1	16.0	48	80-9625	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9933	1	12.0	96X18	80-9914	2	3.33	48X10	80-9916	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.5	42X36	80-9623	2	<p>(VARIABLE LEGEND)</p> <p>(VARIABLE LEGEND)</p> <p>BLANK OR VARIABLE LEGEND</p> <p>STOP-SLOW PADDLE</p> <p>(SIDE A) STOP (SIDE B) SLOW</p> <p>COPY &amp; BORDER - RED BACKGROUND - WHITE</p> <p>(SIDE A) COPY &amp; BORDER - RED BACKGROUND - WHITE</p> <p>(SIDE B) COPY &amp; BORDER - ORANGE BACKGROUND - BLACK PLAIN</p> <p>NEW</p> <p>COPY &amp; BORDER - BLACK BACKGROUND - YELLOW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>2.51</td><td>19</td><td>80-9950</td><td>PADDLE</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.51	19	80-9950	PADDLE
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<p>5 8-2015 UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.</p> <p>4 6-2012 REVISED NOTE #1 TO REFERENCE "O.S.T.A."</p> <p>3 4-2012 REVISED NEW SIGNAL SIGN(S) TO CONFORM TO 2009 MUTCD.</p> <p>2 2-2011 MINOR REVISIONS.</p> <p>1 3-2010 REMOVED OBSOLETE SIGNS (50-5925, 50-5935).</p> <p>REV. DATE REVISION DESCRIPTION</p>	<p>Plotted Date: 8/25/2015</p>	<p>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</p> <p>NOT TO SCALE</p>	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p> <p>Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-1220_01</p>	<p>SUBMITTED BY: NAME/DATE/TIME:</p> <p>APPROVED BY: NAME/DATE/TIME:</p>	<p>CTDOT STANDARD SHEET</p> <p>OFFICE OF ENGINEERING</p>	<p>STANDARD SHEET TITLE:</p> <p>SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS</p>	<p>STANDARD SHEET NO.:</p> <p>TR-1220_01</p>																																																																																																																																																																											

- NOTES:
- R1-SERIES SIGN THE LEGEND "O.S.T.A." SHALL APPEAR.
  - POSTS - SEE STANDARD SHEET TR-1208-02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
  - POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
  - ALL POSTS NOTED ARE FOR LONG TERM INSTALLATION. SEE STANDARD SHEET TR-1208-02.
  - FOR TEMPORARY SUPPORTS SEE STANDARD SHEET TR-1220-02 - "CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES".
  - FOR SPECIFIC SIGN DESIGN, CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERRECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
  - ALL CONSTRUCTION SIGNS TO BE PAID FOR UNDER THE CONSTRUCTION SIGNS ITEM IN THE CONTRACT.
  - MATERIALS & COLORS SHALL CONFORM TO STATE SPECIFICATIONS.
- MATERIALS:
- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES. ALUMINUM THICKNESS FOR POST MOUNTED SIGNS SHALL BE .100" EXCEPT SIGN #s. 80-9815, 80-9728, 80-9519, & 51-6147 (L OR R) WHICH SHALL BE .125", PLYWOOD THICKNESS FOR POST MOUNTED SIGNS SHALL BE 1/2" EXTERIOR GRADE A-C OR BETTER. SIGN BLANKS SHALL HAVE ONE COAT OF PRIMER PAINT PRIOR TO APPLICATION OF RETROREFLECTIVE SHEETING & COPY.
- COLORS:
- BACKGROUND - FLUORESCENT ORANGE - EXCEPT AS NOTED.  
LEGEND - BLACK - EXCEPT AS NOTED.  
ALL SIGNS WITH FLUORESCENT ORANGE BACKGROUND TO USE TYPE VIII RETROREFLECTIVE SHEETING.  
ALL OTHER SIGNS TO USE TYPE IV RETROREFLECTIVE SHEETING EXCEPT AS NOTED BY \*.  
\* SIGNS TO USE TYPE IX RETROREFLECTIVE SHEETING.

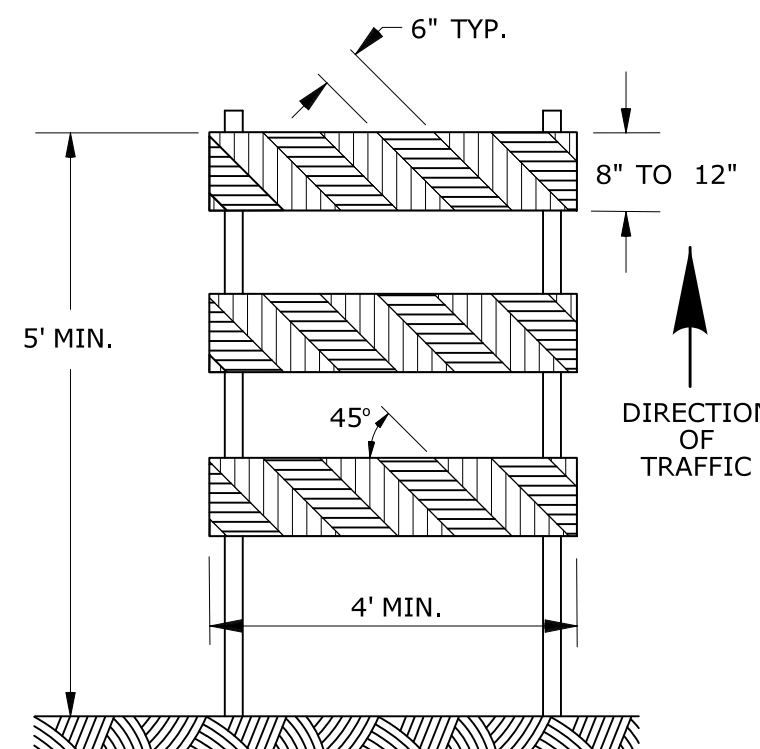
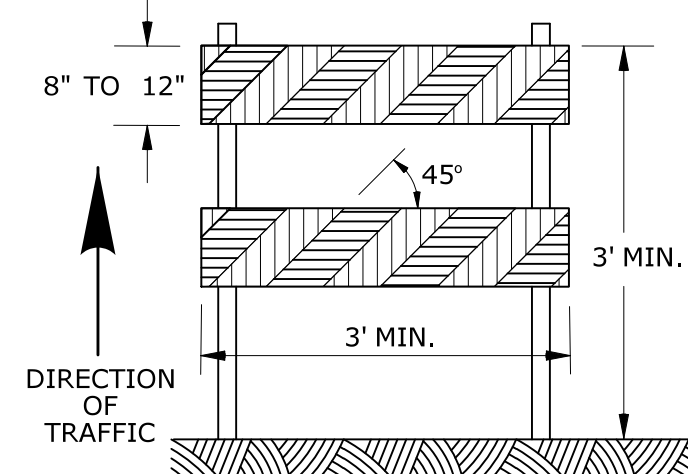
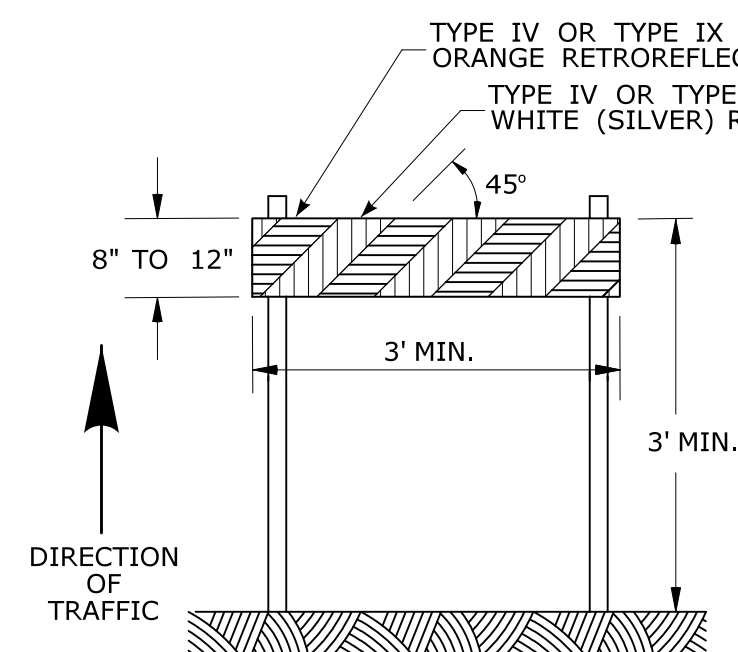


**PORTABLE CONSTRUCTION SIGNS**

NOTES FOR PORTABLE SIGN SUPPORTS:

- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
- PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220.01 - "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.

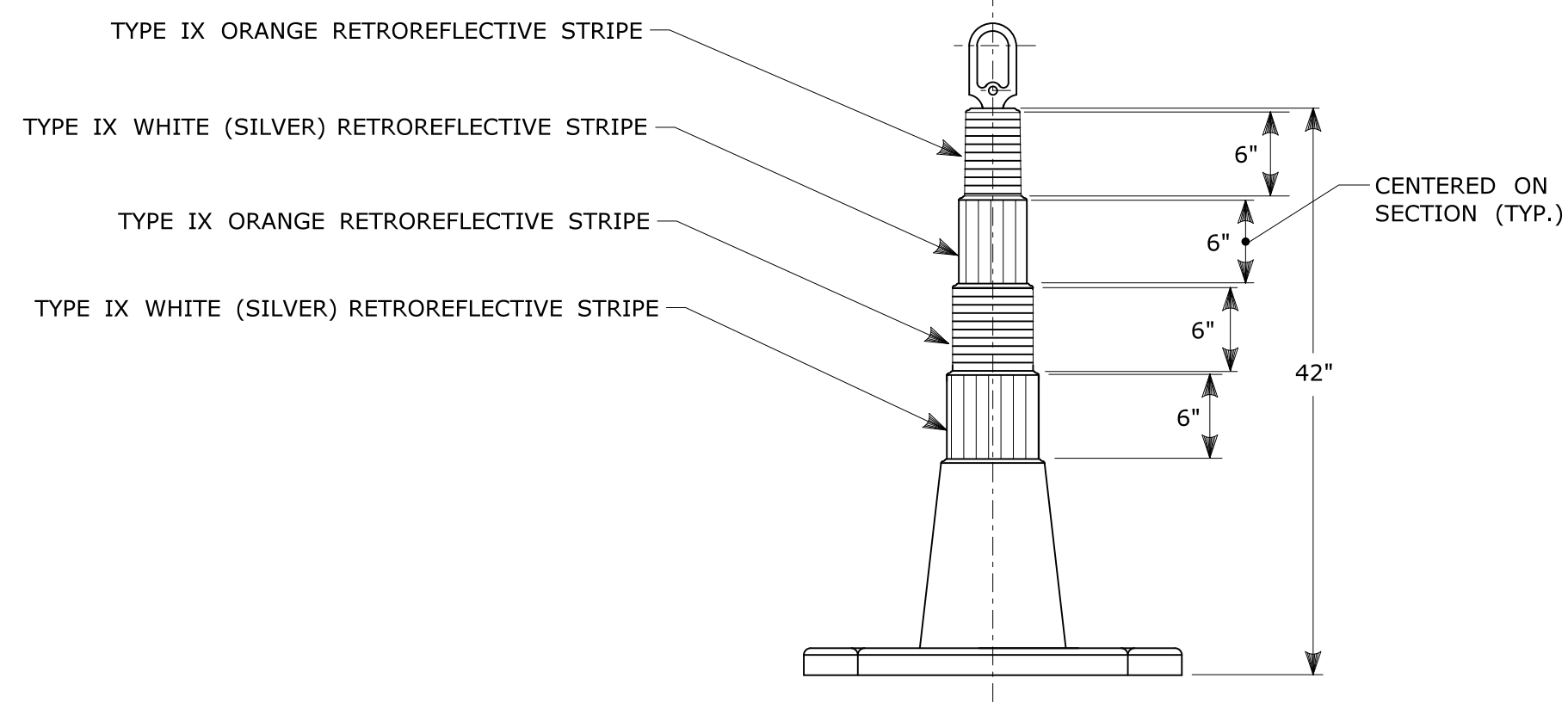
\* FOR E5-1 (EXIT SIGNS) USE MIN 48".



**CONSTRUCTION BARRICADES**

NOTES:

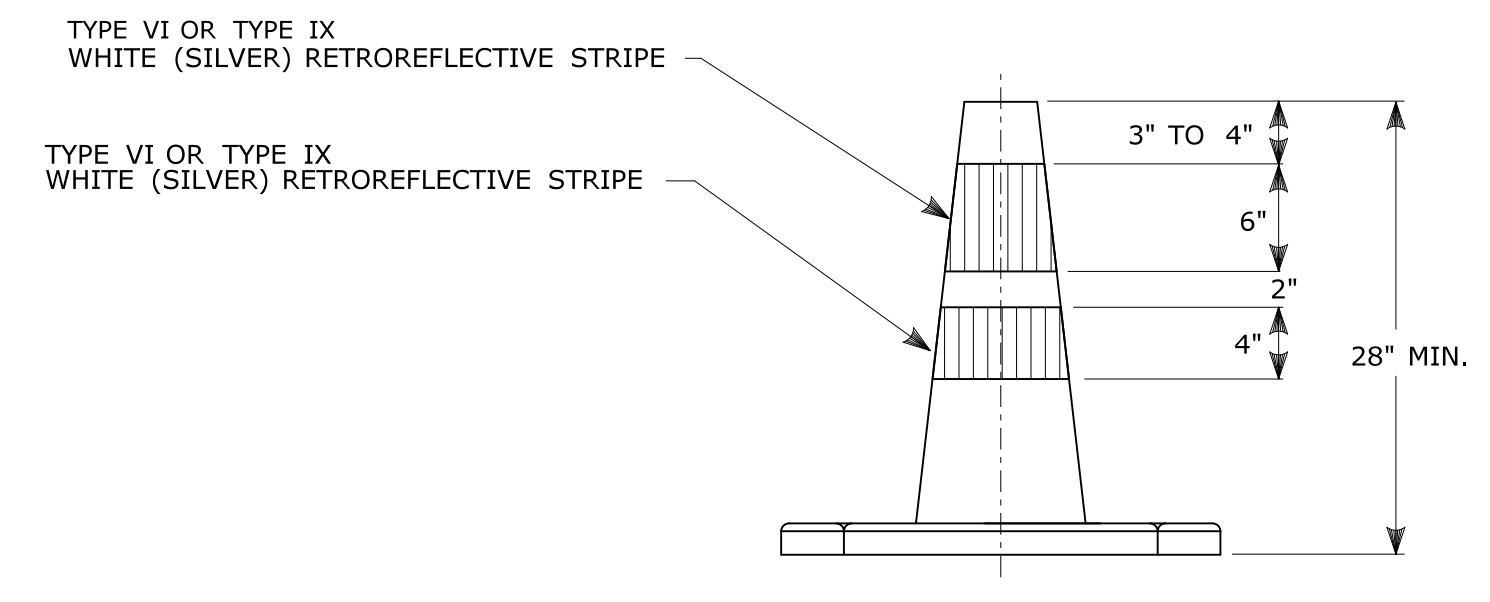
- CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
- MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
- THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



**42" TRAFFIC CONE**

NOTES:

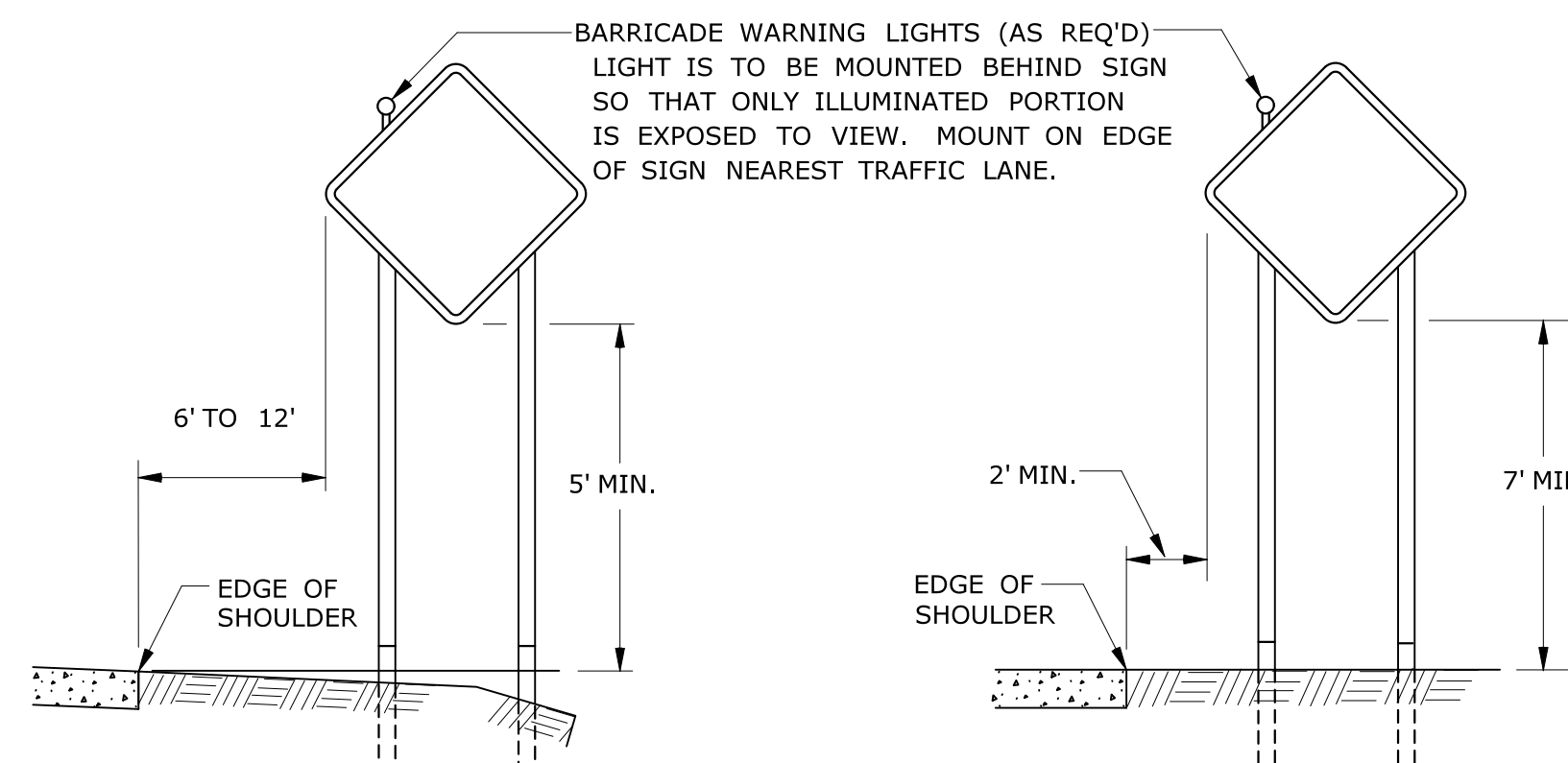
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



**TRAFFIC CONE**

NOTES:

- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



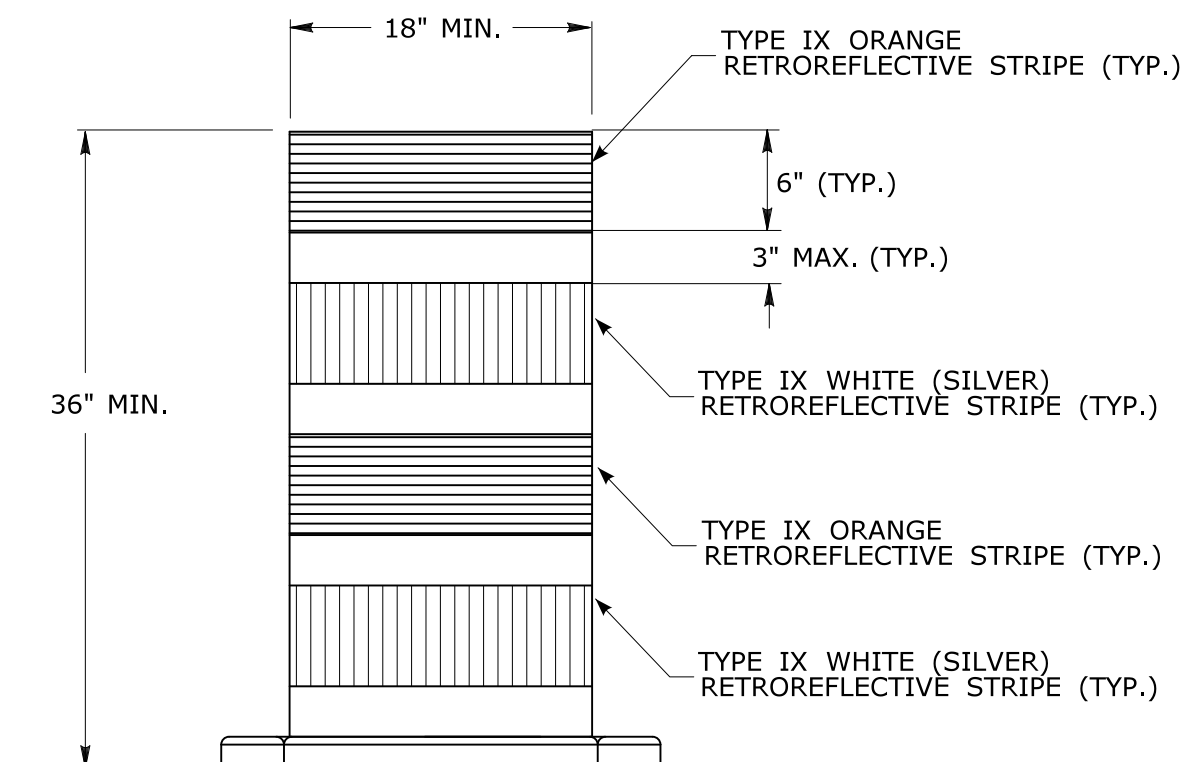
**RURAL AREA**

**URBAN AREA**

**PLACEMENT OF CONSTRUCTION SIGNS  
TYPICAL LONG TERM INSTALLATION**

NOTES:

- SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.
- SEE TYPICAL SHEETS:  
 "TYPICAL SIGN SUPPORT AND SIGN PLACEMENT DETAILS-GORE EXIT SIGN"  
 "TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS"



**TRAFFIC DRUM  
FRONT VIEW**

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.				SUBMITTED BY: _____ NAME/DATE/TIME: _____	<b>CTDOT STANDARD SHEET</b>	STANDARD SHEET TITLE: <b>CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES</b>	STANDARD SHEET NO.: <b>TR-1220_02</b>
2 8-2015 UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.	1 2-2011 MINOR REVISIONS.	NOT TO SCALE	APPROVED BY: _____ NAME/DATE/TIME: _____	<b>OFFICE OF ENGINEERING</b>			
REV. DATE REVISION DESCRIPTION	Plotted Date: 8/14/2015	File name: CTDOT_TRAFFIC_STD.DGN Model: TR-1220_02					